

The Port Dover Secondary Plan

DRAFT September 2021

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1.0 INTRODUCTION

1.1 Purpose

- a) The purpose of this Secondary Plan (this Plan) is to provide a detailed land use plan and policy framework to guide future growth and development within the Secondary Plan Area. The planning horizon is to the year 2036 to match the time horizons in the current Norfolk County Official Plan.

This Plan is also intended to provide a long-term planning framework to manage growth and development well beyond 2031, and is cognizant of the planning policy framework of the Provincial Policy Statement (2020), as well as the ongoing legislative changes of the Province that affect land use planning throughout Ontario.

- b) Port Dover is the second largest Urban Area in the County. The County will support and promote the continued development of Port Dover as a significant urban waterfront community and tourism node in the County. The County will support and promote Port Dover as a sustainable waterfront urban community focused on port and lake-based activities, and containing an efficient pattern of development. In the future, Port Dover will continue to grow and to evolve into a complete community that is well-balanced and attractive.

1.2 Administration

- a) All development in Port Dover shall conform to the vision, guiding principles, and policies of this Plan, along with:

- i) **Schedule A: Community Structure;**
- ii) **Schedule B: Land Use Plan;**
- iii) **Schedule C: Roads Plan;** and,
- iv) **Schedule D: *Active Transportation Plan*.**

- b) In addition, Urban Design Guidelines are attached as **Appendix I**, the Cultural Heritage Resources Map is attached as **Appendix II**, and Urban Forestry Guidelines are attached as **Appendix III**. The attached Appendices do not form a statutory part of this Plan.

- i) The Urban Design Guidelines are meant to guide the preparation of detailed development plans in accordance with the vision, principles and policies for Port Dover. It is the intent of this Plan that all new development be consistent with the Urban Design Guidelines; and,

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- ii) The Cultural Heritage Resources Map identifies the properties that have been listed or designated as significant cultural heritage resources within Port Dover. Those resources are subject to the policies of Section 5.7 of the Norfolk County Official Plan.
- iii) The Urban Forestry Guidelines provide best practices for cultivating a healthy urban forest, including appropriate species selection.
- c) This Plan includes sidebar notes and graphics that are provided to provide additional explanation to a number of key policies of this Plan. Sidebar notes and graphics are not a statutory element of this Plan.
- d) This Plan is to be read in its entirety, and all relevant policies are to be applied to all applications for development. This Plan is also to be read in conjunction with all the relevant policies of the Norfolk County Official Plan, which are to be conformed with when considering any application for development, or making any land use planning decision. Where there is a conflict between the policies of the Norfolk County Official Plan and this Plan, the policies of this Plan shall apply.
- e) In addition, all development shall be consistent with the relevant policies of the Provincial Policy Statement. Where this Plan makes reference to an Act of the Legislature or an Ontario Regulation, such reference shall include its successor upon amendment or replacement.
- f) This Plan shall conform with any decisions of the Ontario Land Tribunal (OLT), or its predecessors. Where there is a conflict between the policies of this Plan and a decision of the OLT, the decision of the OLT shall prevail.

2.0 VISION AND PRINCIPLES

2.1 Vision Statement

Port Dover will remain a healthy, sustainable and complete community growing and prospering within the context of its 18 kilometres of waterfront and recognizing and celebrating the history and character of the community. Visual and physical access to the waterfront for the public is a crucial element of the community.

The Downtown and Urban Waterfront Districts combined will remain the heart and soul of Port Dover, as well as a major tourist attraction with new development contributing to the creation of successful mixed use districts and neighbourhoods.

New development will contribute to establishing beautiful neighbourhoods, a network of green spaces and protected natural areas, a connected trail system, safe streets and a variety of shops and services for both residents and tourists.

2.2 Guiding Principles

- a) The Secondary Plan includes Guiding Principles' that alongside the Vision Statement to give direction on how the physical aspects of the community will be developed.

Principle 1: Downtown Port Dover will thrive as a destination for residents and visitors alike to access shopping, restaurants, culture and entertainment while new development will be compatible in character, and will enhance adjacent streetscapes and public spaces.

Principle 2: Port Dover's vibrant Urban Waterfront District will continue to be a pedestrian focused tourist destination that provides for a mixture of land uses, including residential uses. Development will be compatible in character, and shall celebrate the waterfront. Physical and visual access to the waterfront will be preserved and enhanced.

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- Principle 3:** Throughout Port Dover, the public's visual and physical access to the waterfront will be a priority and will be preserved and enhanced over time. A variety of both land-based and water-based recreational activities will be promoted adjacent to the waterfront for both residents and tourists alike.
- Principle 4:** Existing neighbourhoods throughout Port Dover will evolve over time, and are expected to include small-scale intensification projects and Additional Residential Units. All new development within an existing neighbourhood will be compatible with existing development.
- Principle 5:** New neighbourhoods will be well connected, offer a range and mix of housing types (including affordable and rental housing), a mix of appropriately scaled retail and service commercial uses and community facilities, with green space connecting to the broader network.
- Principle 6:** Retail, service commercial uses, tourist-focused facilities and community facilities will remain important components of a complete community that will help to ensure Port Dover is a community to live, work, shop and play.
- Principle 7:** Cultural heritage conservation and enhancement will play an important role when new development is proposed on a site that includes, or is in proximity to, an identified cultural heritage resource.
- Principle 8:** The health and connectivity of a network of public spaces, natural heritage features and their associated ecological functions will be protected and enhanced for the long-term.
- Principle 9:** A well-connected, multi-modal transportation network that gives priority to creating safe streets for pedestrians and cyclists while providing a balanced supply of parking will be provided throughout Port Dover. The concepts of complete streets and active transportation will be incorporated into all transportation planning decisions.
- Principle 10:** Development will be planned in a logical, efficient and cost effective manner, and will be coordinated with planning for transportation and municipal infrastructure.

3.0 BUILDING A SUCCESSFUL COMMUNITY

3.1 Objectives for a Successful Community

- a) A successful community incorporates a range of elements that work in combination to ensure a well-functioning, beautiful and desirable place to live, to work and to play. A successful community is:
- i) ***A Complete Community*** - A complete community meets people's needs for daily living throughout an entire lifetime by providing convenient access to an appropriate mix of jobs, local services, a full range of housing, including affordable housing, public services and community infrastructure including educational and recreational facilities. A complete community has a robust open space system. Convenient access to options for Active Transportation are crucial elements of a complete community. Objectives are:
- » To accommodate a range and mix of housing types to allow for multiple choices and enhanced opportunities for residents to remain in the community for their entire lifecycle.
 - » To create a community that is walkable, with connected public gathering places, where opportunities for social interaction are increased and where commercial and community services can be provided within easy walking and cycling distances and that meet the needs of people of all ages, backgrounds, and capabilities throughout the various stages of their lives; and,
 - » To ensure easy access for all residents to a wide range of uses including shopping and restaurants, parks and open spaces, employment opportunities, educational and cultural opportunities, live-work options, mobility options, a mix and diversity of housing types, and a range of community facilities, services and amenities.

- ii) ***A Viable Community*** - A viable community has a strong economy that is market responsive, efficient and cost effective. Objectives are:
- » To attract long-term economic development opportunities by protecting for, and establishing opportunities for businesses and commercial activities focused on residents and tourists;
 - » To ensure that development is cost-effective and appropriate for the market place, including the flexibility to respond to, and ensure positive changes in the market place over time;
 - » To optimize the use of existing infrastructure investments and to promote the coordinated, efficient and cost-effective delivery of service infrastructure and community infrastructure that is appropriate for the planned urban development forms; and,
 - » To ensure that all development is sustainable and financially viable to the County over its life cycle.

- iii) ***A Beautiful and High Quality Community*** - A high quality and ultimately a beautiful community includes well designed buildings and streetscapes. A beautiful community protects its waterfronts and river banks, natural heritage features and viewscapes and includes an accessible and well-designed system of public parks and open spaces that celebrate the community, and provide opportunities for enjoyment by the entire population. A high quality community shall engender a sense of pride as a place to live and a sense of stewardship in its long-term care and maintenance. Objectives are:

- » To develop a welcoming community that encourages and supports active living, social engagement, civic pride and the creation of a sense of place and wellbeing. Fundamental to achieving this objective is the celebration of the views over the water and public access to the Lake Erie and Silver Lake shorelines and the river banks of the Lynn and Black Rivers;
- » To include gateways that clearly identify where you are, and when you have entered. Gateways help recognize entry points into the community. Gateways can include buildings, structural elements and/or landscape features;

- » To build beautiful streets and streetscapes. Streets need to accommodate all modes of transportation and be designed to be pedestrian friendly and safe. Building facades play a crucial role in defining the street edge, animating the street and creating the image and character of the community. Together the streets and the adjacent building facades create a streetscape;
- » To ensure that parks and open spaces, as well as Port Dover's waterfront, are beautiful, accessible and linked; and,
- » To require high quality architecture that transcends a theme or a specific period in time is fundamental to a beautiful community. Buildings shall be compatible with one another, but there must be a diversity of scale and a diversity of style as it may be defined through building materials, colour and architectural details.

iv) ***A Healthy Community*** - A healthy community consciously seeks to improve the health of its citizens by putting public health high on the social and political agenda. Physical, social and mental wellbeing are the necessary components of public health, including access to health care, healthy food, clean air and water, and opportunities for physical activity. A fundamental element of a healthy community is the inclusion of active transportation. Active transportation refers to any form of human-powered transportation - walking, cycling, using a wheelchair, scooters, inline skating or skateboarding. Objectives are:

- » To plan for an active transportation system that is highly integrated and connected within the community, and to transportation systems that serve the broader region;
- » To design the community around pedestrian activity with a substantial number of destinations, including parks, cultural and community facilities, shopping and restaurant opportunities within walking distance to promote walking and cycling that encourages daily physical activity;
- » To ensure that the appropriate level of infrastructure and amenities are provided along active transportation routes to ensure pedestrian comfort, and enjoyable and safe environments through which to travel; and,

- » To plan for “age-in-place” facilities within the community that anticipate changing housing needs for an aging population.

v) **A Sustainable Community** - Sustainability is commonly referred to as activities that meet present needs without compromising the ability of future generations to meet their own needs. A sustainable community is environmentally and socially healthy and resilient. It meets the challenges of climate change, and other environmental issues through integrated solutions rather than through fragmented, incremental approaches that meet one objective at the expense of the others. A sustainable community manages its human, natural and financial resources equitably and takes a long-term view - one that is focused on both present and future generations. Sustainability success relies upon having specific and measurable targets for indicators related to energy, water, carbon and waste. Objectives are:

- » To protect and enhance local and regional ecosystems and biological diversity;
- » To promote the responsible use of resources to ensure long-term sustainability, reduce greenhouse gas emissions, and reduce demands for energy, water and waste systems;
- » To demonstrate leadership in sustainable forms of green building design and technology, including the incorporation of renewable and alternative energy sources; and,
- » To require a transportation system that reduces the reliance on the automobile as the primary mode of transportation and promotes active transportation.

vi) **A Resilient Community** - A resilient community can effectively respond to emergencies because it has a plan in place, responsibilities assigned and facilities available. Natural or human made disasters are considered and the necessities of life are provided, particularly for those who are most at risk. Access to power, food, water and health care is ensured, while emergency services are equipped to operate and provide assistance in all conditions. Objectives are:

- » To ensure access to power, food, water and health care services during and immediately following a disaster event; and,

- » To ensure that health care and emergency service facilities are located within the community, and to ensure that they are adequately equipped to operate and provide assistance in all conditions.

3.2 Policies for a Successful Community

Promoting the Economy

- a) Ongoing and enhanced economic development is a fundamental prerequisite to Port Dover's future success and sustainability. The attraction of real estate investment and tourism are key elements of the vision for the Port Dover community.
- b) The County will plan for a strong and healthy economy and anticipate changing economic trends. To help attract and retain a diverse and skilled labour force, the County will support a strong and healthy economy by:
 - i) Promoting ongoing opportunities for the expansion and diversification of Port Dover's retail and service commercial sector;
 - ii) Constructing, upgrading and maintaining high quality municipal infrastructure systems and community facilities;
 - iii) Facilitating efficient and convenient transportation options for the movement of people and goods; and,
 - iv) Supporting options for live/work units and planning for an appropriate range of Home-based businesses.
- c) The County will support tourism as an integral part of economic development. The County acknowledges that tourism will continue to bring economic benefits to Port Dover. Tourism is supported by having a healthy, livable and diverse community, which includes visual and physical public access to the waterfront, to the parks system and other outdoor leisure and recreational opportunities, which are connected by an integrated active transportation system. To further develop water-based tourism, opportunities shall be identified to:
 - i) Improve physical and visual public access to the waterfront;
 - ii) Improve existing marina facilities; and,
 - iii) Encourage year-round access, parking, accommodation facilities and related commercial uses to support water-based tourism activities.
 - iv) Ensuring an Adequate Housing Supply/Affordable Housing

d) Providing a range of housing types includes not only various forms, sizes and tenures, but also includes affordable and special needs housing. Residents of all ages, income levels and abilities rely on a range and mix of housing types to offer a meaningful place to grow and a safe and secure place to live. An appropriate range of housing choices contributes to the overall health and well-being of communities.

e) The County shall ensure a diversity of housing options are available to residents of all ages, abilities and incomes. The County will work with the private sector and other stakeholders to provide a full range of housing types to meet the projected demographic and market requirements of the current and future residents of Port Dover. The County may become directly involved in the supply of housing through land acquisitions and development partnerships.

f) The County will develop an affordable housing implementation framework to help achieve the affordable housing target of a minimum of 25% of new housing units in Port Dover. The County shall promote the supply of new affordable housing in a variety of locations, dwelling types and tenures. Policies for the provision of affordable housing in Port Dover are as follows:

- i) Affordable housing must include a mix and range of building types, lots sizes, unit types/sizes, and tenures to provide opportunity for all household types, including larger families and residents with special needs;
- ii) Identifying optimal sites, including publicly-owned lands for affordable housing, early in the development process to maximize affordable/accessible housing funding opportunities in consultation with the building industry, non-profit agencies and other stakeholders. This includes:
 - » Working with the development industry to achieve affordable/accessible housing targets;
 - » Encouraging the development of intrinsically affordable housing, which includes modest amenities, standard materials, minimal details and flexibility within units; and,
 - » Support for the development of additional residential units as a key component of intensification throughout Port Dover's existing neighbourhoods.

- g) Further, the County will consider innovative financial arrangements, tools, policies and partnerships to encourage the private sector and other stakeholders in the development and maintenance of non-profit and affordable/accessible housing, such as:
- i) Pre-zoning identified sites;
 - ii) The allocation of sewage treatment and water supply capacity;
 - iii) Community Improvement Plans;
 - iv) Grants, or other financial incentives, funded through the Community Benefits Charge By-law; and,
 - v) Reduced municipal fees and charges.

Promoting Sustainability and Climate Change Mitigation

- h) The County will use its array of planning tools to help guide development and redevelopment to be more sustainable. The County may consider the use of the Community Benefits By-law, Community Improvement Plans and associated incentive programs to assist with the implementation of sustainable development design standards, including the following initiatives:
- i) Maximizing vegetation to support improved air quality;
 - ii) Reducing greenhouse gas emissions;
 - iii) Maximizing opportunities for the use of renewable energy systems; and,
 - iv) Promoting the use of active transportation and reduced vehicle trips.

The County shall work to improve County-wide transit services, carpooling and other traffic demand management measures to reduce greenhouse gas emissions from transportation. The County may prepare Air Quality and Climate Change Reports to monitor its progress towards reducing emissions of air pollutants and greenhouse gases, and to increase awareness of air quality and climate change.

Conserving Water

- i) The County shall promote water conservation by encouraging for all new development:
- i) Achieving 10% greater water efficiency than the Ontario Building Code and to encourage, through appropriate incentive programs, 20% greater water efficiency than the Ontario Building Code; and,

- ii) Including the installation of rainwater harvesting and re-circulation/reuse systems for outdoor irrigation and outdoor water use. In addition, other water conservation opportunities to be considered include the use of water efficient and drought resistant plant materials in parks, along streetscapes and in public and private landscaping, including;
- iii) Avoidance of turf grass areas, and when required, install drought resistant sod; and,
- iv) Reduce the impact caused by new development in the natural hydrological cycle by installing permeable driveway and parking lot surfaces.

Conserving Energy

- j) The County supports development that minimizes energy consumption, optimizes passive solar gains through design, and makes use of renewable, on-site generation and district energy options including but not limited to solar, wind, biomass, and geothermal energy. As such:
 - i) The County will promote reducing energy consumption in all County-owned, maintained and operated facilities and equipment., which shall be designed to at a minimum, LEED Silver Certification or similar standards to reduce energy consumption and incorporate renewable energy sources; and,
 - ii) Existing County buildings will be retro-fitted to implement more sustainable design construction features wherever possible; and,
 - iii) The County will encourage all new development to include energy efficient building design and practices and targets, including:
 - » Grade-related (3.5 storeys or less) residential buildings achieve a performance level that is equal to a rating of 83 or more when evaluated in accordance with Natural Resources Canada's EnerGuide for New Houses: Administrative and Technical Procedures;
 - » Medium density and high density residential (3.5 storeys and greater) and non-residential buildings be designed to achieve 40% greater efficiency than the Model Energy Code for Buildings, 1997; and,
 - » Non-residential and mixed use buildings shall be designed to achieve 25% greater energy efficiency than the Model National Energy Code for Buildings, 1997.

Preserving and Enhancing the Waterfront

- k) The County will preserve and enhance the public's visual and physical access to the waterfront in the following ways:
 - i) Where development is proposed, the provision of public access to the waterfront shall be pursued through:
 - » Parkland dedication required under the Planning Act, or purchases from funds allocated through the cash-in-lieu of parkland dedication; and/or,
 - » Negotiations through the development approval process; and,
 - ii) By pursuing initiatives outside of the development approval process, such as:
 - » Gifts/Bequests;
 - » Land exchanges;
 - » Purchases;
 - » Easement agreements;
 - » Partnering with individual landowners; and/or,
 - » Partnering with community organizations or service clubs.

Protecting and Enhancing the Natural Heritage System

- l) The County shall protect and enhance the natural heritage system within Port Dover. The relevant policies of Section 3.0 Sustainable Natural Heritage of the Norfolk County Official Plan shall be applied when considering all development within Port Dover.

Forest Resource Enhancement

- m) The County will preserve, protect, manage, replace and, where appropriate acquire, tree stands, hedgerows, woodlands and forested areas within Port Dover. It is a policy of this Secondary Plan that the County achieve a minimum of 40 percent tree canopy cover within Port Dover by 2031. To this end, the County shall:
 - i) Implement a Tree Protection By-law, which will include a tree replacement ratio where tree removal is unavoidable;
 - ii) Implement street tree and naturalization programs to increase urban canopy cover; and,

- iii) Require the planting of trees in all public works projects.

Promoting Local Food Production

- n) The creation of opportunities for local food production is supported by the County. Development plans and building designs shall provide opportunities for local food growing and production through:
 - i) Community gardens;
 - ii) Edible landscapes;
 - iii) Small scale food processing (i.e. community kitchens, food co-ops, community food centres);
 - iv) Food-related home occupations/industries; and,
 - v) A farmer's market.

Conserving Cultural Heritage Resources

- o) Cultural Heritage Resources include archaeological resources, built heritage resources and cultural heritage landscapes. The combination of human-made buildings and structures as well as the natural landscape create an area that is valued by the community. The listed and designated cultural heritage resources within Port Dover are identified in **Appendix II**. The County's policies for the conservation of cultural heritage resources are included in Section 5.7 of the Norfolk County Official Plan.

Ensuring Compatible Development

- p) The principle of compatible development will be applied to all applications for development within Port Dover to ensure the sensitive integration of new development with existing built forms and landscapes in a way that enhances the image and character of the community. The definition of compatible development is as follows:

"Compatible development is not development that is either the same as, or even similar to development in the vicinity. Compatible development is development that enhances the character of the existing community, without causing any undue, adverse impact on adjacent properties."

- q) Within the framework of compatible development, the County shall support contemporary architecture and design approaches that interpret and enrich living culture, and thereby creates a 21st century cultural heritage for future generations. To ensure development is compatible, when considering any application for development within Port Dover, particular attention shall be paid to the following:

- i) The landscape, built form and functional character of the surrounding community is enhanced;
- ii) No undue, adverse impacts are created on adjacent properties in the vicinity;
- iii) The height and massing of nearby buildings is appropriately considered, and buffers and/or transitions in height and density to adjacent properties are implemented, where necessary;
- iv) On-site amenity space is provided and it is reflective of the existing patterns of private and public amenity space in the vicinity;
- v) Streetscape patterns, including block lengths, setbacks and building separations are maintained; and,
- vi) Utility, infrastructure and transportation system capacity exists to serve the proposed development and there are no undue adverse impacts on the County's sewer, water, storm water management and transportation systems.

Crime Prevention Through Environmental Design

- r) The County will promote building and site design that assist in the reduction of the incidence of crime through the implementation of Crime Prevention Through Environmental Design (CPTED) principles including:
 - i) Adequate lighting;
 - ii) Clear sight lines, allowing view from one end of the walkway to the other;
 - iii) Appropriate landscaping, but avoiding landscaping that might create blind spots or hiding places;
 - iv) Adequate fencing and fenestration;
 - v) Clear signage that delineates permitted use and speed; and,
 - vi) Streetscape and building design that promotes 'eyes on the street'.

Accessibility for Ontarians with Disabilities Act

- s) The County has a duty to accommodate persons with disabilities that applies to all forms of development within Port Dover. The County will consider accessibility for persons with disabilities in all land-use planning and development decisions. As such:

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- i) All new and/or renovated County-owned, leased, or operated facilities, parks and open spaces, municipal infrastructure systems, and any other space that is accessible to the public, shall comply with the accessibility requirements as set out by the Ontario Building Code and the Accessibility for Ontarians with Disabilities Act; and,
- ii) All new private sector development shall incorporate universal physical access features and follow the accessibility requirements as set out by the Ontario Building Code and the Accessibility for Ontarians with Disabilities Act. Accessible private sector development shall be achieved through Site Plan Approval, enforcement of the Building Code, and the implementation of all applicable Provincial legislation and standards.

4.0 GROWTH MANAGEMENT

4.1 Growth Projections

- a) Growth in Port Dover shall be established in the Norfolk County Official Plan, as it is amended from time to time. It is anticipated that as Port Dover builds out to its defined community boundary, it will have the capacity to accommodate between 20,000 and 25,000 residents through a combination of new neighbourhoods and residential intensification. It is expected that level of growth will create a significant demand for:
- i) Enhanced retail and service commercial facilities, as well as community facilities; and,
 - ii) Long-term and comprehensive municipal infrastructure capacity, including sewage treatment facilities and an adequate water supply.

It is also anticipated that the achievement of that level of growth in Port Dover will not occur within the time horizon of this Plan, and it will be appropriately phased in accordance with the phasing policies of this Plan.

4.2 Community Structure

- a) Port Dover is unique and will accommodate a varied range and type of growth and development. Overall the community is expected to be a complete community that will accommodate:
- i) A full range of housing types, including affordable and special needs housing;
 - ii) Business opportunities to provide a wide range of employment and services to residents, businesses and visitors;
 - iii) A concentration of public service facilities, including social, cultural, entertainment, health, educational and other supporting facilities; and
 - iv) an open space, natural heritage and recreational network that is integrated with open spaces throughout the County, and provides appropriate passive, natural and active areas; and,
 - v) Development on full municipal services, and an appropriate level of transportation infrastructure.

- b) Further, the County shall ensure through its planning activities that Port Dover develops with efficient land use patterns that minimize the extension of municipal service infrastructure and will sustain the community and financial well-being of the County over the long-term.
- c) **Schedule A - Community Structure** identifies, conceptually, the urban structure of Port Dover. It identifies a number of key geographic components of the community that help articulate where and how growth will occur, and that will consequently influence the community's success in achieving the identified vision for the future. **Schedule A** identifies the following components:
 - i) **The Settlement Area Boundary** – The Settlement Area Boundary of Port Dover establishes the jurisdictional extent of this Secondary Plan. It is expected that all of the forecasted population and employment growth can be accommodated within the Settlement Area Boundary identified on **Schedule A**;
 - ii) **The Greenlands System** – The Greenlands System provides an important structural element throughout Port Dover, and generally identifies areas where development will be prohibited in order to protect natural heritage and hydrologic features and their ecological functions. The Greenlands System also identifies the existing parks and open spaces;
 - iii) **The Built-Up Area** – The Built-Up Area includes lands already developed with urban land uses. The Built-Up Area includes those areas within the Settlement Area Boundary where the intensification target is to be measured. The intensification target for Port Dover is and,
 - iv) **The Development Areas** – Development Areas include those vacant areas within the Settlement Area Boundary that are outside of the Built-Up Area, and include future development opportunities for Port Dover. The Development Areas are further subdivided into Development Areas 1 through 5 on **Schedule A**.
- d) Growth in Port Dover will occur through a combination of intensification within the Built-Up Area, and development within the Development Areas. These two areas have different policy frameworks to guide their planning, approval and development:
 - i) **Intensification within the Built-Up Area - Schedule A** identifies the Built Boundary and the Built-Up Area of Port Dover. This Plan requires that a minimum of 25% of all new residential development within Port Dover shall occur within the Built-Up Area on an annual basis; and,

- ii) ***Growth within the Development Areas - Schedule A*** identifies the Development Areas of Port Dover. The Development Areas are expected to accommodate significant growth as they develop as primarily new neighbourhoods. Residential development in the new neighbourhoods shall achieve a minimum density target of 15 units per gross developable hectare, where gross developable hectare means the total land area, less any defined lands within the Hazard Land, Provincially Significant Wetland, and/or the Significant Woodlands Designations.
- e) All new residential development within Port Dover shall be subject to Section 5.3 of the Norfolk County Official Plan, including the requirement that all new housing shall contribute to the achievement of a minimum of 25% of all new housing to be affordable and accessible to low and moderate income households, of which at least 10% of all new units are affordable to low income households.
- f) The County shall monitor growth in Port Dover to ensure stated housing objectives are being met, and that growth is occurring in a logical and cost effective manner.
- g) Sites identified as a Gateway on **Schedule A** are intended to become identifiable entrances to Port Dover. Gateways abut, or are within the road's right-of-way associated with these key intersections, and it is expected that all corner sites associated with an identified Gateway will include:
 - i) Buildings and/or structures that reinforce the importance of the gateway location. This includes the use of high quality building materials, windows and entrances facing the streets and unified and consistent architectural detailing; and/or,
 - ii) Special landscape treatments, appropriate signage, and high quality streetscaping.

To facilitate the construction of an identified Gateway, partnerships among the County, developers and/or service clubs shall be explored. Further, the County may establish other Gateway locations throughout Port Dover to identify key Districts or features, including Gateways related to the Active Transportation System. No Amendment to this Plan is required for the County to identify and establish Gateways that are not conceptually identified on **Schedule A**.
- h) **Schedule A** identifies an Industrial Influence Area, and those lands are subject to Section 6.7.2 of the Norfolk County Official Plan.

4.3 Phasing Policies

- a) Fundamental to Port Dover's long-term ability to accommodate projected growth, is the provision of appropriate municipal infrastructure, including sewage treatment facilities and an adequate water supply. Norfolk County will continue to monitor available sewage treatment and water supply capacity to ensure that adequate facilities are planned and built in concert with expected development activity.
- b) All development approvals within Port Dover shall be explicitly linked to the ability of the County to provide municipal sewage treatment and water supply infrastructure, based on the capacity of both the wastewater treatment plant and the water filtration plant to accommodate growth, as well as commitments to the timing and funding of any required road, active transportation facility, public service facility, park dedication and/or storm water management facility.

These works, facilities and lands shall be provided for in subdivision and site plan agreements and development approvals shall be conditional upon the timing and funding of such facilities, works and/or lands, to the satisfaction of the County.

- c) The Town shall allocate capacity to individual development proposals on the basis of the following criteria:
 - i) Conformity with all of the relevant policies of this Plan, including, where applicable:
 - » Achievement of the required housing mix and Greenfield Density Target;
 - » Contribution to the Intensification Target; and,
 - » Contribution toward the Attainable and Affordable Housing Targets;
 - ii) Consistency with the guidelines of the Urban Design Manual, including the Green Building Technology approaches identified therein;
 - iii) The mix of land uses and the ability to accommodate, or support defined community needs, including Public Service Facilities; and,
 - iv) The status of individual development applications, with respect to:
 - » The proximity of the proposed development to existing and planned municipal water and wastewater systems;

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- » The need for, or the triggering of, capital works projects for existing and planned municipal water and wastewater system upgrades required to accommodate the proposed development; and,
- » The need for, and the timing involved, to undertake and obtain any required Environmental Assessment Approvals for municipal water and wastewater system upgrades required to accommodate the proposed development.

5.0 LAND USE POLICIES

5.1 Land Uses Permitted in All Designations

5.1.1 Public Uses and Utilities

- a) Public uses, and public and private utilities, including telecommunication facilities, are permitted in all land use designations in this Plan, subject to any regulatory requirements, such as the provisions of the Environmental Assessment Act., and in conformity with Section 8.7 of the Norfolk County Official Plan.

5.1.2 Electricity Generation, Transmission and Distribution Systems

- a) Electricity generation facilities, transmission and distribution systems are permitted in all land use designations. Renewable energy systems are subject to Provincial legislation and regulations.

5.1.3 Accessory Uses, Buildings and Structures

- a) Any use, building or structure which is normally incidental and subordinate to a use permitted by this Plan shall be permitted on the same lot as the permitted use, subject to the regulations of the Implementing Zoning By-law, and provided adequate water supply and waste disposal facilities are available.

5.2 Land Uses Prohibited in All Designations

- a) The following uses are prohibited in all land use designations in this Plan:
 - i) Uses that are noxious, polluting, or produce or store hazardous substances;
 - ii) Uses that involve the recycling and/or the storage of contaminated materials; and,
 - iii) Uses that are prohibited pursuant to the provisions of the Environmental Protection Act.
- b) The Implementing Zoning By-law shall incorporate provisions setting out those uses which are prohibited in all zone categories.

- c) Any proposed sites, or expansions to existing waste disposal sites, shall not be permitted in any land use designation.
- d) Recreational vehicle park development, other than in approved, site specific Official Plan Amendments or Zoning By-laws existing as of the date of adoption of this Plan, or in existing recreational vehicle parks, shall be prohibited in all land use designations of this Plan.

5.3 Land Use Specific Policies

5.3.1 Low Density Residential Uses

- a) Low density residential uses shall include single detached, semi-detached and townhouse dwelling units and shall not exceed a density of 40 units per net residential hectare.
- b) Additional Residential Units shall also be permitted in low density residential uses, subject to the policies of Section 5.3.3 of the Norfolk County Official Plan.

5.3.2 Medium Density Residential Uses

- a) Medium density residential uses shall include all forms of residential buildings containing three or more dwelling units such as tri-plex and four-plex buildings, live/work buildings, street, block and stacked townhouses, and low-rise apartments. Medium density residential development shall generally shall be between 40 and 80 units per net residential hectare.
- b) Additional Residential Units shall also be permitted in street townhouse units, subject to the policies of Section 5.3.3 of the Norfolk County Official Plan.
- c) New medium density residential development may be permitted through an Implementing Zoning By-law, subject to the following conditions:
 - i) The site is within a designation that permits medium density residential development;
 - ii) The proposed development is compatible, and can be sensitively integrated with the surrounding land uses. Special measures, such as angular planes, increased building setbacks, or enhanced landscaped buffer strips may be required in order to ensure sensitive integration;
 - iii) The site is adequate in size and configuration to accommodate on-site parking facilities and amenities;
 - iv) The transportation, utilities and service infrastructure can adequately serve the proposed development; and,

- v) The community and neighbourhood amenities such as parks, open space, recreational facilities and institutional services, can adequately serve the proposed development.

5.3.3 High Density Residential Uses

- a) High density residential uses shall include apartment buildings and all forms of multiple residential dwelling unit buildings having a density between 60 and 120 units per net residential hectare.
- b) New high density residential development may be permitted through an Implementing Zoning By-law, subject to the following conditions:
 - i) The site is within a designation that permits high density residential development;
 - ii) The proposed development is compatible, and can be sensitively integrated with the surrounding land uses. Special measures, such as increased building setbacks, angular planes, or landscaped buffer strips may be required in order to ensure sensitive integration;
 - iii) The site is adequate in size and configuration to accommodate on-site parking facilities and amenities;
 - iv) The transportation, utilities and service infrastructure can adequately serve the proposed development; and,
 - v) The community and neighbourhood amenities such as parks, open space, recreational facilities and institutional services, can adequately serve the proposed development.

5.3.4 Special Needs Housing

- a) Special Needs Housing includes all forms of communal housing: Group Homes, Lodging Houses, Halfway Houses, Homes for Special Care and senior care facilities. Special Needs Housing includes all other types of residences licensed or funded under a federal or provincial statute for the accommodation of persons living under supervision in a single housekeeping unit and who, by reason of their age, emotional, mental, social or physical condition, require a group living arrangement for their well-being.
- b) Special Needs Housing shall be permitted in accordance with Section 5.3.3 of the Norfolk County Official Plan.

5.3.5 Home-Based Businesses

- a) There are two forms of home-based businesses:
 - i) Home occupations are small scale operations conducted by persons in their own home and tend to be professional or personal services; and,
 - ii) Home industries are typically carried out by the occupant of a property within accessory buildings on the property and may include artisan studios, maker spaces, small scale manufacturing, processing or repair uses with limited retail operations.
- b) Home occupations and artisan studios may be permitted in residences in accordance with the following provisions:
 - i) The use is carried out entirely within the dwelling unit, and can be appropriately accommodated within a residential structure;
 - ii) The use is clearly secondary to the primary use of the property as a residence in terms of floor space utilization;
 - iii) The property is the principal residence of the person carrying on the home occupation use;
 - iv) Outside storage of goods, materials, equipment or service vehicles such as trailers and commercially licensed vehicles related to the home occupation use shall not be permitted;
 - v) The activities associated with the home occupation use, including traffic generated and hours of operation, do not adversely affect the surrounding area;
 - vi) Adequate water supply and sewage disposal facilities are available and the requirements of the Ontario Building Code are satisfied;
 - vii) Solid waste beyond the volume normally generated by a household as defined by regional and provincial data is not permitted;
 - viii) The retail sales of any goods or wares shall not be permitted as a primary home occupation use; and,
 - ix) Compliance with on-site parking requirements and other provisions regulating home occupations in the Zoning Bylaw.
- c) The Implementing Zoning By-law may include additional provisions regulating Home Occupations.
- d) Home industries may be permitted in accordance with the following provisions:

- i) The use is carried out within an accessory building or structure separate from a residential dwelling;
 - ii) The use is clearly secondary to the primary use of the property and shall not detract from the primary use of the property;
 - iii) Only accessory retail sales of products directly produced by the home industry is permitted;
 - iv) The repair, storage or sale of motor vehicles is not considered to be a home industry;
 - v) The use must be compatible with adjacent uses; and,
 - vi) The use must be in compliance with any zoning and/or licensing provisions.
- e) The Implementing Zoning By-law may include additional provisions regulating home industries.

5.3.6 Live-Work Units

- a) Live-work units have the potential to integrate small-scale service commercial, retail or office uses at-grade. Live-work units are typically in a street townhouse residential building and are subject to the associated development policies identified in this Plan. In addition, live-work units shall provide:
- i) Amenity areas and buffering with planting and/or fencing from adjacent residential dwellings; and,
 - ii) Adequate parking and drop-off/pick-up facilities.

5.3.7 Education Facilities

- a) The County will work with the Boards of Education to ensure the reservation of an adequate number and distribution of school sites and related community facilities throughout Port Dover to accommodate the needs of residents. These sites and facilities shall be planned and developed in accordance with the respective policies, practices and guidelines of the School Boards.
- b) The County shall require the location of school sites to be adjacent to parks or other recreation facilities to allow for shared use of facilities and shall work with the Boards of Education to allow public use of school facilities, under appropriate agreements. The County shall also encourage the development of shared school buildings where feasible and when the Boards of Education's partnership criteria and policies can be met, to maximize the use of land and financial resources.

- c) The selection of school sites shall also consider safe connectivity between the school site and adjacent community, and the availability of community infrastructure that supports active transportation within the school catchment area.
- d) Education facilities may be permitted in any designation identified in this Plan, with the exception of Hazard Land, Provincially Significant Wetland and Significant Woodlands Designations through a Site Specific Zoning By-law Amendment. The Implementing Zoning By-law may include additional provisions regulating education facilities.

5.3.8 Community Facilities

- a) Community facilities include facilities designed to meet the recreational, social, self-directed learning and cultural needs of the residents including public libraries, museums, cultural centres or other similar uses, excluding educational facilities. The County will work with community organizations to ensure that provision is made for such facilities in appropriate locations to serve the residents' needs.
- b) In determining appropriate locations for community facilities, the County shall have regard for the type of service provided by the facility, recognizing that some uses will serve a localized population, while others will serve the whole or large portions of the County, as well as tourists and other visitors.
- c) Where appropriate, community facilities will be located in community hubs to promote cost-effectiveness and facilitate service integration, access to transit and active transportation.
- d) Community facilities may be permitted in any designation identified in this Plan, with the exception of Hazard Land, Provincially Significant Wetland and Significant Woodlands Designations through a Site Specific Zoning By-law Amendment. The Implementing Zoning By-law may include additional provisions regulating community facilities.

5.3.9 Emergency Services

- a) The County shall ensure the efficient and effective allocation of emergency services in a planned effort to keep pace with growth in consultation with Fire, Police and Emergency Medical Services.
- b) The County shall consult with the emergency service providers with respect to the establishment of station locations. Such stations shall have convenient access to arterial roads, a close relationship to the intended service area and shall be integrated with the surrounding development, including appropriate architectural design, landscaping and buffering from residential buildings.

- c) Emergency services may be permitted in any designation identified in this Plan, with the exception of Hazard Land, Provincially Significant Wetland and Significant Woodlands Designations through a Site Specific Zoning By-law Amendment. The Implementing Zoning By-law may include additional provisions regulating emergency services.
- d) Site plans and draft plans of subdivision will be reviewed to ensure that they are designed to accommodate fire prevention and timely emergency response.

5.3.10 Bed and Breakfast Establishments

- a) Bed and breakfast establishments are permitted within any single-detached dwelling unit provided the use does not substantially alter the residential character of the property. The Implementing Zoning By-law may contain specific regulations pertaining to parking, signage and other matters associated with a permitted bed and breakfast use.

5.3.11 Day Care Facilities

- a) Day care facilities may be permitted in a number of designations identified in this Plan, subject to specific regulations in the Implementing Zoning By-law and in accordance with the following policies:
 - i) The use will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads;
 - ii) The use is intended to serve and support the surrounding residential area; and,
 - iii) The site is large enough to accommodate the building, on-site play areas, parking/drop-off facilities and appropriate buffering, where required.

5.3.12 Small-Scale Places of Worship

- a) New places of worship with the capacity to accommodate a congregation of less than 200 people shall be permitted in accordance with the policies of this Plan, through an Implementing Zoning By-law, subject to the following criteria:
 - i) The use will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads; and,
 - ii) The site is large enough to accommodate the building, on-site parking areas and appropriate buffering, where required.
 - iii) The use will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads;

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- iv) The site is large enough to accommodate the building, on-site parking and appropriate amenity areas and buffering, where required; and,
 - v) Direct access shall be provided to places of worship from all parts of the surrounding community through a comprehensive active transportation network.
- b) Small scale places of worship may be permitted in any designation identified in this Plan, with the exception of Hazard Land, Provincially Significant Wetland and Significant Woodlands Designations through a Site Specific Zoning By-law Amendment. The Implementing Zoning By-law may include additional provisions regulating small scale places of worship.

6.0 LAND USE DESIGNATIONS

- a) The land use designations that apply within Port Dover are identified on **Schedule B: Land Use Plan**, and include:
- i) Port Dover Downtown Designation;
 - ii) Port Dover Urban Waterfront District Designation;
 - iii) Existing Neighbourhood Designation;
 - iv) Future Neighbourhood Designation;
 - v) Mixed use Designation;
 - vi) Commercial Designation;
 - vii) Major Institutional Designation;
 - viii) Parks and Open Space Designation;
 - ix) Industrial Designation;
 - x) Major Public Infrastructure Designation;
 - xi) Hazard Land Designation;
 - xii) Provincially Significant Wetland Designation; and,
 - xiii) Significant Woodlands Designation.

6.1 Port Dover Downtown Designation

6.1.1 Intent

- a) Downtown Port Dover is the historic heart of the community and serves as a primary gathering place and location for events and festivities that give identity to the community. Downtown Port Dover has a mix of commercial, institutional and residential uses, and is anchored by Main Street. A grid pattern of streets create small blocks, making it easy to walk around and connect to the waterfront. Powell Park is a major focal point. Buildings in Downtown Port Dover exhibit a variety of materials, with brick and siding most commonly used. There are a number of buildings with a heritage designation or potential concentrated Downtown.

- b) The Port Dover Downtown Designation is intended to be an area that serves as the primary activity centre for Port Dover, and is an appropriate location for a wide range of uses, including retail, service commercial uses and restaurants, recreational, entertainment, business and professional, governmental, institutional, arts and cultural, community, employment and residential uses. Downtown Port Dover will be the focus of compact development oriented to the pedestrian scale.

6.1.2 Permitted Uses

- a) The diversity of land uses permitted within the Port Dover Downtown Designation shall foster the health and continued growth of the Downtown, while conserving and enhancing cultural heritage resources and community identity. Downtown Port Dover will be a focus of tourism and business activities, and will continue to grow and develop with the following permitted uses:
 - i) Retail and service commercial uses;
 - ii) Restaurants;
 - iii) Farmers' markets;
 - iv) Offices,
 - v) Accommodations for tourists and the travelling public including: hotels and motels, inns, bed and breakfast establishments, guest houses, lodge-style facilities, resort-oriented condominiums, or other similar forms of accommodation;
 - vi) Convention centres;
 - vii) Private clubs;
 - viii) Recreational and entertainment uses;
 - ix) Arts and cultural facilities;
 - x) Residential Apartments, including special needs housing;
 - xi) Small scale places of worship;
 - xii) Community facilities;
 - xiii) Institutional uses;
 - xiv) Emergency Services;
 - xv) Day Care Facilities;
 - xvi) Parking facilities at grade, or in structure; and,
 - xvii) Parks, open spaces and walkways/trails.

- b) In addition to the identified list of permitted uses, the following may also be permitted within the Port Dover Downtown Designation:
 - i) Uses accessory to any of the identified permitted uses; and,
 - ii) Pop-up uses and activities. The County may establish policies and procedures to facilitate the establishment of pop-up uses and activities.
- c) The County reserves the right, through the Implementing Zoning By-law, to further refine the list of permitted uses to ensure that new development is appropriate in the context of the adjacent and surrounding community.
- d) New Drive-through commercial facilities, as well as single detached, semi-detached and Townhouse dwellings are specifically not permitted within the Port Dover Downtown Designation.

6.1.3 Development Policies

- a) The County shall encourage the development, redevelopment and rehabilitation within the Port Dover Downtown Designation. Further, the County will encourage developments that increase the amount and intensity of residential apartments within the Port Dover Downtown Designation by supporting appropriate residential development and redevelopment, in accordance with the policies of this Plan and the associated Urban Design Guidelines attached to this Plan as **Appendix I**.
- b) All development within the Port Dover Downtown Designation shall be compatible with existing development in proximity. Where appropriate, historic streetscape patterns such as block lengths, building heights, setbacks and separations will be maintained.
- c) The County will encourage the establishment of a wide range of arts and cultural facilities and activities in the Port Dover Downtown Designation through:
 - i) The placement of public art and murals; and,
 - ii) The placement of wayfinding signage to direct residents and visitors to shops, services and attractions.

- d) Certain lands within the Port Dover Downtown Designation have been identified by the Long Point Region Conservation Authority as being Hazard Land. Consequently, any use of such land will be subject to the policies of Section 7.3 of the Norfolk County Official Plan. All development applications that are within the Hazard Land area within the Port Dover Urban Waterfront Designation shall be evaluated in consultation with the Conservation Authority.
- e) Linkages between the Port Dover Downtown Designation and the Active Transportation Network identified on **Schedule D** shall be created and enhanced to foster pedestrian activity and encourage tourism within the Port Dover Downtown Designation.
- f) Parks and open spaces shall be distributed throughout the Port Dover Downtown Designation, and these facilities shall be linked together and connected with the broader County network of natural and open space areas. All developments within the Port Dover Downtown Designation shall be required to provide public parkland, or cash-in-lieu of parkland. Public parkland shall be provided subject to the following:
 - i) Public open space to be dedicated shall be landscaped prior to conveyance in a manner satisfactory to the County; and/or,
 - ii) Where cash-in-lieu of parkland is accepted for development within the Port Dover Downtown Designation, the County shall allocate the funds generated for public parkland improvements within the Port Dover Downtown Designation. The funds generated shall be used to provide additional public parkland or for the aesthetic and/or functional improvement of existing public parkland areas.
- g) Where practical, the municipality may provide or cooperate with private landowners and other public agencies to provide additional open space facilities within Port Dover Downtown Designation. The development of open space facilities that can be the focus of community festivals, events and activities attractive to all segments of the community will be encouraged.
- h) Permitted retail uses shall be limited in scale to a maximum of 3,000 square metres of Gross Floor Area per individual retail use.
- i) The height, massing and layout of buildings within the Downtown Designation shall be oriented to a pedestrian scale. The maximum building height within the Port Dover Downtown Designation shall be 6 storeys, or 20 metres, whichever is less, and may be subject to the following provisions that will be more fully articulated within the Implementing Zoning By-law:

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- i) All development within the Port Dover Downtown Designation shall have a minimum height of 2 storeys; and,
- ii) All development within the Port Dover Downtown Designation shall incorporate a floor to ceiling height of the first floor of 4.5 metres.

Building heights greater than 6 storeys may be considered within the Port Dover Downtown Designation only through a Site Specific Amendment to this Plan and a Site Specific Zoning By-law Amendment, where appropriate mechanisms are employed to ensure compatibility and an appropriate transition to abutting properties. Mechanisms employed through the Site Specific planning policies and regulations may include a reduction in the permitted building height, the implementation of enhanced building setbacks, the requirement for landscape planting strips, the imposition of a step back and/or the imposition of an angular plane.

- j) All new development with frontage along Main Street, St. George Street or St. Andrew Street within the Port Dover Downtown Designation shall have at least 50% of its ground floor Gross Floor Area dedicated to non-residential uses, with a preference for retail and service commercial uses and restaurants. Residential uses shall only be permitted above the ground floor and/or on the ground floor in the rear of the building.
- k) Where residential uses are proposed in an existing building of commercial character, they shall only be permitted above the ground floor and on the ground floor in the rear of the building, provided that the street frontage is maintained for commercial uses. For the purposes of this policy, the commercial character of a building may be determined in consultation with the Chief Building Official of the County.
- l) In an existing building of residential character, residential uses and/or commercial uses shall be permitted, provided the residential character of the building is maintained. For the purposes of this policy, the residential character of a building may be determined in consultation with the Chief Building Official of the County.
- m) In the consideration of any Zoning By-law Amendment proposing to establish any of the permitted uses, the following shall be addressed:
 - i) Adequate and appropriate access to the property from a public road; and,
 - ii) The provision of adequate and appropriate parking and loading facilities.

- n) It is recognized that in the Port Dover Downtown Designation, it may not be feasible or desirable for all new developments to provide for parking on site. As such, the provision of parking spaces shall be subject to the following policies:
- i) Private and public parking lots will be encouraged at locations convenient to Main Street in Port Dover;
 - ii) On-street parking is important to the economic vitality of the Port Dover Downtown and will be maintained to the extent practical. The temporary use of on-street parking spaces for pop-up uses or outdoor cafes or eating areas may be permitted subject to the policies and procedures that may be established by Council;
 - iii) In reviewing development applications within the Port Dover Downtown, the County will recognize the importance of the Downtown mixed use context and will promote a comprehensive parking strategy that considers reduced parking standards for urban mixed use developments, or other classes of development, based on an understanding of opportunities for shared parking, on-street parking and the availability of public parking facilities;
 - iv) The County may accept cash-in-lieu of parking as an alternative to providing any required parking. Where cash-in-lieu of parking is accepted, the funds generated shall be used to provide additional public parking or the aesthetic and/or functional improvement of existing public parking areas within the Port Dover Downtown Designation; and,
 - v) Where a development cannot provide off-street parking on its site, the County may permit the provision of the required parking spaces on an alternative site, provided that the alternative site is within convenient walking distance of the proposed development, and the developer enters into an agreement with the municipality to ensure the continued availability of the alternative site as a parking area.
- o) The site plan control policies in accordance with the policies of Section 9.6.5 (Site Plan Control) of the Norfolk County Official Plan shall apply to all development applications in the Port Dover Downtown Designation.

- p) The County may undertake, by both direct municipal action and by encouraging the actions of other parties, a program of on-going improvements within the Port Dover Downtown Designation, including streetscape improvements, facilities for off-street and on-street parking, improved vehicular connections and circulation patterns, and facilities for those not traveling by private automobile, including improvements for pedestrians, cyclists, the elderly and physically-challenged, and those using taxis, transportation for the disabled and transit services.
- q) The County encourages the establishment of a Port Dover Downtown Business Improvement Area, in accordance with the Municipal Act, corresponding to the boundary of the Port Dover Downtown Designation. The County may promote a larger Business Improvement Area that combines the areas identified as within the Port Dover Downtown Designation and the Port Dover Urban Waterfront Designation.
- r) The County shall pursue the ongoing enhancement of the Port Dover Downtown in co-operation with the boards of management for the Business Improvement Areas (BIA) and other public and private interests and community groups, to enhance the efficiency, convenience, safety and appearance of the areas and the activities they accommodate. As such, the County may pass by-laws designating and delineating a Community Improvement Project Area associated with the Port Dover Downtown Designation, and may undertake Community Improvement Plans, in accordance with Section 9.5.2 (Community Improvement) of the Norfolk County Official Plan to improve public infrastructure and to stimulate private sector investment.

6.1.4 Urban Design Policies

- a) **Interpretation** - These Urban Design Policies are intended to guide new development within the Port Dover Downtown Designation in combination with the other relevant policies of this Plan and the Urban Design Guidelines attached to this Plan as **Appendix I**. It is recognized that as policies within the statutory part of this Plan, all new development within the Port Dover Downtown Designation shall conform to these Urban Design Policies. However, it is the intent of the County that progressive and innovative development is desirable within Port Dover, and the test of conformity with these Urban Design Policies may be interpreted with some flexibility, so long as the intent of these Policies is achieved, to the satisfaction of the County.

- b) **Context** - The core of the Port Dover Downtown Designation is focused along Main Street where, generally, buildings are located close to the street line and to one another, resulting in a well-defined streetwall. Commercial buildings are typically 2-storeys in height, with flat roofs. Many buildings have an historic character that create a distinct sense of place. At each end of Main Street, buildings are more widely spaced, typically one storey in height, with varying setbacks. On-street parking is replaced by vehicular lanes and parking is provided in large surface lots. These areas are more suburban in character and oriented to vehicular access. It is the objective of these Urban Design Policies to shape a more pedestrian-oriented and well-defined Main Street throughout the Port Dover Downtown Designation.

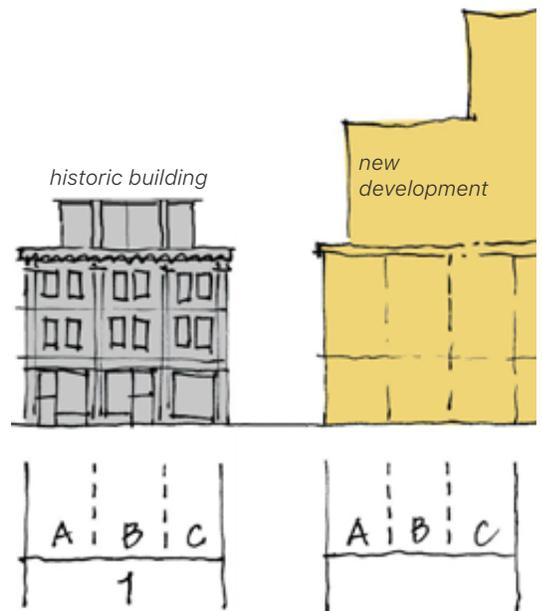
The streets around Main Street are more residential in character, although they also have commercial uses mixed in. Buildings have set backs from each other and from their neighbours while still defining the street edge. There is a mix of historic, traditional and more modern architectural styles. Parking is often accommodated in side driveways or garages set back from the street edge. Large mature trees make a significant contribution to the attractiveness of this area.

- c) **Cultural Heritage** - The County shall protect and enhance, where possible and appropriate, the historic architectural character found within the Port Dover Downtown Designation, particularly focusing on the pedestrian scale and design of the buildings and streetscapes, and the arrangement of windows and treatments on the building facades. Existing listed and designated cultural heritage resources within the Port Dover Downtown Designation are identified in **Appendix II**. Heritage-related design policies for new development within the Port Dover Downtown Designation include:

- i) The historic pattern of development in Downtown Port Dover has been characterized by ground floor commercial uses with narrow street frontages, frequent pedestrian entrances and windows that create pedestrian appeal. The design of new buildings and the redevelopment or adaptive re-use of existing buildings shall enhance the quality of the pedestrian environment by including transparent frontages, the articulation of facades and the use of quality materials at the street level;
- ii) New buildings shall relate to the traditional building stock, either through a traditional style that reflects the character of the area, or a more contemporary style that sets them apart from and highlights heritage buildings through a defined contrast and juxtaposition. Designs shall provide a clear distinction between 'new' and 'old'; and,



New designs shall relate to the traditional building stock



Facade designs shall reference the articulation of neighbouring historic buildings

- iii) Facade designs shall reference the articulation of neighbouring historic buildings, with respect to vertical and horizontal elements, including the rhythm and proportion of its main architectural elements.
- d) **General Design Policies** - The following policies apply everywhere within the Port Dover Downtown Designation:
 - i) A minimum 2-storey street wall shall be maintained facing public streets along all streets;
 - ii) Buildings shall be oriented to frame the street edge and to create a strong street wall;
 - iii) Buildings shall front directly onto public streets and other public spaces, in order to clearly define the public realm and create an attractive and safe pedestrian environment;
 - iv) Buildings at corner locations shall be sited to address the intersection, with consideration given to both street frontages;
 - v) Buildings shall align with the existing street wall. In residential areas, buildings shall generally be aligned with the setbacks of their neighbours;
 - vi) Main building entrances shall face the street and shall be clearly defined with architectural details and easily identifiable within the facade composition. Publicly accessible front entrances for stores, offices, and institutional uses shall permit barrier-free access and universal accessibility, including both visual and physical accessibility;
 - vii) Ground floor facades shall be highly transparent, including transparent windows and entrance doors, to establish a strong visual connection between the street and the interior of active ground floor uses; and,
 - viii) Facade rhythm may be established along a street wall through architectural articulation, including the use of fenestration, bands, columns, and other repeated elements.
- e) **Additional Main Street Design Policies** - The historic buildings along Main Street exhibit many urban design principles that these policies seek to reinforce. These principles are not about style, they are about the relationship of the building to the street. Additional design policies for development along Main Street within the Port Dover Downtown Designation are as follows:
 - i) Along the Main Street frontage, buildings shall provide a step-back of 2.0 metres above the street wall height - typically above the 2nd Storey;

- ii) Buildings shall be sited adjacent to the sidewalk and in line with the established street wall. They shall occupy the full width of their frontage unless there is a provision for public space or access. Building walls at the interior side lot lines will generally be blank, without windows or doors, in anticipation of future development that is also built to the same interior side yard;
- iii) Buildings fronting onto Main Street shall have active uses at grade, such as commercial, retail, office or institutional uses. A rhythm of fine-grain and narrow shop frontages shall be established;
- iv) Parking shall not be visible from Main Street. If present, it shall be located behind the building or in structure;
- v) Vehicular access to sites is discouraged along Main Street. Preferred access locations are from adjacent streets and shared laneways and driveways;
- vi) Where site access is unavoidably located along Main Street, minimize its width. Consider single lanes, one way lanes, and shared spaces that incorporate vehicular and pedestrian access together;
- vii) Sidewalks along Main Street shall be wider, with a minimum 2.0 metre unobstructed pedestrian clearway. In addition to the 2 m clearway, a landscape/ furnishing zone shall be provided adjacent to the curb for lighting, signs, seating and where feasible, planting. In addition to the clearway, additional space on the sidewalk adjacent to the building may be used as a Market Zone for outdoor patios and retail display; and,
- viii) For locations on Main Street that display a more suburban context, where more generous setbacks are provided, the space shall be used for landscaped areas, additional street tree planting, amenity areas, seating, display areas or sidewalk cafes and patios. Parking spaces shall not be located in the front yard setback space, or within the exterior side yard space on corner lots.

f) **Additional Design Policies for Residential Infill** - The following guidelines anticipate the potential for modest residential infill within the historic small lot fabric of the original survey grid within the Port Dover Downtown Designation:

- i) Avoid parking, driveways and garages along street frontages; these shall be located away from public view and preferably internal to the site, screened by buildings;



2-storey street wall

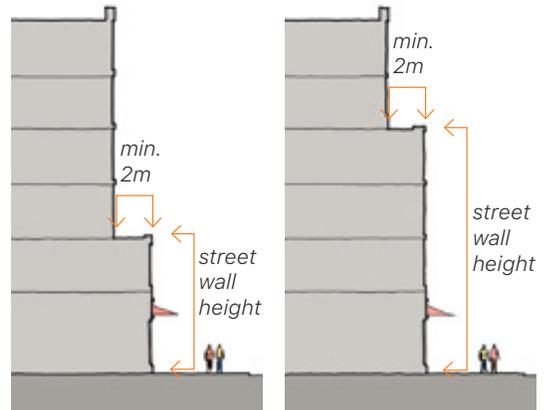


Buildings front directly onto streets and public spaces



Facade rhythm through architectural articulation

- ii) Buildings shall be placed to create a continuous street wall; the setback distance shall be determined with reference to the setback distances of adjacent buildings and provide sufficient transition from private front yards to the public street;
- iii) The massing and scale of the development shall be compatible with the existing and planned context;
- iv) Main entries shall be located on the front of the building and shall be highlighted in the architectural design;
- v) Ensure that end units display the same level of architectural detail and articulation as that of the main front elevation;
- vi) The interface between the front yard and the sidewalk shall be designed with a combination of low fencing, stone walls and/ or hedges and shrubs that enhance the character of the streetscape; and,
- vii) Detailed landscape treatments shall be coordinated with the main building materials and create a year round visually appealing presence along the street.



2 metre step-back above the street wall height

g) **Design Policies for Parking and Access** - Guidelines for parking and access within the Port Dover Downtown Designation generally relate to the design and functionality of on- and off street parking and access driveways. The objective is to provide suitable parking and driveway options while reducing traffic impacts and the negative visual impact of surface parking lots. The following design policies apply:

- i) Parking facilities shall be designed and located to minimize its impact on the streetscape and public realm to ensure that sidewalks and building facades define the street edge. As such:
 - » Parking will not be permitted between the right of way and the building face or within front yard setback areas;
 - » Side yard parking may be considered where site constraints are significant, but is generally discouraged;
 - » Rear yard parking, or encouraging patrons to park in designated parking lots, is preferred;
- ii) Parking lots shall be organized to minimize the number of potential pedestrian-vehicle movement conflicts. Where possible, parking areas shall be coordinated between multiple properties to maximize connectivity, improve traffic flow and increase the efficiency of parking;

- iii) Parking lots shall be designed to enhance the feeling of safety through techniques such as maintaining clear sight lines for natural surveillance of the parking lot as well as minimizing dark, hidden or obscured areas. Parking lot design shall consider:
 - » Planting strips and landscaped traffic islands, medians, or bump-outs shall also be provided within lots to break up the expanse of hard surface;
 - » Pedestrian-scaled lighting, walkways, landscaping, and signage to enhance pedestrian safety, movement and comfort through parking lots to the adjacent sidewalks;
 - » Pedestrian routes through parking areas shall be wide enough to accommodate comfortable travel and their walking surfaces shall be clearly differentiated from the parking areas with texture, material, colour changes or markings;
- iv) Where existing parking areas are adjacent to the sidewalk, a landscaped area shall be located between parking spaces and the sidewalk. Landscaping shall provide clear sight lines at eye level.
- v) Bicycle parking and racks shall be provided in locations that are close to building entrances but situated to avoid any conflicts for movement along pedestrian routes.
- vi) Where possible, access to parking areas shall be provided from side streets and laneways. Access to parking areas shall be defined through clearly designated entrances and exits.



Landscaped medians and islands



Pedestrian-scaled walkways and landscaping enhance safety, movement and comfort

6.1.5 Bucks Orchard Beach Park Site Specific Policy Area

- a) On lands that are identified as Site Specific Policy 6.1.5 on **Schedule B**, in addition to the uses permitted in the Port Dover Downtown Designation, residential uses in accordance with the policies of the Existing Neighbourhood Designation of this Plan shall also be permitted. In the consideration of a Zoning By-law amendment to implement the policies of this Site Specific Policy Area, the following shall be addressed:
 - i) The compatibility of the proposed use with adjacent uses;

- ii) The need for special building design incorporating height limitations, setbacks, buffering and landscaping to enhance compatibility with adjacent uses;
 - iii) Adequate access to the property particularly regarding emergency vehicles; and,
 - iv) The provision of adequate parking.
- b) Site plan control shall be utilized for new development or redevelopment projects in order to address compatibility and to ensure that such development or redevelopment creates a highly aesthetic and unique area.

6.2 Port Dover Urban Waterfront District Designation

6.2.1 Intent

- a) The Port Dover Urban Waterfront District Designation is an important tourism and economic resource. Its historic role as a tourist destination and a commercial fishing port remains important to the success of Port Dover. It is the intent of this Plan that the Port Dover Urban Waterfront District remains a unique area that supports tourism, the commercial fishery and provides recreational, commercial and cultural opportunities for residents and tourists. To achieve this, the policies of the Port Dover Urban Waterfront District Designation shall promote:
- i) Public visual and physical access to the waterfront shorelines, and to, from and within the District through the possible establishment of a boardwalk and/or waterfront promenades;
 - ii) Attractive and diverse public and private sector development that is compatible with the character and charm of the existing community;
 - iii) The recognition of the defined Hazard Lands and the protection and enhancement of the waterfront lands.
 - iv) The protection and promotion of:
 - » The Commercial Fishing Port as an ongoing viable commercial/industrial activity;
 - » The existing residential land uses;
 - » The mixed use and street-oriented commercial district that takes advantage of its locational attributes and its importance as a tourist destination; and,

- » The facilities and services of the Recreational Marina.

6.2.2 Permitted Uses

- a) The Port Dover Urban Waterfront District Designation is intended to accommodate commercial facilities and services in convenient locations to serve the needs of residents and tourists. The Port Dover Urban Waterfront District will be a focus of tourism and business activities, and will continue to grow and develop with the following permitted uses:
 - i) Retail and service commercial uses;
 - ii) Restaurants and snack bars;
 - iii) Accommodations for tourists and the travelling public including: hotels and motels, inns, bed and breakfast establishments, guest houses, lodge-style facilities, resort-oriented condominiums, or other similar forms of accommodation;
 - iv) Convention centres;
 - v) Marinas and related retail, commercial, office, recreation, club house and banquet uses and facilities;
 - vi) Marine industrial uses including boat-works and other marine-related commercial uses;
 - vii) Recreational and entertainment uses;
 - viii) Arts and cultural facilities;
 - ix) Residential uses, including special needs housing;
 - x) Home-based businesses:
 - xi) Community facilities;
 - xii) Emergency Services;
 - xiii) Day Care Facilities;
 - xiv) Parking facilities at grade, or in structure; and,
 - xv) Parks, open spaces and walkways/trails.
- b) In addition to the identified list of permitted uses, the following may also be permitted within the Port Dover Urban Waterfront District Designation:
 - i) Uses accessory to any of the identified permitted uses; and,
 - ii) Pop-up uses and activities. The County may establish policies and procedures to facilitate the establishment of pop-up uses and activities.

- c) Within the Port Dover Urban Waterfront District Designation the existing Commercial Fishing Port is recognized as an ongoing viable commercial/industrial activity. On those lands identified as the Marine Industrial Priority Area, marine industrial uses shall be the primary use of the property. The extent of the Commercial Fishing Port and the regulations that manage its ongoing function shall be detailed within the Implementing Zoning By-law.
- d) The County reserves the right, through the implementing Zoning By-law, to further refine the list of permitted uses to ensure that new development is appropriate in the context of the adjacent and surrounding community.
- e) New Drive-through commercial facilities are specifically not permitted within the Port Dover Urban Waterfront District Designation.

6.2.3 Development Policies

- a) The County shall encourage the development, redevelopment and rehabilitation within the Port Dover Urban Waterfront District Designation by supporting appropriate mixed use development and redevelopment, in accordance with the policies of this Plan and the associated Urban Design Guidelines attached to this Plan as **Appendix I**.
- b) Stand-alone residential developments are not permitted within the Port Dover Urban Waterfront District Designation. New residential apartments shall only be permitted as part of a mixed use building, with another permitted use located at-grade and the residential use located above grade.
- c) All development within the Port Dover Urban Waterfront District Designation shall be compatible with existing development in proximity. Where appropriate, historic streetscape patterns such as block lengths, building heights, setbacks and separations will be maintained.
- d) The County will encourage the establishment of a wide range of arts and cultural facilities and activities in the Port Dover Urban Waterfront District through:
 - i) The placement of public art and murals; and,
 - ii) The placement of wayfinding signage to direct residents and visitors to shops, services and attractions.

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- e) Certain lands within the Port Dover Urban Waterfront District Designation have been identified by the Long Point Region Conservation Authority as being Hazard Land. Consequently, any use of such land will be subject to the policies of Section 7.3 of the Norfolk County Official Plan. All development applications that are within the Hazard Land area within the Port Dover Urban Waterfront District Designation shall be evaluated in consultation with the Conservation Authority.
- f) The County shall encourage and facilitate partnership arrangements, joint ventures, and facility sharing with the Marina Board of Management, the Harbour Authority, Provincial agencies, school boards, other institutions and community groups to expand the supply of parks, open space and parking facilities within the Port Dover Urban Waterfront District Designation.
- g) All development within the Port Dover Urban Waterfront District Designation shall be compatible with existing development in proximity, in consideration of the following issues:
 - i) Building design, roof lines, density, scale and massing;
 - ii) Setbacks, signage, lighting, and buffering of existing and proposed development; and,
 - iii) The use of landscaping and the provision of benches, planters and other street furniture.
- h) Linkages between the Port Dover Urban Waterfront District Designation and the Active Transportation Network identified on **Schedule D** shall be created and enhanced to foster pedestrian activity and encourage tourism within the Port Dover Urban Waterfront District Designation.
- i) Parks and open spaces shall be distributed throughout the Port Dover Urban Waterfront District Designation, and these facilities shall be linked together and connected with the broader County network of natural and open space areas. All developments within the Port Dover Urban Waterfront district Designation shall be required to provide public parkland, or cash-in-lieu of parkland. Public parkland shall be provided subject to the following:
 - i) Public open space to be dedicated shall be landscaped prior to conveyance in a manner satisfactory to the County; and/or,

- ii) Where cash-in-lieu of parkland is accepted for development within the Port Dover Urban Waterfront District Designation the County shall allocate the funds generated for public parkland improvements within the Port Dover Urban Waterfront District Designation. The funds generated shall be used to provide additional public parkland or for the aesthetic and/or functional improvement of existing public parkland areas.
- j) Where practical, the municipality may provide or cooperate with private landowners and other public agencies to provide additional open space facilities within Port Dover Urban Waterfront District Designation. The development of open space facilities that can be the focus of community festivals, events and activities attractive to all segments of the community will be encouraged.
- k) Retail sales outlets and associated commercial uses shall be limited in size, scale and scope by the Implementing Zoning By-law.
- l) Outdoor storage, including commercial fishing and marina related items, including the on-land storage of boats, as a use accessory to primary marine industrial uses, or to marinas and related marine commercial facilities may be permitted, having regard to compatibility with adjacent uses and potential impact on future redevelopment. The Implementing Zoning By-law and/or Site Plan Agreement may delineate where open storage may be located together with appropriate setbacks, screening and buffering. The Implementing Zoning By-law and/or Site Plan Agreement may also delineate where the boats may be stored, and may limit the number of boats permitted to be stored at any given time.
- m) The height, massing and layout of buildings within the Port Dover Urban Waterfront District Designation shall be oriented to a pedestrian scale. The maximum building height within the Port Dover Urban Waterfront District Designation shall be 4 storeys, or 13.5 metres, whichever is less, and may be subject to the following provisions that will be more fully articulated within the Implementing Zoning By-law:
 - i) All development within the Port Dover Urban Waterfront District Designation shall have a minimum height of 2 storeys; and,
 - ii) All development within the Port Dover Urban Waterfront District Designation shall incorporate a floor to ceiling height of the first floor of 4.5 metres.

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Building heights greater than 4 storeys may be considered within the Port Dover Urban Waterfront District Designation only through a Site Specific Amendment to this Plan and a Site Specific Zoning By-law Amendment, where appropriate mechanisms are employed to ensure compatibility and an appropriate transition to abutting properties. Mechanisms employed through the Site Specific planning policies and regulations may include a reduction in the permitted building height, the implementation of enhanced building setbacks, the requirement for landscape planting strips, the imposition of a step back and/or the imposition of an angular plane.

- n) Where residential uses are proposed in an existing building of commercial character, they shall only be permitted above the ground floor, ensuring that the street frontage is maintained for commercial uses. For the purposes of this policy, the commercial character of a building will be determined in consultation with the Chief Building Official of the County.
- o) In an existing building of residential character, residential uses and/or commercial uses shall be permitted, provided the residential character of the building is maintained. For the purposes of this policy, the residential character of a building will be determined in consultation with the Chief Building Official of the County.
- p) In the consideration of any Zoning By-law Amendment proposing to establish any of the permitted uses, the following shall be addressed:
 - i) Adequate and appropriate access to the property from a public road; and,
 - ii) The provision of adequate and appropriate parking and loading facilities.
- q) The County will recognize the importance of the desired mixed use context and will promote a comprehensive parking strategy within the Port Dover Urban Waterfront District Designation that considers reduced parking standards for urban mixed use developments, or other classes of development, based on an understanding of opportunities for shared parking, on-street parking, and the availability of public parking facilities. In addition:
 - i) The County may accept cash-in-lieu of parking as an alternative to providing any required parking. Where cash-in-lieu of parking is accepted, the funds generated shall be used to provide additional public parking or the aesthetic and/or functional improvement of existing public parking areas within the Port Dover Urban Waterfront District Designation; and,

- ii) Where a development cannot provide off-street parking on its site, the County may permit the provision of the required parking spaces on an alternative site, provided that the alternative site is within convenient walking distance of the proposed development, and the developer enters into an agreement with the municipality to ensure the continued availability of the alternative site as a parking area.
- r) The Site Plan Control policies of Section 9.6.5 of the Norfolk County Official Plan shall apply to all development applications in the Port Dover Urban Waterfront District Designation.
- s) The County may undertake, by both direct municipal action and by encouraging the actions of other parties, a program of on-going improvements within the Port Dover Urban Waterfront District Designation, including streetscape improvements, facilities for off-street and on-street parking, improved vehicular connections and circulation patterns, and facilities for those not traveling by private automobile, including improvements for pedestrians, cyclists, the elderly and physically-challenged, and those using taxis, transportation for the disabled and transit services.
- t) The County encourages the establishment of a Port Dover Urban Waterfront District Business Improvement Area, in accordance with the Municipal Act, corresponding to the boundary of the Port Dover Urban Waterfront District Designation. The County may promote a larger Business Improvement Area that combines the areas identified as within the Port Dover Downtown Designation and the Port Dover Urban Waterfront District Designation.
- u) The County shall pursue the ongoing enhancement of the Port Dover Urban Waterfront District in co-operation with the boards of management for the Business Improvement Areas (BIA) and other public and private interests and community groups, to enhance the efficiency, convenience, safety and appearance of the areas and the activities they accommodate. As such, the County may pass By-laws designating and delineating a Community Improvement Project Area associated with the Port Dover Urban Waterfront District Designation, and may undertake Community Improvement Plans, in accordance with Section 9.5.2 (Community Improvement) of the Norfolk County Official Plan to improve public infrastructure and to stimulate private sector investment.

6.2.4 Urban Design Policies

a) **Interpretation** - These Urban Design Policies are intended to guide new development within the Port Dover Urban Waterfront District Designation in combination with the other relevant policies of this Plan and the Urban Design Guidelines attached to this Plan as **Appendix I**. It is recognized that as policies within the statutory part of this Plan, all new development within the Port Dover Urban Waterfront District Designation shall conform to these Urban Design Policies. However, it is the intent of the County that progressive and innovative development is desirable within Port Dover, and the test of conformity with these Urban Design Policies may be interpreted with some flexibility, so long as the intent of these Policies is achieved, to the satisfaction of the County.

b) **General Design Policies** - The following policies apply everywhere within the Port Dover Urban Waterfront District Designation:

- i) Buildings shall be oriented to frame the street edge and to create a strong street wall;
- ii) Buildings shall front directly onto public streets and other public spaces, in order to clearly define the public realm and create an attractive and safe pedestrian environment;
- iii) Buildings at corner locations shall be sited to address the intersection, with consideration given to both street frontages;
- iv) Buildings shall align with the existing street wall. Buildings shall generally be aligned with the setbacks of their neighbours. For sites with ground floor commercial, consideration shall be given to zero setback;
- v) Main building entrances shall face the street and shall be clearly defined with architectural details and easily identifiable within the facade composition. Publicly accessible front entrances for stores, offices, and institutional uses shall permit barrier-free access and universal accessibility, including both visual and physical accessibility;
- vi) Ground floor facades shall be highly transparent, including transparent windows and entrance doors, to establish a strong visual connection between the street and the interior of active ground floor uses;
- vii) Facade rhythm shall be established along a street wall through architectural articulation, including the use of fenestration, bands, columns, and other repeated elements;



Buildings front directly onto streets and public spaces



Facade rhythm through architectural articulation

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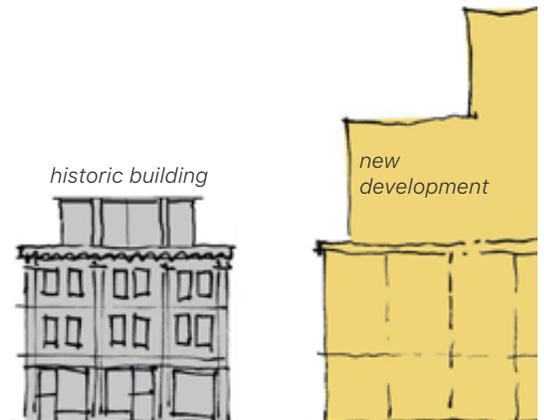
- viii) Streets shall be designed to accommodate all modes of transportation, including walking, cycling, cars and service vehicles; and,
- ix) Streets and sidewalks shall create a pedestrian environment through defined standards for landscaped areas, paving, street trees, and other appropriate street furniture, and shall form a connected system of optional routes to, from and within the area.

c) **Cultural Heritage** - The County shall protect and enhance, where possible and appropriate, the historic architectural character found within the Port Dover Urban Waterfront District Designation, particularly focusing on the pedestrian scale and design of the buildings and streetscapes, and the arrangement of windows and treatments on the building facades. Existing listed and designated cultural heritage resources within the Port Dover Urban Waterfront District Designation are identified in **Appendix II**. Heritage-related design policies for new development within the Port Dover Urban Waterfront Designation include:

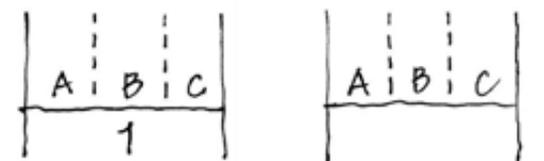


New designs shall relate to the traditional building stock

- i) The historic pattern of development in the Port Dover Urban Waterfront District Designation has been characterized by ground floor commercial uses with narrow street frontages, frequent pedestrian entrances and windows that create pedestrian appeal. The design of new buildings and the redevelopment or adaptive re-use of existing buildings shall enhance the quality of the pedestrian environment by including transparent frontages, the articulation of facades and the use of quality materials at the street level;
- ii) New buildings shall relate to the traditional building stock, either through a traditional style that reflects the character of the area, or a more contemporary style that sets them apart from and highlights heritage buildings through a defined contrast and juxtaposition. Designs shall provide a clear distinction between 'new' and 'old'; and,
- iii) Façade designs shall reference the articulation of neighbouring historic buildings, with respect to vertical and horizontal elements, including the rhythm and proportion of its main architectural elements.



d) **Design Policies for Parking and Access** - Guidelines for parking and access within the Port Dover Urban Waterfront District Designation generally relate to the design and functionality of on- and off street parking and access driveways. The objective is to provide suitable parking and driveway options while reducing traffic impacts and the negative visual impact of surface parking lots. The following design policies apply:



Facade designs shall reference the articulation of neighbouring historic buildings

- i) Parking facilities shall be designed and located to minimize its impact on the streetscape and public realm to ensure that sidewalks and building facades define the street edge. As such:
 - » Parking will not be permitted between the right of way and the building face or within front yard setback areas;
 - » Side yard parking may be considered where site constraints are significant, but is generally discouraged;
 - » Rear yard parking, or encouraging patrons to park in designated parking lots, is preferred;
- ii) Parking lots shall be organized to minimize the number of potential pedestrian-vehicle movement conflicts. Where possible, parking areas shall be coordinated between multiple properties to maximize connectivity, improve traffic flow and increase the efficiency of parking;
- iii) Parking lots shall be designed to enhance the feeling of safety through techniques such as maintaining clear sight lines for natural surveillance of the parking lot as well as minimizing dark, hidden or obscured areas. Parking lot design shall consider:
 - » Planting strips and landscaped traffic islands, medians, or bump-outs shall also be provided within lots to break up the expanse of hard surface;
 - » Pedestrian-scaled lighting, walkways, landscaping, and signage to enhance pedestrian safety, movement and comfort through parking lots to the adjacent sidewalks;
 - » Pedestrian routes through parking areas shall be wide enough to accommodate comfortable travel and their walking surfaces shall be clearly differentiated from the parking areas with texture, material, colour changes or markings;
- iv) Where existing parking areas are adjacent to the sidewalk, a landscaped area shall be located between parking spaces and the sidewalk. Landscaping shall provide clear sight lines at eye level.
- v) Bicycle parking and racks shall be provided in locations that are close to building entrances but situated to avoid any conflicts for movement along pedestrian routes.



Landscaped medians and islands



Pedestrian-scaled walkways and landscaping enhance safety, movement and comfort

- vi) Where possible, access to parking areas shall be provided from side streets and laneways. Access to parking areas shall be defined through clearly designated entrances and exits.

6.2.5 Lynn River/Black Creek Special Policy Area

- a) On lands within the Port Dover Urban Waterfront District Designation identified as Site Specific Policy Area 6.2.5 on **Schedule B** includes land north of Highway No. 6 bordering on both sides of the Lynn River and Black Creek. Traditionally, marine industries have established within this area. This Special Policy Area is to be considered and applied in addition to the other policies of this Section.
- b) The marine industry is still an important component of the local economy and the Lynn River acts as the main artery for marine traffic to this area. With the increase in tourism, which is becoming more important to the economic health of the County, this area has the potential to support a variety of interests relating to tourism, recreation and innovative residential development, while at the same time recognizing that the established marine industries will remain. A large portion of the Lynn River/Black Creek Special Policy Area are considered Hazard Lands due to potential flooding and that the risk of flooding may impact existing and future land uses.
- c) New uses may be established provided they are in suitable locations where land use conflicts can be minimized and adequate access and parking can be provided. The intent of this Plan is to create a continuous link for commercial and tourism related uses between the Downtown Area and the Urban Waterfront District through the Lynn River/Black Creek Area and back to the Downtown Area through a connection along Market Street, Chapman Street and Powell Park. The Lynn River/Black Creek Special Policy Area is in transition and none of the changes in land use are expected to occur immediately. The principal method of redevelopment will be through applications to amend the Implementing Zoning By-law. In the consideration of any such application, effort shall be made to eliminate or minimize potential land use conflicts.
- d) In addition to the land uses permitted in the underlying land use designation, the following uses shall be permitted within the Lynn River/Black Creek Special Policy Area:
 - i) Marine industrial uses;
 - ii) Commercial and recreational uses catering specifically to boaters;
 - iii) Retail commercial uses catering specifically to tourism and marine recreational activities;
 - iv) Commercial accommodation including hotels, motels and bed and breakfast establishments;

- v) Restaurants;
- vi) Convention centres; and,
- vii) Residential uses, within a mixed use building, that exhibit the following characteristics:
 - » Architecture and detailing in built form and urban design that reflects a nautical or marine theme; and
 - » An orientation to the waterfront, taking advantage of the views and vistas.
- e) In the consideration of any application to amend the Implementing Zoning By-law proposing to establish a commercial or residential use, the following shall be addressed:
 - i) The compatibility of the proposed use with adjacent and neighbourhood land uses, particularly existing marine industrial uses;
 - ii) The need for special building design, setbacks, buffering and landscaping to enhance compatibility with adjacent uses;
 - iii) Adequate access to the property particularly for emergency vehicles; and,
 - iv) The provision of adequate parking and loading facilities.
- f) The County shall use site plan control, in accordance with the policies of Section 9.6.5 (Site Plan Control) of the Norfolk County Official Plan, to ensure that new development and redevelopment, is compatible with the intended character and the natural environment, and creates a highly aesthetic and unique mixed use area.

6.2.6 Dover Wharf Site Specific Policy

- a) On lands within the Port Dover Urban Waterfront District Designation identified as Site Specific Policy Area 6.2.6 on **Schedule B**, in addition to the uses permitted, a 48 unit residential development shall be permitted.

6.3 Existing Neighbourhood Designation

6.3.1 Intent

- a) The Existing Neighbourhood Designation in Port Dover is expected to continue to accommodate attractive neighbourhoods which will provide for a variety of residential forms as well as neighbourhood facilities such as elementary schools, parks, places of worship and convenience commercial uses integral to and supportive of a residential environment. A variety of housing types are needed to meet the needs of a diverse population. Opportunities to provide housing for individuals or groups with special needs including the elderly and those with special physical, social or economic needs within the County will be encouraged.
- b) It is the intent of this Plan to enhance the existing neighbourhoods within Port Dover, while managing their ongoing evolution, including opportunities for sensitive intensification. It is also the intent of the County to recognize the existing neighbourhoods of Port Dover as communities that consist of primarily low density residential house forms that have limited potential to accommodate significant levels of intensification, but that are prime candidates for the introduction of additional residential units (accessory apartments) and home-based businesses.

6.3.2 Permitted Uses

- a) Within the Existing Neighbourhood Designation, the following uses are permitted:
 - i) Low density and medium density residential uses;
 - ii) Special needs housing;
 - iii) Home-based businesses;
 - iv) Day care facilities;
 - v) Bed and breakfast establishments;
 - vi) Community facilities;
 - vii) Small scale places of worship;
 - viii) Parking facilities at grade, or in structure; and,
 - ix) Parks, open spaces and walkways/trails.
- b) In addition to the identified list of permitted uses, uses accessory to any of the identified permitted uses are also permitted.

- c) Residential development approved in site specific Official Plan Amendments, or in Draft Plans of Subdivision approved by the County prior to the adoption of this Plan are also permitted within the Existing Neighbourhood Designation.
- d) The County reserves the right, through the implementing Zoning By-law, to further refine the list of permitted uses to ensure that new development is appropriate in the context of the adjacent and surrounding community.

6.3.3 Development Policies

- a) The County shall permit the development, redevelopment and rehabilitation within the Existing Neighbourhood Designation by supporting a mixture of low density housing types, in accordance with the policies of this Plan and the associated Urban Design Guidelines attached to this Plan as **Appendix I**.
- b) Medium density residential uses may also be permitted on a limited basis within the Existing Neighbourhood Designation where they assist in defining a small scale neighbourhood focus such as a park, school, place of worship or commercial use. Such submissions will be subject to a Site Specific Zoning By-law Amendment and Site Plan Control process, including the submission of a planning justification report and other required studies, to ensure that the development proposal is compatible with adjacent uses, to the satisfaction of the County.
- c) All new development shall be compatible with existing adjacent residential uses in terms of orientation, privacy, landscaping, shadow casting, and visual impact. Where new development is introduced within, or abutting an Existing Neighbourhood Designation, the County will consider additional setbacks, angular lanes and enhanced landscaping as techniques to ensure an appropriate transition/interface.
- d) Certain lands within the Existing Neighbourhood Designation have been identified by the Long Point Region Conservation Authority as being Hazard Land. Consequently, any use of such land will be subject to the policies of Section 7.3 of the Norfolk County Official Plan. All development applications that are within the Hazard Land area within the Existing Neighbourhood Designation shall be evaluated in consultation with the Conservation Authority.
- e) All development within the Existing Neighbourhood Designation shall be compatible with existing development in proximity, in consideration of the following issues:
 - i) Building design, roof lines, density, scale and massing;
 - ii) Setbacks, signage, lighting, and buffering of existing and proposed development; and,

- iii) The use of landscaping and the provision of benches, planters and other street furniture.
- f) Linkages throughout the Existing Neighbourhood Designation to the Active Transportation Network identified on **Schedule D** shall be created and enhanced to foster pedestrian activity.
- g) All developments within the Existing Residential Designation shall be required to provide public parkland, or cash-in-lieu of parkland. Public parkland shall be provided subject to the following:
 - i) Public open space to be dedicated shall be landscaped prior to conveyance in a manner satisfactory to the County; and/or,
 - ii) Where cash-in-lieu of parkland is accepted for development within the Existing Neighbourhood Designation the County shall allocate the funds generated for public parkland improvements within Port Dover. The funds generated shall be used to provide additional public parkland or for the aesthetic and/or functional improvement of existing public parkland areas.

6.3.4 Urban Design Policies

- a) **Interpretation** - These Urban Design Policies are intended to guide new development within the Existing Neighbourhood Designation in combination with the other relevant policies of this Plan and the Urban Design Guidelines attached to this Plan as **Appendix I**. It is recognized that as policies within the statutory part of this Plan, all new development within the Existing Neighbourhood Designation shall conform to these Urban Design Policies. However, it is the intent of the County that progressive and innovative development is desirable within Port Dover, and the test of conformity with these Urban Design Policies may be interpreted with some flexibility, so long as the intent of these Policies is achieved, to the satisfaction of the County.
- b) **Cultural Heritage** - The County shall protect and enhance, where possible and appropriate, the historic architectural character found within the existing Neighbourhood Designation within Port Dover, particularly focusing on the pedestrian scale and design of the buildings and streetscapes, and the arrangement of windows and treatments on the building facades. Existing listed and designated cultural heritage resources within the Existing Neighbourhood Designation within Port Dover are identified in **Appendix II**. Heritage-related design policies for new development within the Existing Neighbourhood Designation within Port Dover include:

- i) New buildings shall relate to the traditional building stock, either through a traditional style that reflects the character of the area, or a more contemporary style that sets them apart from and highlights heritage buildings through a defined contrast and juxtaposition. Designs shall provide a clear distinction between 'new' and 'old'; and,
 - ii) Façade designs shall reference the articulation of neighbouring historic buildings, with respect to vertical and horizontal elements, including the rhythm and proportion of its main architectural elements.
- c) ***Design Policies for Residential Infill*** - The following guidelines anticipate the potential for modest residential infill within the existing residential neighbourhoods within the Existing Neighbourhood Designation in Port Dover:
- i) Avoid parking, driveways and garages along street frontages, which shall be located away from public view and preferably internal to the site, screened by buildings;
 - ii) Buildings shall be placed to create a continuous street wall; the setback distance shall be determined with reference to the setback distances of adjacent buildings and provide sufficient transition from private front yards to the public street;
 - iii) The massing and scale of the development shall be compatible with the existing and planned context;
 - iv) Main entries shall be located on the front of the building/unit and shall be highlighted in the architectural design;
 - v) Ensure that end units display the same level of architectural detail and articulation as that of the main front elevation; and,
 - vi) Detailed landscape treatments shall be coordinated with the main building materials and create a year round visually appealing presence along the street.

6.3.5 Silver Lake Special Policy Area

- a) Within the Existing Neighbourhood Designation in Port Dover, the area along the west side of Silver Lake extending north from the vicinity of Chapman and Patterson Streets to the rear of lots fronting on Queen Street, as delineated as Special Policy Area 6.3.5 on **Schedule B**, is the Silver Lake Special Policy Area. This Special Policy Area is to be considered and applied in addition to the other policies of this Section.

- b) The Silver Lake Special Policy Area is an area in transition which has traditionally included certain industrial and open space uses. There is the potential to create a specific linkage between the Downtown Area of Port Dover with the open space areas and the Lynn Valley Trail. It is the policy of this Plan to encourage a transition to open space and recreational uses in this area.
- c) Notwithstanding the underlying land use designations, permitted uses in the Silver Lake Special Policy Area, delineated on **Schedule B**, shall be limited to:
 - i) Passive and active open space and recreational uses;
 - ii) Public and/or private commercial recreational facilities;
 - iii) Small scale commercial uses including a farmers' market;
 - iv) Limited residential uses, such as senior housing complexes, lifestyle communities, and residential care facilities; and,
 - v) Institutional uses.
- d) The County shall use site plan control, in accordance with the policies of Section 9.6.5 (Site Plan Control) of the Norfolk County Official Plan to ensure that new development and redevelopment, including redevelopment of and conversion of existing buildings, is compatible with the intended character of the areas and the natural environment, and the adjacent residential areas.

6.3.6 Mill Store Site Specific Policy Area

- a) On land within the Existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.6, in addition to the uses permitted, an existing retail establishment focusing on the sale of fabrics and clothing, a farmers' market, a place of sports and recreation, and an adult training centre shall be permitted.

6.3.7 Restaurant Site Specific Policy Area

- a) On land within the Existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.7, in addition to the uses permitted, a restaurant shall also be permitted to locate within the existing dwelling.

6.3.8 Office Site Specific Policy Area

- a) On land within the Existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.8, an administrative office shall also be permitted within the existing one-storey residential dwelling unit.

6.3.9 Gardening Supply Outlet Site Specific Policy Area

- a) On land within the existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.9, in addition to the uses permitted, a gardening supply outlet shall be permitted for the sale of supplies directly related to the installation and maintenance of the various landscaping backyard pond displays.

6.3.10 Health Clinic Site Specific Policy Area

- a) On land within the Existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.10, in addition to the permitted uses, an animal hospital, a clinic or doctor's offices, a financial institution, offices, a pharmacy, and a retail store associated with a clinic or doctor's office shall be permitted. The commercial structure or building shall have a gross floor area of up to 900 m², where the number of commercial retail units is limited to a maximum of two units with a maximum usable floor area of 200 m² per unit. The usable floor area of a pharmacy shall be limited to a maximum of 50 m².

6.3.11 Dover Coast Residential Site Specific Policy Area

- a) On land within the Existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.11, Notwithstanding the policies of this Plan, golf course uses shall also be permitted.
- b) For any residential proposal on the Subject Land and on the Other Lands owned by the applicant, as identified on **Schedule B**, that are within the Industrial Influence Area, as identified on **Schedule A** to this Plan, a distance of no more than 300 metres, the following studies shall be completed and submitted to Norfolk County and Haldimand County:
 - i) Planning Justification Report;
 - ii) Air Quality Assessment; and,
 - iii) Noise Study.
- c) Each of the completed studies may be peer reviewed at the discretion of Norfolk County and/or Haldimand County and the proponent shall pay the reasonable cost of the peer review. This review may include consultation with the Province.

6.3.12 Shore Acres Park Site Specific Policy Area

- a) In addition to the list of permitted uses within the Existing Neighbourhood Designation, the lands identified as Site Specific Policy Area 6.3.12 on **Schedule B**, the existing recreational trailer park shall also be a permitted use.

6.4 Future Neighbourhood Designation

6.4.1 Intent

- a) It is the intent of the County to promote well-designed and attractive residential neighbourhoods throughout Port Dover. The Future Neighbourhood designation will include an appropriate range and mix of housing types, parks and open space features and an array of community facilities. The Future Neighbourhood Designation will include provisions for the establishment of Neighbourhood Centres.

6.4.2 Permitted Uses

- a) Within the Future Neighbourhood Designation, the following uses are permitted:
 - i) Low density, medium density and high density residential uses;
 - ii) Live-work units;
 - iii) Special needs housing;
 - iv) Neighbourhood centres;
 - v) Home-based businesses;
 - vi) Day care facilities;
 - vii) Bed and breakfast establishments;
 - viii) Community facilities;
 - ix) Education facilities;
 - x) Small scale places of worship;
 - xi) Emergency services;
 - xii) Parking facilities at grade, or in structure; and,
 - xiii) Parks, open spaces and walkways/trails.
- b) In addition to the identified list of permitted uses, uses accessory to any of the identified permitted uses are also permitted.

- c) Residential development approved in site specific Official Plan Amendments, or in Draft Plans of Subdivision approved by the County prior to the adoption of this Plan are also permitted within the Future Neighbourhood Designation.
- d) The County reserves the right, through the implementing Zoning By-law, to further refine the list of permitted uses to ensure that new development is appropriate in the context of the adjacent and surrounding community.

6.4.3 Development Policies

- a) All development within the Future Neighbourhood Designation shall proceed through a Secondary Plan/Official Plan Amendment Process that will establish the more detailed land use designations and associated policy framework to properly guide future development.
- b) The County shall require, prior to the approval of any Secondary Plan/Official Plan Amendment, or any associated development application (Draft Plan of Subdivision or implementing Zoning By-law) within any of the individual Development Areas identified on **Schedule A**, that are also designated Future Neighbourhood on **Schedule B**, that a Block Plan be prepared for the entire individual Development Area. The purpose of the Block Plan is to promote comprehensive planning, and to:
 - i) Identify the detailed land use and density distribution, and to ensure that the required density target is achieved;
 - ii) Confirm the boundaries of the Natural Heritage System;
 - iii) Identify the location for the Neighbourhood Centres;
 - iv) Identify the parkland system, and the active transportation network;
 - v) Identify the location for any required educational and/or community facilities;
 - vi) Identify the detailed road pattern, including Local Roads;
 - vii) Articulate the details for the provision of sewer, water and storm water management systems;
 - viii) Identify the road network and infrastructure system connections to all properties within and adjacent to the individual Neighbourhood Area; and,
 - ix) Potentially form the basis for a Developer's Group Agreement, where the identified Neighbourhood Area includes multiple landowners.

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Required Block Plans shall be adopted by the County and shall include all of the necessary supporting technical studies, as identified in the required pre-consultation process, to the satisfaction of the County. Required Block Plans shall form the basis for the subsequent approval of Draft Plans of Subdivision and implementing Zoning By-laws.

- c) The County shall permit the development of a range and mix of housing types and densities, as well as appropriate neighbourhood supporting land uses, in accordance with the policies of this Plan and the Urban Design Guidelines attached as **Appendix I**. In addition, within any of the individual Development Area identified on **Schedule A**, that are also designated Future Neighbourhood on **Schedule B**, at least one Neighbourhood Centre shall be established. Permitted Neighbourhood Centres shall be centrally located within each of the identified Development Areas. Generally, a Neighbourhood Centre shall:
 - i) Be located at an intersection, where at least one road is a collector or arterial; and,
 - ii) Be within a walking distance of 5 to 10 minutes for most of the residents of the defined Development Area.
- d) Within a Neighbourhood Centre, the mix of uses shall be compatible and sensitively integrated with the surrounding residential uses in terms of building mass, height, setbacks, orientation, privacy, landscaping, shadow casting, accessibility and visual impact. In addition to the residential uses permitted within a Neighbourhood Centre, at least one of the following additional land uses shall be required:
 - i) One local convenience retail store use up to 190 square metres of non-residential gross floor area per Neighbourhood Centre. Apartment units may be permitted above the ground floor at the rear or to the side of the local convenience store; and/or,
 - ii) Institutional and community uses which provide services to the neighbourhood.
- e) Linkages throughout the Development Areas to the Active Transportation Network identified on **Schedule D** shall be created and enhanced to foster pedestrian activity.
- f) All developments within the Future Neighbourhood Designation shall be required to provide public parkland, or cash-in-lieu of parkland. Public parkland shall be provided subject to the following:
 - i) Public open space to be dedicated shall be landscaped prior to conveyance in a manner satisfactory to the County;

- ii) The first priority for parkland dedication is the achievement of new public parks within each of the Development Areas identified on **Schedule A**; and/or,
- iii) Where cash-in-lieu of parkland is accepted for development within the identified Development Areas, the County shall use the funds to provide public parkland within the Development Area where the funds were generated, or for the aesthetic and/or functional improvement of existing public parkland areas.

6.4.4 Urban Design Policies

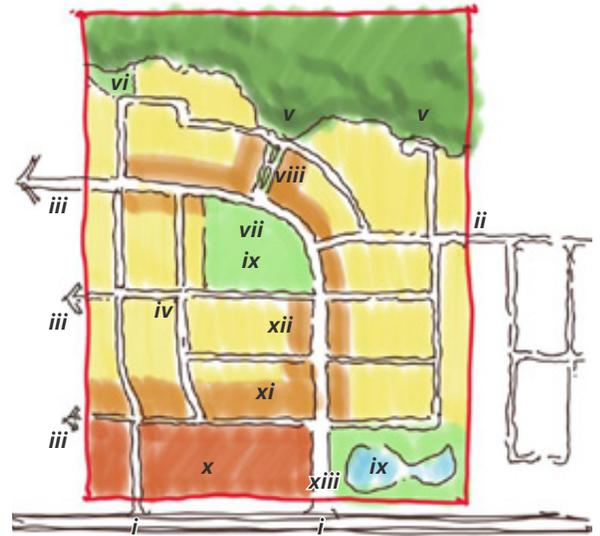
- a) **Interpretation** - These Urban Design Policies are intended to guide new development within the Future Neighbourhoods Designation in combination with the other relevant policies of this Plan and the Urban Design Guidelines attached to this Plan as **Appendix I**. It is recognized that as policies within the statutory part of this Plan, all new development within the Future Neighbourhood Designation shall conform to these Urban Design Policies. However, it is the intent of the County that progressive and innovative development is desirable within Port Dover, and the test of conformity with these Urban Design Policies may be interpreted with some flexibility, so long as the intent of these Policies is achieved, to the satisfaction of the County.
- b) **Development abutting Natural Features** - The following policies apply to lands that abut lands within the Provincially Significant Wetlands, Hazard Land and/or the Significant Woodlands Designations, as defined on **Schedule B**. It is a policy of the County to:
 - i) Protect and incorporate the surrounding natural heritage system as an integral part of the neighbourhood's structure;
 - ii) Minimize development that encroaches into the natural heritage system and negatively impact the health and diversity of it due to noise, light pollution, debris, and unauthorized access;
 - iii) Provide frequent access points and public street frontage to promote views and accessibility to natural heritage areas;
 - iv) Create views and vistas to natural heritage features, parks and open spaces through the location, arrangement and configuration of streets and blocks;
 - v) Locate parks and open spaces prominently, with adjacency or strong connections to the natural heritage system and trail network; and,



Street arranged to provide view of natural heritage feature

- vi) Back lotting of the natural heritage features shall be minimized.
- c) **Sustainable Design Policies** - Development within the identified Future Neighbourhoods Designation, as identified on **Schedule B** shall be developed with regard to the following:
- i) The principles of LEED-ND (Leadership in Energy and Environmental Design – Canada) as they evolve; and,
 - ii) Building orientation to maximize potential for passive and active solar energy.
- d) **Neighbourhood Layout Policies** - It is a policy of the County to:
- i) Create a connected, pedestrian-oriented and highly interconnected street and block pattern, with connections to adjacent communities and to community amenities/ destinations;
 - ii) Limit blocks to no more than 180m in length; blocks that are longer than this in length shall include mid-block landscaped pedestrian links of at least 8m in width;
 - iii) Provide appropriate transition to/integration with adjacent uses;
 - iv) Changes in land use, lotting and built form shall occur along a rear lot line so that similar uses and forms shall frame both sides of a street;
 - v) Parks and open spaces shall be provided with substantial frontage on a public road. Back lotting of parks and open spaces shall be minimized;
 - vi) Locate higher density forms of development at prominent locations such as around parks, adjacent to major roads, at gateways and along special streets; and,
 - vii) Require built form that is a minimum of three storeys in height around parks and at prominent locations.
- e) **Road Pattern Policies** - It is a policy of the County to:
- i) Maximize number of connections to the major road network;
 - ii) Connect to existing road stubs;
 - iii) Provide multiple future road connections to undeveloped areas;
 - iv) Provide well-connected internal road network;

- v) Provide direct connection to natural features from internal streets;
- vi) Provide vista parks from internal streets to natural features where direct frontage is not feasible;
- vii) Locate key destinations such as retail and service commercial uses, parks and schools within 5 minute walk (400m radius) of most residents;
- viii) Use streets and public spaces to create linkages from natural heritage features and lower intensity land uses into the central area of the neighbourhood;
- ix) Ensure publicly accessible open spaces such as parks, storm water management facilities and natural heritage features have significant frontage on internal public roads;
- x) Provide higher density and mixed uses adjacent to higher order roads;
- xi) Create a transition of higher density to lower density uses;
- xii) Provide a mix of housing forms throughout the neighbourhood and along streetscapes; and,
- xiii) Locate built form and public space to create and accentuate entry points to the neighbourhood.



Demonstration plan of road pattern policies

6.5 Mixed Use Designation

6.5.1 Intent

- a) It is the intent of this Plan that the lands within the Mixed Use Designation develop and intensify over time with a range and mixture of land uses and higher intensity character, in a manner that is sensitive to the adjacent residential neighbourhoods. The Mixed Use Designation is expected to provide retail and service commercial uses that serve a growing local population.

6.5.2 Permitted Uses

- a) Within the Mixed Use Designation, the following uses are permitted:
 - i) Retail and service commercial uses;
 - ii) Restaurants;
 - iii) Offices;
 - iv) Hotels and motels;
 - v) Convention centres,

- vi) Private clubs;
 - vii) Recreational and entertainment uses;
 - viii) Arts and cultural facilities;
 - ix) Medium density and high density residential uses, including special needs housing;
 - x) Live/work units;
 - xi) Home-based businesses;
 - xii) Small scale places of worship;
 - xiii) Community facilities;
 - xiv) Education facilities;
 - xv) Institutional uses;
 - xvi) Emergency services;
 - xvii) Day care facilities;
 - xviii) Parking facilities at grade, or in structure; and,
 - xix) Parks, open spaces and walkways/trails.
- b) In addition to the identified list of permitted uses, uses accessory to any of the identified permitted uses are permitted.
- c) The County reserves the right, through the Implementing Zoning By-law, to further refine the list of permitted uses to ensure that new development is appropriate in the context of the adjacent and surrounding community.

6.5.3 Development Policies

- a) The County shall encourage mixed use development within the Mixed Use Designation, in accordance with the policies of this Plan and the associated Urban Design Guidelines attached to this Plan as **Appendix I**.
- b) All development within the Mixed Use Designation shall be compatible with existing development in proximity.
- c) Linkages between the Mixed Use Designation and the Active Transportation Network identified on **Schedule D** shall be created and enhanced to foster pedestrian activity.
- d) All developments within the Mixed Use Designation shall be required to provide public parkland, or cash-in-lieu of parkland. Where cash-in-lieu of parkland is accepted for development within the Mixed use Designation, the funds generated shall be used to provide additional public parkland, or for the aesthetic and/or functional improvement of existing public parkland areas.

- e) Permitted retail uses shall be limited in scale to a maximum of 3,000 square metres of Gross Floor Area per individual retail use. Further, permitted retail uses shall have a minimum of 125 square metres of Gross Floor Area per individual retail use.
- f) The maximum building height within the Mixed Use Designation shall be 6 storeys, or 20 metres, whichever is less, and shall also be subject to the following provisions that will be more fully articulated within the Implementing Zoning By-law:
 - i) Where a property within the Mixed use Designation abuts existing or proposed low density residential uses, appropriate mechanisms shall be employed to ensure compatibility and an appropriate transition to those abutting properties. Mechanisms may include a reduction in the permitted building height, the implementation of enhanced building setbacks, the requirement for landscape planting strips, the imposition of a step back and/or the imposition of an angular plane;
 - ii) All development within the Mixed Use Designation shall have a minimum height of 2 storeys; and,
 - iii) All development within the Mixed Use Designation shall incorporate a floor to ceiling height of the first floor of 4.5 metres.

Taller buildings may be considered for approval by the County at the Gateway locations identified on **Schedule A** that are also within the Mixed Use Designation on **Schedule B**.

- g) All new development with frontage along Main Street, or Highway 6 within the Mixed Use Designation have at least 50% of its ground floor Gross Floor Area dedicated to non-residential uses, with a preference for retail and service commercial uses and restaurants. Residential uses shall only be permitted above the ground floor and/or on the ground floor in the rear of the building.
- h) In the consideration of any Implementing Zoning By-law proposing to establish any of the permitted uses, the following shall be addressed:
 - i) Adequate and appropriate access to the property from a public road; and,
 - ii) The provision of adequate and appropriate parking and loading facilities. All development within the Mixed use Designation shall provide adequate parking on-site.

- i) Comprehensive block development of lands in separate ownerships will be required, to achieve well-designed and integrated development, including:
 - i) Integrated internal circulation systems;
 - ii) Co-ordinated access points, to minimize the total number of access points to abutting roads;
 - iii) Compatible building design and location, to achieve a consistent streetscape;
 - iv) Complementary landscaping plans;
 - v) Integrated parking areas; and
 - vi) Consistent signage and lighting facilities.
- j) The site plan control policies in accordance with the policies of Section 9.6.5 (Site Plan Control) of the Norfolk County Official Plan shall apply to all development applications in the Mixed Use Designation.

6.5.4 Urban Design Policies

- a) **Interpretation** - These Urban Design Policies are intended to guide new development within the Mixed Use Designation in combination with the other relevant policies of this Plan and the Urban Design Guidelines attached to this Plan as **Appendix I**. It is recognized that as policies within the statutory part of this Plan, all new development within the Mixed Use Designation shall conform to these Urban Design Policies. However, it is the intent of the County that progressive and innovative development is desirable within Port Dover, and the test of conformity with these Urban Design Policies may be interpreted with some flexibility, so long as the intent of these Policies is achieved, to the satisfaction of the County.
- b) **General Design Policies** - The following policies apply everywhere within the Port Dover Mixed Use Designation:
 - i) A minimum 2-storey street wall shall be maintained facing public streets along all streets;
 - ii) Buildings shall be oriented to frame the street edge and to create a strong street wall;
 - iii) Buildings shall front directly onto public streets and other public spaces, in order to clearly define the public realm and create an attractive and safe pedestrian environment;
 - iv) Buildings at corner locations shall be sited to address the intersection, with consideration given to both street frontages;



2-storey street wall



Buildings front directly onto streets and public spaces



Facade rhythm through architectural articulation

- v) Buildings shall align with the existing street wall. In residential areas, buildings shall generally be aligned with the setbacks of their neighbours;
- vi) Main building entrances shall face the street and shall be clearly defined with architectural details and easily identifiable within the facade composition. Publicly accessible front entrances for stores, offices, and institutional uses shall permit barrier-free access and universal accessibility, including both visual and physical accessibility;
- vii) Ground floor facades shall be highly transparent, including transparent windows and entrance doors, to establish a strong visual connection between the street and the interior of active ground floor uses; and,
- viii) Facade rhythm will be established along a street wall through architectural articulation, including the use of fenestration, bands, columns, and other repeated elements.

c) **Design Policies for Parking and Access** - Guidelines for parking and access within the Mixed Use Designation generally relate to the design and functionality of on- and off street parking and access driveways. The objective is to provide suitable parking and driveway options while reducing traffic impacts and the negative visual impact of surface parking lots. The following design policies apply:

- i) Parking facilities shall be designed and located to minimize its impact on the streetscape and public realm to ensure that sidewalks and building facades define the street edge;
- ii) Parking lots shall be organized to minimize the number of potential pedestrian-vehicle movement conflicts. Where possible, parking areas shall be coordinated between multiple properties to maximize connectivity, improve traffic flow and increase the efficiency of parking;
- iii) Parking lots shall be designed to enhance the feeling of safety through techniques such as maintaining clear sight lines for natural surveillance of the parking lot as well as minimizing dark, hidden or obscured areas. Parking lot design shall consider:
 - » Planting strips and landscaped traffic islands, medians, or bump-outs shall also be provided within lots to break up the expanse of hard surface;



Landscaped medians and islands



Pedestrian-scaled walkways and landscaping enhance safety, movement and comfort

- » Pedestrian-scaled lighting, walkways, landscaping, and signage to enhance pedestrian safety, movement and comfort through parking lots to the adjacent sidewalks;
 - » Pedestrian routes through parking areas shall be wide enough to accommodate comfortable travel and their walking surfaces shall be clearly differentiated from the parking areas with texture, material, colour changes or markings;
- iv) Where existing parking areas are adjacent to the sidewalk, a landscaped area shall be located between parking spaces and the sidewalk. Landscaping shall provide clear sight lines at eye level.
 - v) Bicycle parking and racks shall be provided in locations that are close to building entrances but situated to avoid any conflicts for movement along pedestrian routes.
 - vi) Where possible, access to parking areas shall be provided from side streets and laneways. Access to parking areas shall be defined through clearly designated entrances and exits.

6.6 Commercial Designation

- a) The County shall encourage the development, redevelopment and rehabilitation within the Commercial Designation, identified on **Schedule B**.
- b) The County will encourage new development within the Commercial Designation in accordance with the policies of Section 7.11 of the Norfolk County Official Plan and consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.
- c) All development within the Commercial Designation shall be compatible with existing development in proximity.

6.7 Major Institutional Designation

6.7.1 General Policies

- a) The County shall encourage the development, redevelopment and rehabilitation within the Major Institutional Designation, identified on **Schedule B**.
- b) The County will encourage new development within the Major Institutional Designation in accordance with the policies of Section 7.14 of the Norfolk County Official Plan and consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.

- c) All development within the Major Institutional Designation shall be compatible with existing development in proximity.

6.7.2 Former Port Dover Secondary School Site Specific Policy Area

- a) On lands designated as Major Institutional – Site Specific Policy Area 6.7 on **Schedule B**, in addition to the permitted uses of the Major Institutional Designation, an elementary school, a day care facility, a public library and public recreational uses may be permitted.

6.8 Parks and Open Space Designation

- a) The County shall encourage the development, redevelopment and rehabilitation within the Parks and Open Space Designation, identified on **Schedule B**.
- b) The County will encourage new parks and community facilities to be identified within the Parks and Open Space Designation in accordance with the policies of Sections 5.6 and 7.15 of the Norfolk County Official Plan and consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.

6.9 Industrial Designation

6.9.1 General Policies

- a) The County shall encourage the development, redevelopment and rehabilitation within the Industrial Designation, identified on **Schedule B**.
- b) The County will encourage new development within the Industrial Designation in accordance with the policies of Section 7.13 of the Norfolk County Official Plan and consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.
- c) All development within the Industrial Designation shall be compatible with existing development in proximity.

6.9.2 Silver Lake Industrial Site Specific Policy Area

- a) On land designated Industrial– Site Specific Policy Area 6.9.2 on **Schedule B** to this Plan, a trucking operation shall not be permitted and any proposals for redevelopment shall take into account the nature of the surrounding land uses.

6.10 Major Public Infrastructure Designation

- a) The County shall encourage the development, redevelopment and rehabilitation within the Major Public Infrastructure Designation, identified on **Schedule B**.

- b) The County will encourage new development within the Major Public Infrastructure Designation in accordance with the policies of Section 7.17 of the Norfolk County Official Plan and consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.
- c) All development within the Major Public Infrastructure Designation shall be compatible with existing development in proximity.

6.11 Hazard Land Designation

6.11.1 General Policies

- a) Lands identified as within the Hazard Land Designation identified on **Schedule B** shall be subject to the policies of Section 7.3 of the Norfolk County Official Plan.

6.11.2 Restaurant/Spa Site Specific Policy Area

- a) On land designated Hazard Lands – Site Specific Policy Area 6.11.2 on **Schedule B** to this Plan, in addition to the uses permitted, a restaurant and a spa in conjunction with the restaurant shall be permitted.

6.12 Provincially Significant Wetland Designation

- a) Lands identified as within the Provincially Significant Wetland Designation identified on **Schedule B** shall be subject to the policies of Section 7.4 of the Norfolk County Official Plan.

6.13 Significant Woodlands

- a) Lands identified as within the Significant Woodlands Designation identified on **Schedule B** shall be subject to the applicable policies of Sections 4.6 and 5.5 of the County of Norfolk Official Plan.
- b) In the consideration of development applications in which there is a net loss of any area within the Significant Woodland Designation, the County will require this loss be compensated by the developer with the replacement of trees, or cash-in-lieu of trees. The trees, or the cash-in-lieu of trees collected to purchase trees, shall be planted in a location to be determined by the County. In determining appropriate compensation, consideration shall be given to the significance and value of the ecological function that the existing tree inventory provides.

7.0 Networks and Infrastructure

- a) This Plan includes **Schedule C: Roads Plan** and **Schedule D: Active Transportation Plan**. To implement the development of new roads, and active transportation facilities, the relevant policies of Sections 8.1, 8.2 and 8.3 of the Norfolk County Official Plan shall apply. Changes to the location of any potential roads, or trails that are identified on **Schedule C** or **Schedule D** may be implemented without an Amendment to this Plan, provided that the principles inherent to this Plan are achieved, to the satisfaction of the County. In addition, all roads, trails and active transportation facilities within Port Dover shall be designed to be consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.
- b) With respect to water transportation, the relevant policies of Section 8.6 of the Norfolk County Official Plan shall apply.
- c) With respect to matters related to required storm water management systems, the relevant policies of Sections 8.7 and 8.9.4 of the Norfolk County Official Plan shall apply. In addition, all storm water management facilities within Port Dover shall be designed to be consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.
- d) With respect to matters related to required utilities, telecommunications, water, and wastewater systems, the relevant policies of Sections 8.7 and 8.9 of the Norfolk County Official Plan shall apply.
- e) With respect to matters related to noise, vibration, odour and light emissions, the relevant policies of Section 8.8 of the Norfolk County Official Plan shall apply.
- f) With respect to matters related to waste management, energy supply and transmission and emergency services, the relevant policies of Sections 8.10, 8.11 and 8.12 of the Norfolk County Official Plan shall apply.
- g) With respect to matters related to capital and public works, the relevant policies of Section 8.13 of the Norfolk County Official Plan shall apply.

8.0 Implementation and Monitoring

- a) This Plan applies to all planning decisions within the boundaries of the community of Port Dover, as identified on the Schedules to this Plan. To implement and monitor this Plan, all of the relevant policies of Section 9 of the Norfolk County Official Plan shall be applied.

This Plan promotes significant reliance on the principles and policies related to Urban Design. In addition, Urban Design Guidelines have been established and are attached to this Plan as **Appendix I**. To assist the County with the implementation of the principles, policies and guidelines related to urban design the County will utilize the complete application requirements of Section 9.7 of the County Official Plan and may establish:

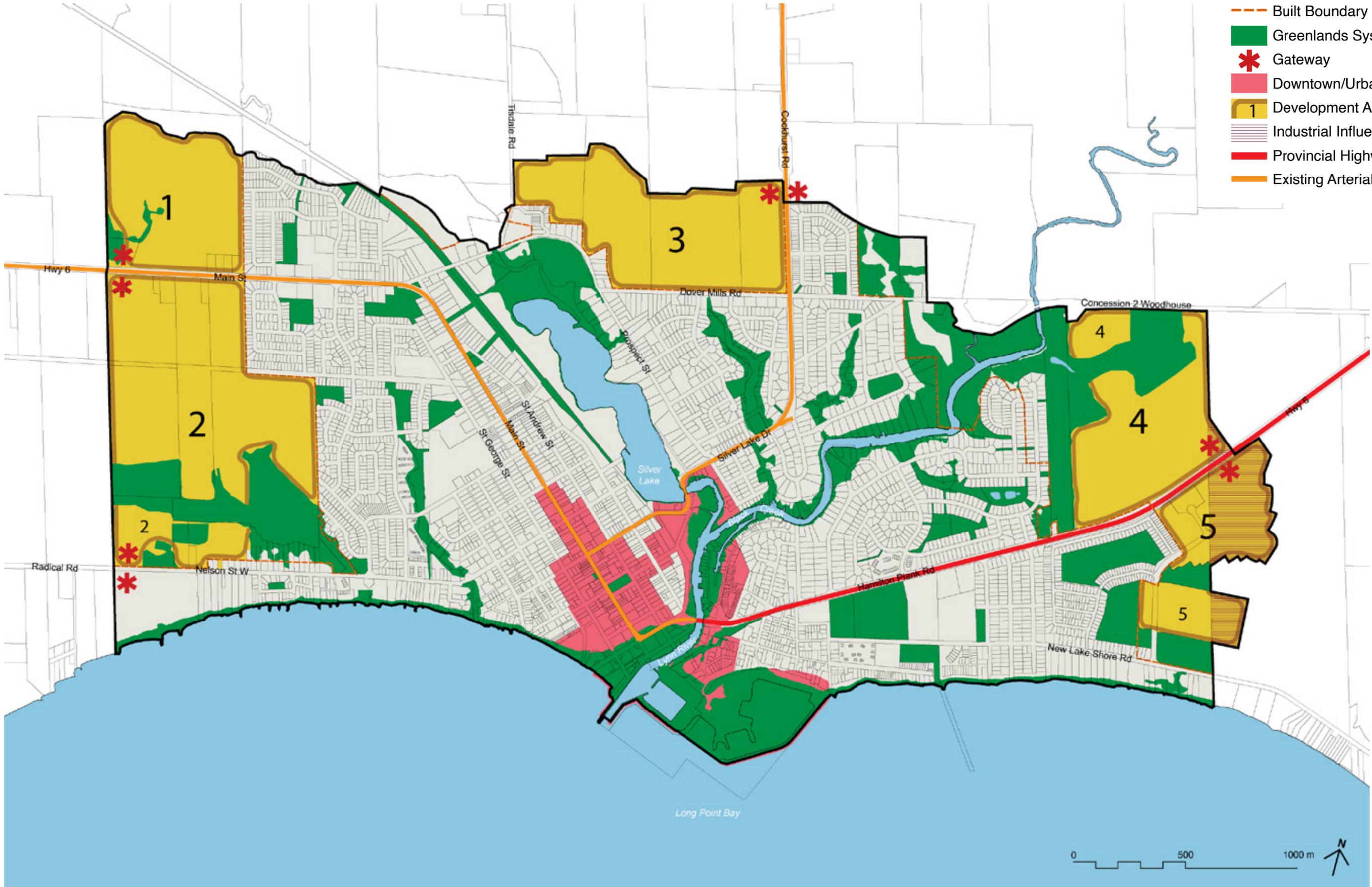
- i) An urban design peer review protocol;
- ii) A process for architectural control, including the appointment of a control architect(s); and/or,
- iii) The creation of a Design Review Panel.

Schedule A: Community Structure

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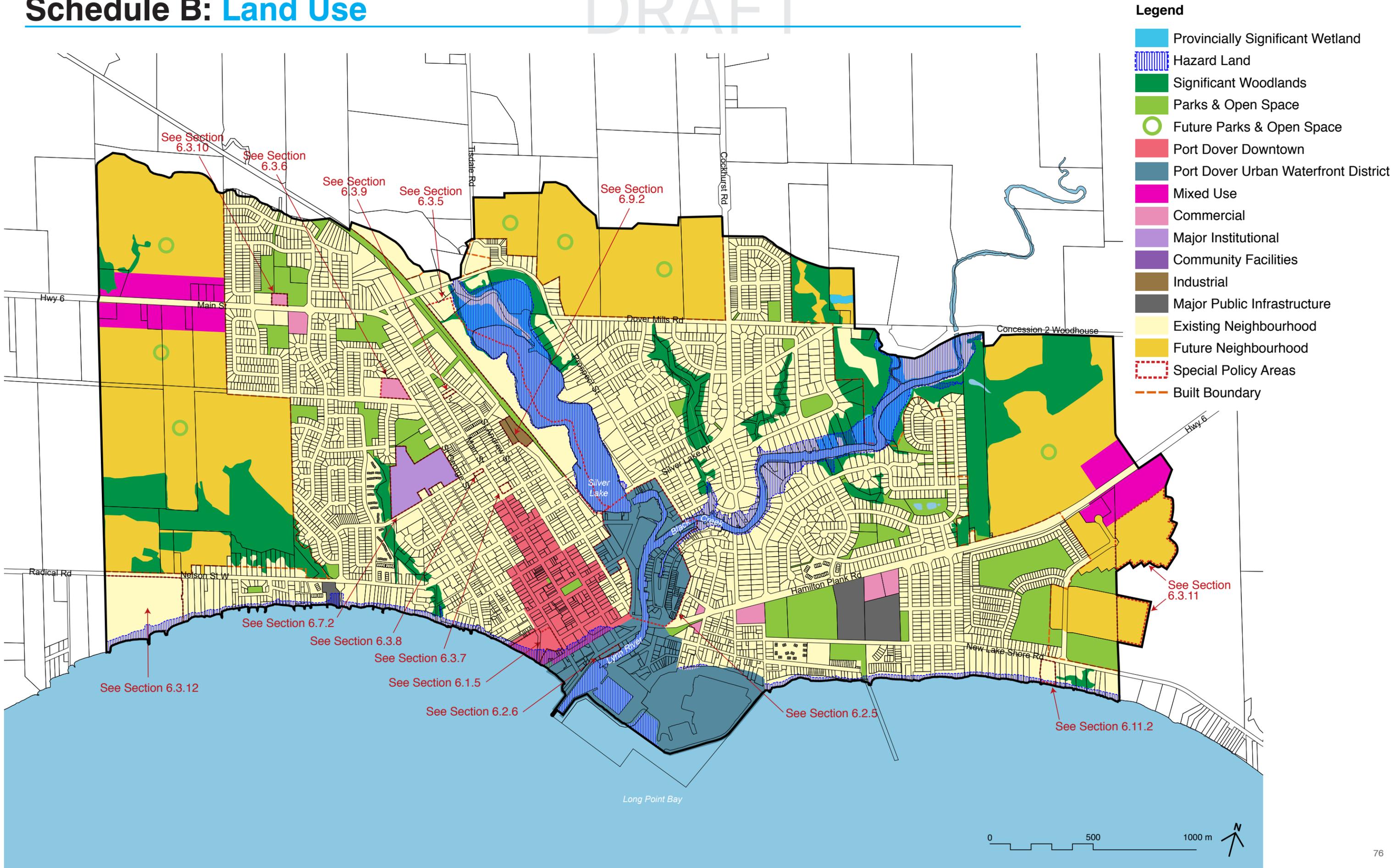
Legend

- Settlement Boundary
- - - Built Boundary
- Greenlands System
- * Gateway
- Downtown/Urban Waterfront
- 1 Development Areas
- ▨ Industrial Influence Area
- Provincial Highway
- Existing Arterial Road



Schedule B: Land Use

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Schedule C: Roads Plan

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Legend

- Existing Provincial Highway
- Existing Arterial Road
- Existing Collector Road
- Potential Arterial Road
- Potential Collector Road
- Local Road

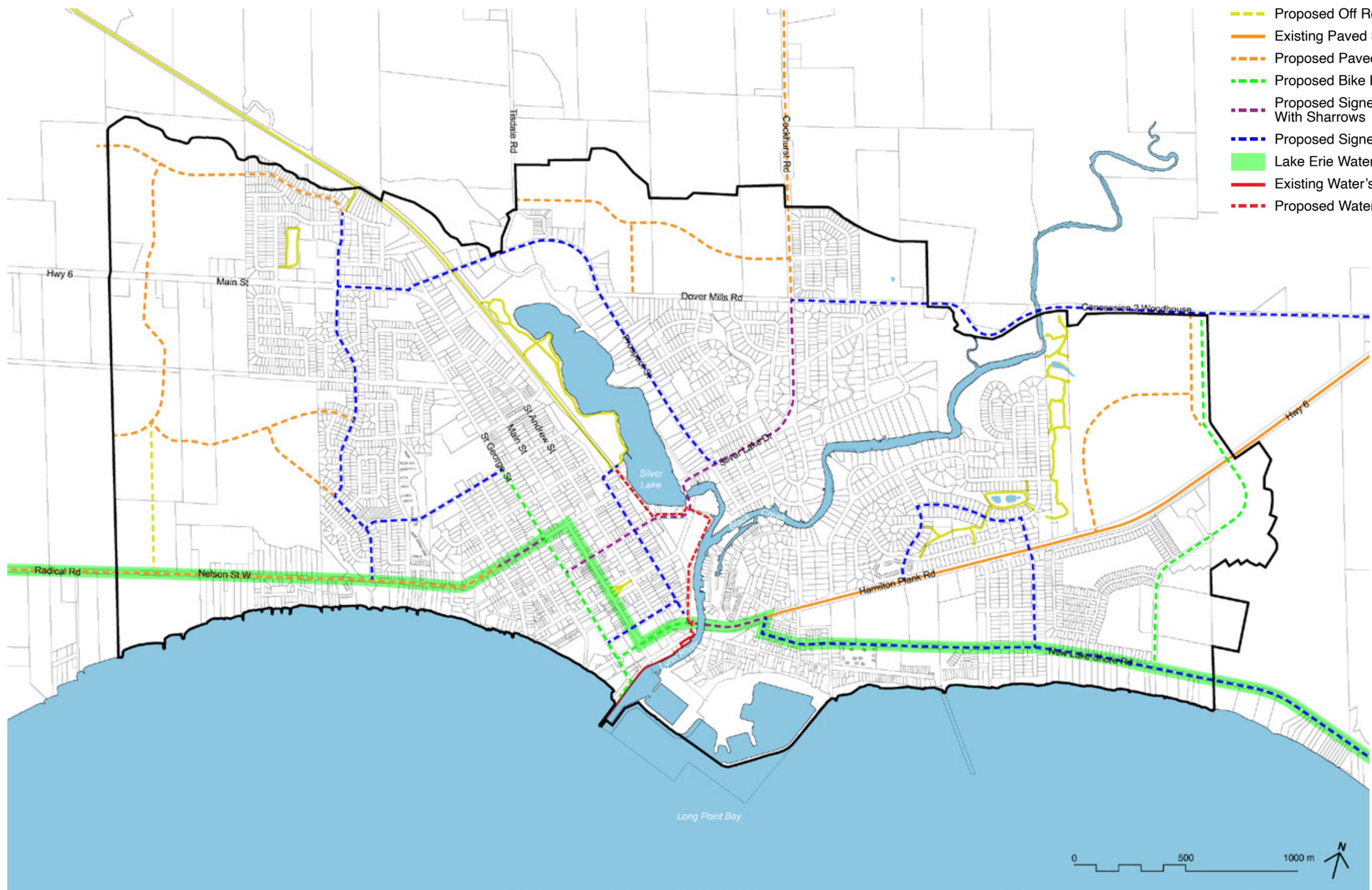


Schedule D: Active Transportation Plan

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Legend

- Existing Off Road Trail
- Proposed Off Road Trail
- Existing Paved Shoulder
- Proposed Paved Shoulder
- Proposed Bike Lane
- Proposed Signed Route With Sharrows
- Proposed Signed Route
- Lake Erie Waterfront Trail
- Existing Water's Edge Trail
- Proposed Water's Edge Trail



**Appendix I: Port Dover Urban
Design Guidelines**

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1 Introduction

The Port Dover Secondary Plan is the guiding document used to direct and manage growth in Port Dover. It articulates the vision and guiding principles for how the community should be developed and outlines the policies for how land in the community should be used. The Secondary Plan helps to ensure that future planning and development meets the specific needs of the community. Urban Design Guidelines provide a 'how-to' manual for implementation of the Secondary Plan's policies that reinforce the character and charm of Port Dover, respects its natural and cultural heritage and protects the small town feel cherished by those that live here.

Urban design involves the arrangement and design of the built environment and provides a framework that gives form, shape and character to buildings, public spaces, streets and amenities. It blends architecture, landscape architecture and town planning together to make urban centres, districts and neighbourhoods functional and attractive. The Urban Design Guidelines promote high quality urban design that is based upon the quality, scale and character of the surrounding existing and emerging contexts, reinforce 'human scaled' environments, and promote a sense of place.

Urban Design Guidelines (Guidelines) provide design guidance, criteria, and standards for how to shape the built environment, both the individual elements, as well as how these should be spatially arranged and relate to one another. The Guidelines are intended to provide direction for homeowners, designers, architects, landscape architects and developers to understand the County's objectives for design in Port Dover. The Guidelines also inform staff and Council in their review, and approval of development applications. The Guidelines will be used through the development process including Subdivision Planning, Site Planning, Landscape and Building Design.

The Urban Design Guidelines are an appendix to the Port Dover Secondary Plan. Some guidelines will be brought forward to become policy in the

Secondary Plan, others will be implemented through zoning, others through site plan control.

As such, they are not a "statutory" part of the Secondary Plan, and conformity to these Guidelines is not required. Urban Design Guidelines are always interpreted with inherent flexibility. However, they will be subject to the test "to be consistent with". These Guidelines are also intended to be given statutory force through the Implementing Zoning By-law, and/or through Site Plan Approval.

The Guidelines are organized to provide direction on development and redevelopment in the private and public realm. Some of the Guidelines apply to specific districts or areas within Port Dover, and others apply to particular uses or functions throughout the community of Port Dover.

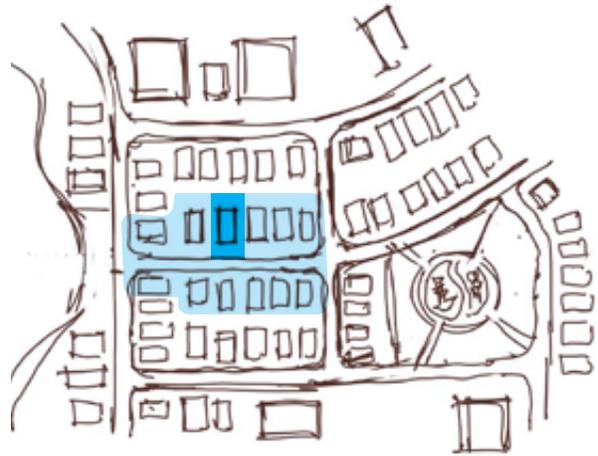
1.1 Compatibility

The Guidelines provide a framework for design that respects and reinforces the character of the Downtown Port Dover, the waterfront and the existing neighbourhoods. The Guidelines are based on a contextual approach to design that considers the visual impact to and relationships with adjacent and surrounding developments. This approach promotes compatible forms and designs, pedestrian scaled and oriented streetscapes, and allows for appropriate flexibility, innovation and diversity in design, qualities intrinsic to evolving communities.

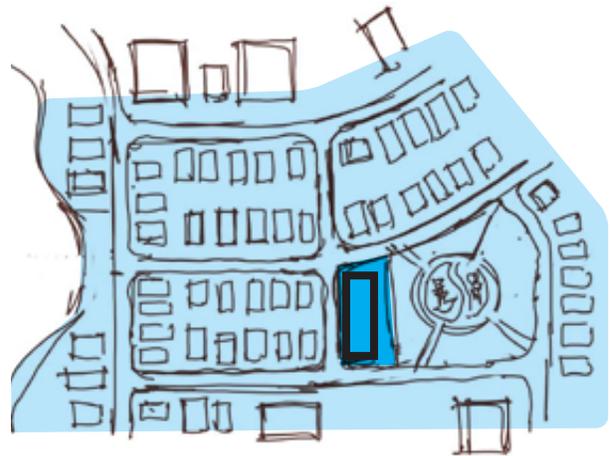
In determining compatibility, an area of influence in the vicinity of the new development is used. New development should be compatible with the existing development within its area of influence. The scale of new development determines the appropriate scale of the area of influence. Norfolk County staff should be consulted in the determination of the appropriate size of the area of influence.



The area of influence for modifications to an existing site or building will generally be the immediate neighbouring and facing properties.



The area of influence for demolition or redevelopment of a single house or property will generally be the block.



The area of influence for large redevelopment sites will generally be the streetscape and neighbourhood.

Scale of new development	Area of influence for determining compatibility
modifications to an existing house or property	existing house and immediate neighbours (including across the street)
demolition or redevelopment of a single house or property	streetscape/block
large scale redevelopment; development with land assembly; intensification	neighbourhood

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Larger developments must consider impacts to the neighbourhood. Guidelines for determining the scale of the neighbourhood include:

- a local area where there is convenient access to one or more land uses or amenities common to daily living, such as housing, a school, park, shopping, personal/professional/medical services, daycare, and/or community uses;
- more than one block; usually several blocks linked together to form a network;
- an area bounded by a five-minute walk or 400 metre radius; and
- edges can be defined by major roads and/or natural features.

2 Downtown and Urban Waterfront

2.1 Introduction

Downtown and the Urban Waterfront are the heart and soul of Port Dover. Both areas establish the inherent small town character of Port Dover with its commercial fishing harbour - each forming critical components of its sense of place. Guidelines in this chapter establish direction for development and redevelopment in both areas.

Port Dover Downtown

Downtown has a mix of commercial, institutional and residential uses, and is anchored by Main Street. A grid pattern of streets create small blocks, making it easy to walk around and connect to the waterfront. Powell Park, an urban park, is a major focal point.

Buildings exhibit a variety of materials, with brick and siding most commonly used. There are a number of buildings with a heritage designation or potential concentrated Downtown.

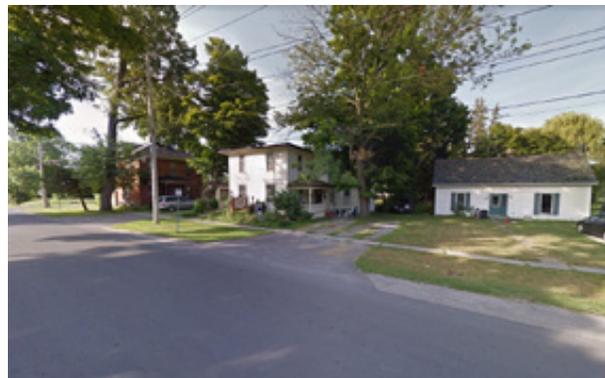
Main Street

In the core of the Downtown area, buildings are located close to the street line and to one another, resulting in a well-defined streetwall. Commercial buildings are typically 2-storeys in height, with flat roofs. Many buildings have an historic character that create a distinct sense of place. These are design characteristics that these guidelines seek to reinforce.

At each end of Main Street, buildings are more widely spaced, typically one storey in height, with varying set backs. On-street parking is replaced by vehicular lanes and parking is provided in large surface lots. These areas are more suburban in character and oriented to vehicular access. As these areas change or redevelop over time, these Guidelines will shape a more pedestrian-oriented and well-defined Main Street through Downtown.

Downtown Residential

The streets around Main Street are more residential in character, although they also have commercial uses mixed in. Buildings have set backs from each other and from their neighbours while still defining the street edge. There is a mix of historic, traditional and more modern architectural styles. Parking is often accommodated in side driveways or garages set back from the street edge. Large mature trees make a significant contribution to the attractiveness of this area.



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Main Street Existing Character

The photograph and diagram on this page illustrate fundamental design qualities of Main Street and underpin the guidelines that follow.



1. Buildings frame the street edge
2. Two storey street wall
3. Fine grain with narrow storefronts
4. Transparency at ground level
5. Frequent entrances
6. Signage within a sign band

Port Dover Urban Waterfront

The Urban Waterfront has a rich history as a commercial fishing port that continues today, particularly on the east side of Lynn River with both a land and water based function. The lands along the Lynn River to the north of the harbour are no longer used as part of the commercial fishing industry and are in the process of being redeveloped for residential uses. The west side of the harbour is a key destination for residents and visitors with a hub of restaurants and shops that cater to beach goers.



The Urban Waterfront includes two piers marking the entrance to the Lynn River. The Port Dover Lighthouse is at the end of the West Pier, and is a popular spot to visit. The West Pier includes seating and safety railings along its length. Just to the east is the commercial fishing basin and its associated port area. This working waterfront area is an important economic driver for Port Dover and provides an authentic, industrial character along the lake.



Further east is the Port Dover Harbour Marina with approximately 400 slips and a commercial basin used by the Coast Guard and Ministry of Natural Resource and Forestry.

Buildings in Port Dover's Urban Waterfront area are simple, functional buildings. There are many rectangular structures with peaked roofs and gable ends, clad in wood or metal siding. Public areas along the water's edge use durable, functional materials such as steel and concrete in paving, walls, bollards, railings and furniture.

2.2 General Building Guidelines for Downtown and Urban Waterfront

This section applies to all sites in the Downtown and Urban Waterfront. Additional guidelines in this document, for example, for parks, streets or active transportation, may also apply within the Downtown and Urban Waterfront.

Height & Massing

- 1 Taller buildings may be encouraged at strategic locations such as corner sites and other prominent sites to enhance community structure, sense of place and provide landmarks.

Placement & Orientation

- 2 Where more generous setbacks are appropriate, such as along streets with residential uses, the space should be used for landscaped areas, additional street tree planting, amenity areas, seating, display areas or sidewalk cafés and patios.
- 3 Along commercial street frontages, modest setbacks (typically 1 - 2 metres) could be provided where the setback provides public amenity or benefit. This setback zone should be designed as an extension of the sidewalk zone. It could accommodate outdoor cafe or patio space, retail display, seating, additional street trees, and/or other amenities. When not in use for patio or retail display, the setback zone should be accessible to the public as a seamless extension of the sidewalk.

Street Wall / Facades

- 4 Building materials should reflect and complement the existing materials in the area and should be high quality, durable and easily maintained.
- 5 The materials selected should be consistent for a building's facade and any walls that are publicly visible.



Port Dover • Materials

6

6 Recommended building materials include brick, stone, wood and glass. One or two of these materials should be selected as base materials and may be complemented by a wider range of accent materials.

7 Design may incorporate contemporary materials, patterns and textures where materials and fastening systems are authentic to their purpose and neatly detailed.

8 Changes of material will be purposeful and coincide with substantial massing elements or datum lines of the building.

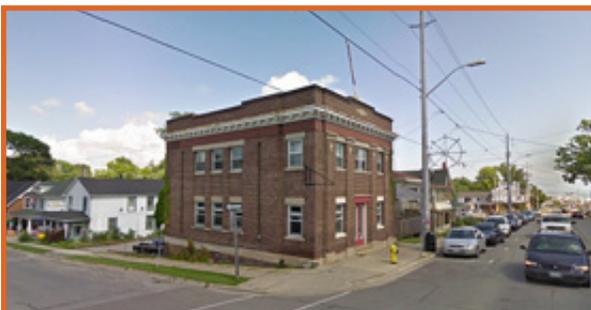


9

9 Where building sides are exposed to public view, the materials on the front facade should wrap around and extend a minimum of 2.0m on the side elevation to avoid the appearance of an 'applique facade,' where the front facade appears insubstantial or tacked-on.

10 The principal material of exposed side facades may differ from the front facade, but should be compatible with it. The side facade treatment should reference key architectural datums such as cornices, floor lines, or vertical rhythms.

11 Spaces between buildings (such as alleyways) should be well-lit and enhanced through fenestration, side entrances, decorative signage, decorative paving, and public art.



Port Dover • Treatment

13

12 Architectural features and decorative elements such as shutters, cornices, awnings, building projections, distinctive roof features, etc. which add visual interest are encouraged on all buildings.

13 Additional architectural treatment is encouraged for corner sites and landmark buildings to enhance the visual prominence of these buildings and their locations.

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- 14** The ground floor should be articulated in a manner that distinguishes it from upper storeys, for example, through canopies, awnings, lighting and signage.
- 15** Blank walls should not be permitted on any building fronting a street.
- 16** Renovations of or changes to existing historical building designs, or new designs in historical styles, should be designed or reviewed/approved by registered, qualified architects who have experience in designs of that stylistic expression to ensure building massing, proportion, and details exhibit integrity to historical usage.
- 17** There should be a consistent colour palette for a building's facade and any walls that are publicly visible. The colour palette should complement the building expression and/or business. Colours that enhance the individuality of a building or business, while complementing the building character of the streetscape, are encouraged.
- 18** Mechanical equipment should be screened from view.

Shopfront Signage

19 Signage should be located and designed to reflect the heritage character of the streetscape, while allowing for the creativity of individual businesses. Diversity in storefront signage should be encouraged to create shopfronts with "personality."

20 Artistic expression and imagination are encouraged.

21 Signage should be attractive, durable, easy to read and complementary to the overall facade design.

22 Sign scale should be in proportion with the building and should reflect the pedestrian scale of the streetscape.

23 Signs should be located outside the pedestrian clearway and, when located over pedestrian areas, should have a minimum clearance of 2.4m from grade.

24 Signage should not obscure windows, doors or architectural features.

25 Fascia signs within a sign band, window signs and signs hanging perpendicular to the building facade are preferred. Sidewalk retailing and sandwich board signs are also encouraged to create vibrancy and interest along the streetscape.



Shopfront Awnings, Canopies and Lighting

26 Awnings and canopies are encouraged for weather protection and shelter, for additional signage opportunities and for the aesthetic appeal of a facade. Retractable awnings are preferred as they are flexible for diurnal and seasonal changes.

27 Awnings and canopies should align with modules of the building's vertical rhythm, such as the space between column or pilaster elements, or the divisions between windows and doors.

28 The design, shape, colour and material selected for awnings and canopies should be complementary to the design of the building's facade and should reflect the character of the context.

29 Signage on awnings should be located on the valence.

30 Awnings should not obscure windows, entrances or architectural elements on a facade, or impede views down a street.

31 Creative exterior and shop window lighting is encouraged to promote vibrant streetscapes at night, encourage pedestrian traffic and enhance the safety of the pedestrian experience.

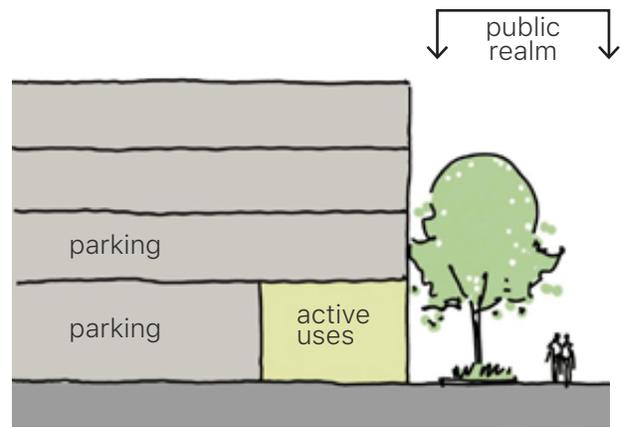
32 Awnings, canopies and lighting should have a minimum clearance of 2.4m from grade to allow for unencumbered pedestrian access underneath.



Parking

33 For large developments, the preferred parking location is underground.

34 Above ground structured parking located **at ground level** should be hidden from public view on all sides facing public streets and the water's edge. Habitable space within the building should be located between the structured parking and the public realm. The habitable space should face the public realm inclusive of windows and entrances. The habitable space could include, for example, retail uses, or portions of residential dwellings.



34 35

35 Above ground structured parking located **above ground level** may be located on the exterior, public-facing facade of the building provided it is well-integrated with the building design. The principal, high quality massing and facade design, inclusive of columns, datum lines, materials, projections, solid/void relationships and other design elements, should be continuously applied across any **above ground level** parking visible from adjacent streets or the water's edge.

36 Surface parking should be small in scale and located internal to sites where it is screened from view of the adjacent streets or the water's edge by the principal building massing.

37 Additional landscaping should be provided to screen parking where parking elements (garage entrances, structured parking, parking lots) are visible from public streets, the water's edge, courtyards or walkways.



35

Parking on upper levels. Facade treatment of upper levels is high quality and consistent throughout entire building expression.

2.3 Urban Waterfront Character

The Port Dover Urban Waterfront has a distinct and established character in its recreational, tourism, industrial and port uses and buildings. New development should be compatible with and reinforce the existing nautical character.

- 1 Building and site elements should incorporate design strategies such as:
 - a. simple, rectangular building shapes;
 - b. peaked roofs, shed roofs or flat roofs; avoid hip roofs;
 - c. gable ends;
 - d. horizontal, vertical or shingle siding in wood, cement board, high quality vinyl or metal;
 - e. standing seam metal roofs;
 - f. simple trim elements such as , frieze and fascia boards, and window surrounds;
 - g. public art or signage that reinforces the nautical context;
 - h. simple, durable marine-grade site furnishings and details for lighting, awnings, railing, bollards, seating and the like.



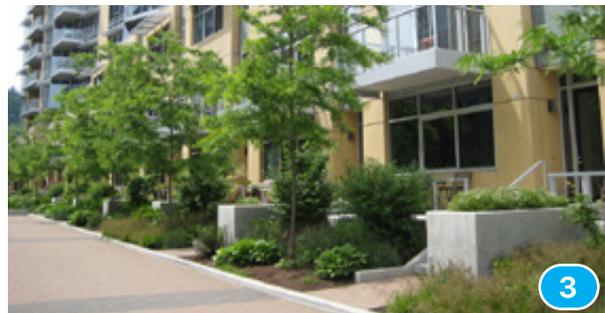
2.4 Water's Edge Sites

Lake Erie and the Lynn River are fundamental to the history, existing character and ongoing activities of Port Dover. The water itself and the publicly accessible edges along the shorelines are key public spaces in Port Dover and define its image. Buildings along the water's edge help create the first impression of Port Dover's beaches, walkways and streets that are so beloved by residents and tourists. It is critical that the buildings along the water's edge reinforce a high quality image and frame the public realm. The following Guidelines apply to all properties within the Downtown and Urban Waterfront that are adjacent to Lake Erie or the Lynn River.

New buildings should not overwhelm these public spaces by abrupt increases in height over the established character of the community. They should look out onto the waterfront with active uses and use of transparency in their facades. It is recognized that new buildings may be taller as the waterfront intensifies, however, new buildings should be shaped by the following guidelines.

Site Organization

- 1 A clear pedestrian zone should be provided as near the water's edge as possible that provides for linkages to existing or planned future pedestrian zones on adjacent properties. Over time, pedestrian zones will link together to provide continuous public access to the water's edge.
- 2 Large sites should provide outdoor, ground level, accessible connections between the water's edge pedestrian zone and an adjacent public street. This can take the form of a landscaped walkway, a publicly accessible street or lane, a publicly accessible privately owned open space, a park, or other means.
- 3 Where buildings have residential units at ground level, provide a 'front yard' landscape zone between the unit and the pedestrian zone to include demarcation of the private realm front yard space, for example, with a low wall, low permeable fence, and/or landscaping.
- 4 Locate parking and servicing functions to the centre of the site. Minimize interruptions for access, servicing or mechanical equipment along publicly accessible frontages.



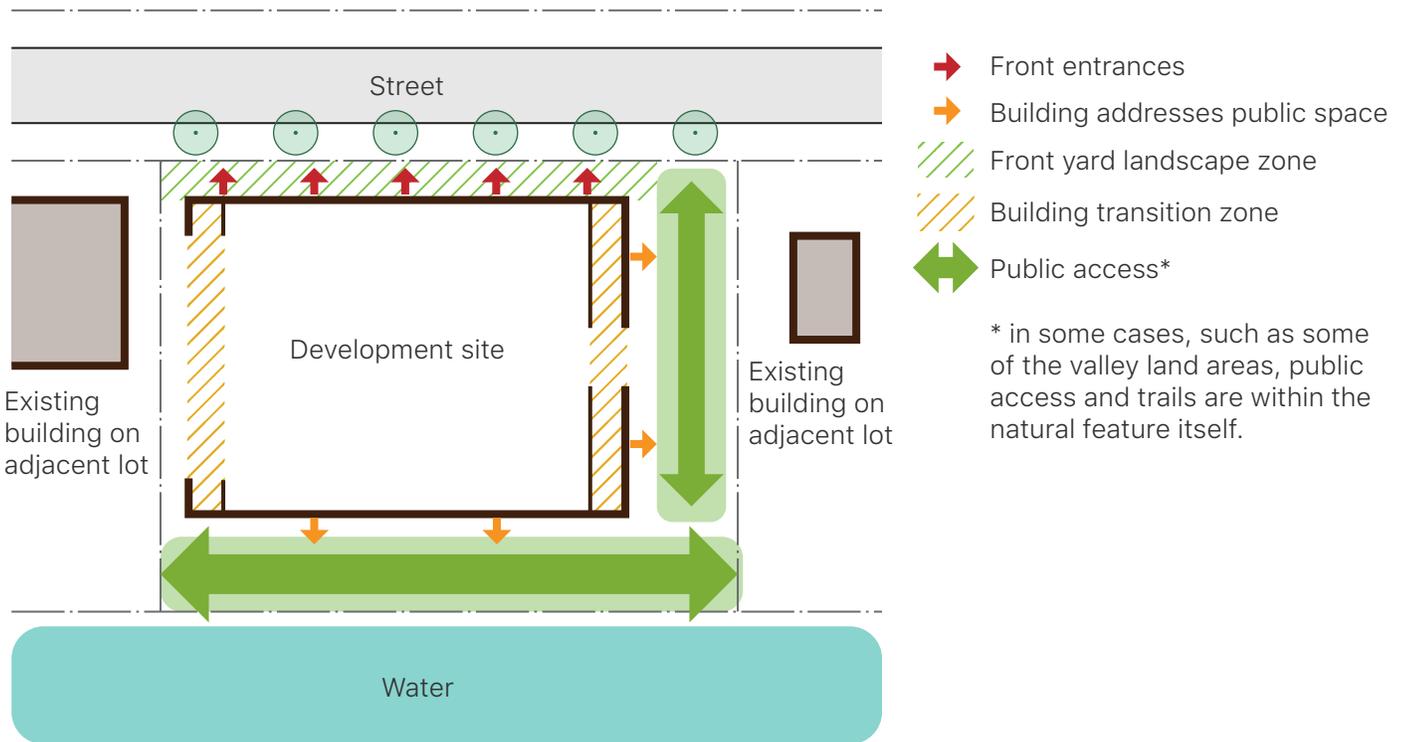
Ground floor units with demarcated front yard landscape zone and direct access to adjacent walkway.

Building Design

- 5** Ensure new building(s) have a transition to existing buildings on adjacent lots through setback, step back, street wall height, and overall building height within a transition zone adjacent to those lots.

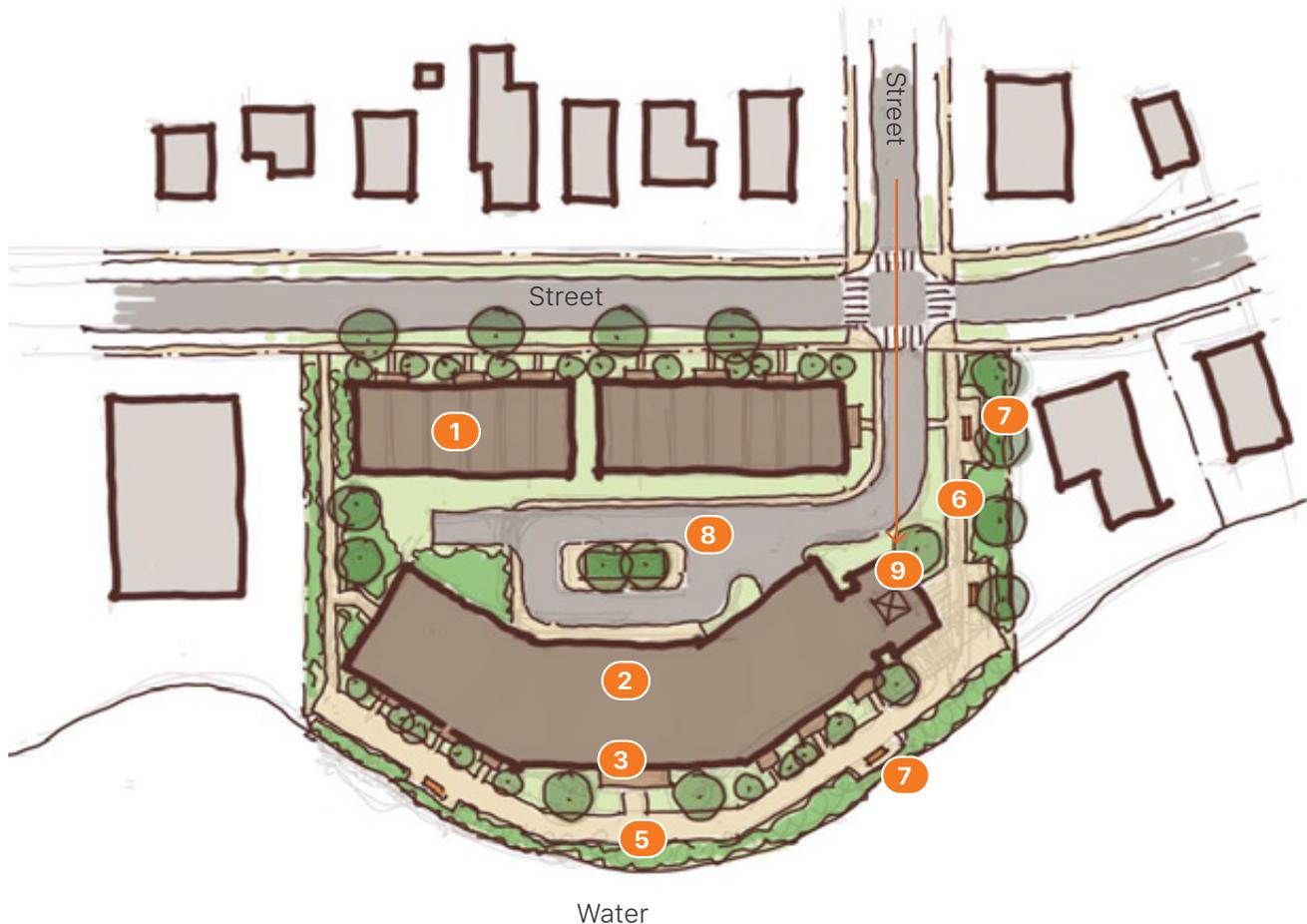
- 6** Buildings facing the water's edge should be designed with a high quality 'frontal' appearance consistent with the other built form guidelines in this section that apply to buildings facing public streets and spaces. In particular, the ground level should be designed to maximize the following design elements:
 - a.** active uses at ground level facing the water's edge, such as commercial/retail, residential units, or common interior spaces such as lobbies and amenity areas. Servicing and utility areas should be minimized;
 - b.** direct building entrances facing the water's edge;
 - c.** a high degree of transparency: windows and doors;
 - d.** for residential units at ground level, entrances to individual units with walkway connections to the pedestrian zone walkway/trail.

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Demonstration Concepts

Showing how the application of these Urban Design Guidelines can shape the conceptual design of the public realm and built form



1. Townhouses face public street and low-rise streetscape; direct entrances to individual units
2. Taller apartment building set back from street
3. Main lobby entrance along water's edge walkway
4. Ground level apartments have direct entrances to public walkway
5. Public walkway along the water's edge extends to site boundaries for future extension to adjacent sites
6. Public walkway links public street with water's edge public walkway
7. Amenity areas and seating along public walkways
8. Drop off, servicing and parking entrance internal to the site
9. Building massing responds to view terminus location from public street

Demonstration Concepts

Sketches developed at the public workshop showing how the application of these Urban Design Guidelines can shape the conceptual design of the public realm and built form

Waterfront site at the foot of St. George, Walker and Harbour Streets



1. 'Shared street' approach to water's edge rights of way
2. Public views and access to waterfront maintained
3. Buildings frame the street edge
4. Parking and servicing functions located internal to the block

Top: St. George Street right of way maintained

Below: St. George Street right of way partially closed, new water's edge street connects Walker Street and Harbour Street



3 Existing Neighbourhoods

3.1 Introduction

Port Dover has existing neighbourhoods that span over a century of urban growth. There is a wide range in the character of buildings, landscape and streetscape conditions.

Some existing neighbourhoods are older, with narrow roads, mature trees, eclectic buildings (many cottage-like), and a strong definition of front and/or side yard gardens. Other neighbourhoods are newer, with standard local roads and housing in bungalow forms. While different, each neighbourhood has a distinct character that must be understood. The guidelines in this chapter seek to ensure that character is maintained with new development or redevelopment.

The Existing Neighbourhoods Guidelines apply to significant change to sites, lots and buildings within existing neighbourhoods. Examples of the types of change that are addressed by the following Guidelines include:

- facade remodelling;
- major additions;
- replacement dwellings;
- additional dwelling units on a lot;
- change in land use or new use;
- lot assembly; and/or
- significant intensification.

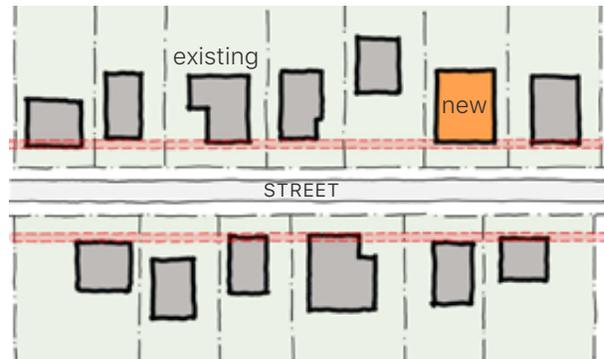
Guidelines in this chapter will most often be used for building additions, remodelling and reconstruction on a lot. Sometimes, the extent of change will be greater. Large sites with multi-unit buildings should consider the principles of the Downtown and Urban Waterfront Design Guidelines in addition to these Existing Neighbourhoods Guidelines.

3.2 Design Guidelines

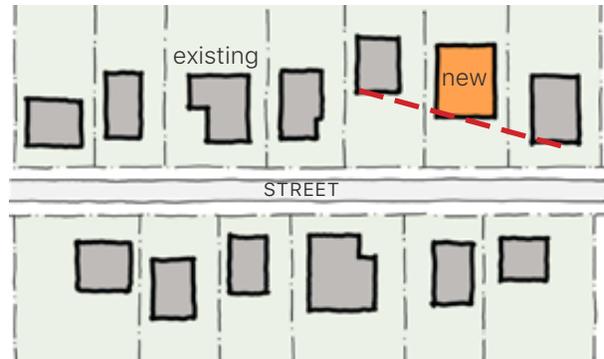
Site Organization

Placement & Orientation

- 1 Building placement and siting on a property should be considered in relation to the street and its neighbours to reinforce the positive characteristics of the existing streetscape.
- 2 Maintain consistent setbacks along the street. New development should have a set back equal to the predominant setback (70%+) on the street (+/- 1.0m), or a distance that is the average of those on either side of the development site (+/- 1.0m).
- 3 Where there is no predominant setback along a street, new development should be set back a distance that is the average of those on either side of the development site (+/- 1.0m).
- 4 Generally, locate dwellings close to the street edge to frame the streetscapes, however, this will depend on the setbacks to houses on either side of the site.

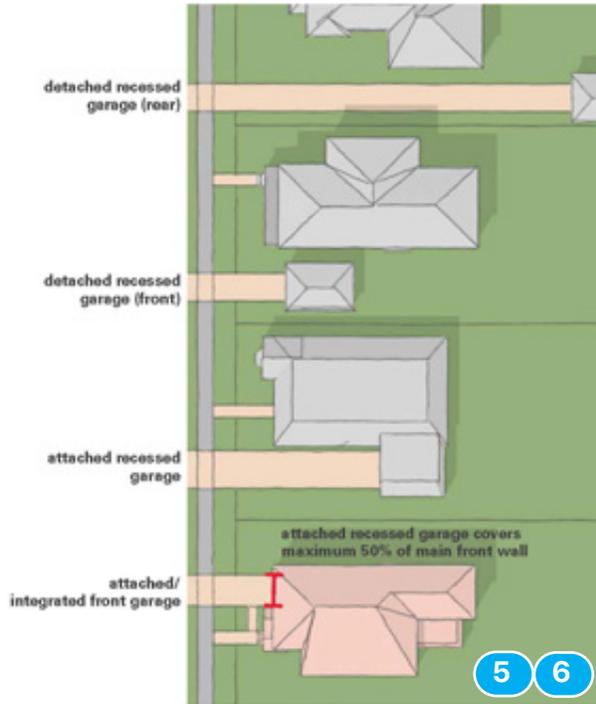


New development is set back at the predominant set back along the streetscape.



New development is set back the average of set backs on adjacent properties.

1 **2** Placement and Orientation



Access & Parking

- 5 Garages should be placed behind the front wall of the dwelling or at the side or rear of the lot, unless the predominant location (70%+) of the garage on other houses on the streetscape projects from the front of the house or is not at the side or rear.
- 6 Front-facing garages attached to the main dwelling should not occupy more than 50% of the building's width.
- 7 Driveways should be located and spaced to reinforce the rhythm along a street and to allow for street trees to be planted in the boulevard.

Landscaping

- 8 Include landscaped areas in front of buildings that provide a transition from private to public areas. A minimum of 50% of the front yard zone should include soft landscaping areas (non-paved areas supporting grass, groundcovers, trees and/or shrubs).
- 9 Where the predominant (70%+) existing streetscape character has design elements such as low stone walls, low permeable fences, planting and/or other landscaping at the front of the lot, new development should provide similar elements.

Building Design

Massing & Elevation Articulation

10 Ensure new building is generally consistent in height and massing with adjacent buildings along the streetscape.

11 Provide appropriate transitions in height to/from existing adjacent buildings and ensure no new building is more than 1.5 storeys or 4.5m higher/lower than the adjacent dwellings, subject to the area's maximum height permission.

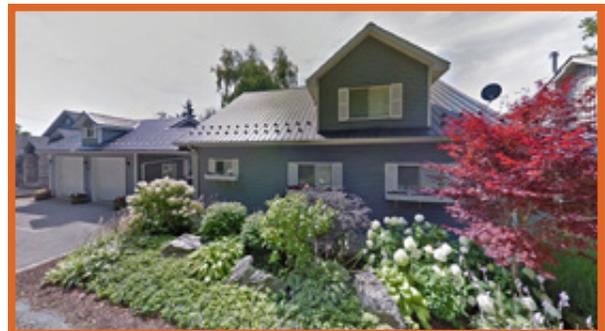
12 Where possible, maintain the existing lot grading and the neighbourhood's characteristic first floor height.

13 Design the building envelope, and individual architectural elements within the building, to reference the architectural treatment of buildings in the neighbourhood. The goal is not to replicate buildings of the neighbourhood, but to ensure new development relates to them by incorporating similarities in design language to promote compatibility. Massing and architectural elements to be considered include:

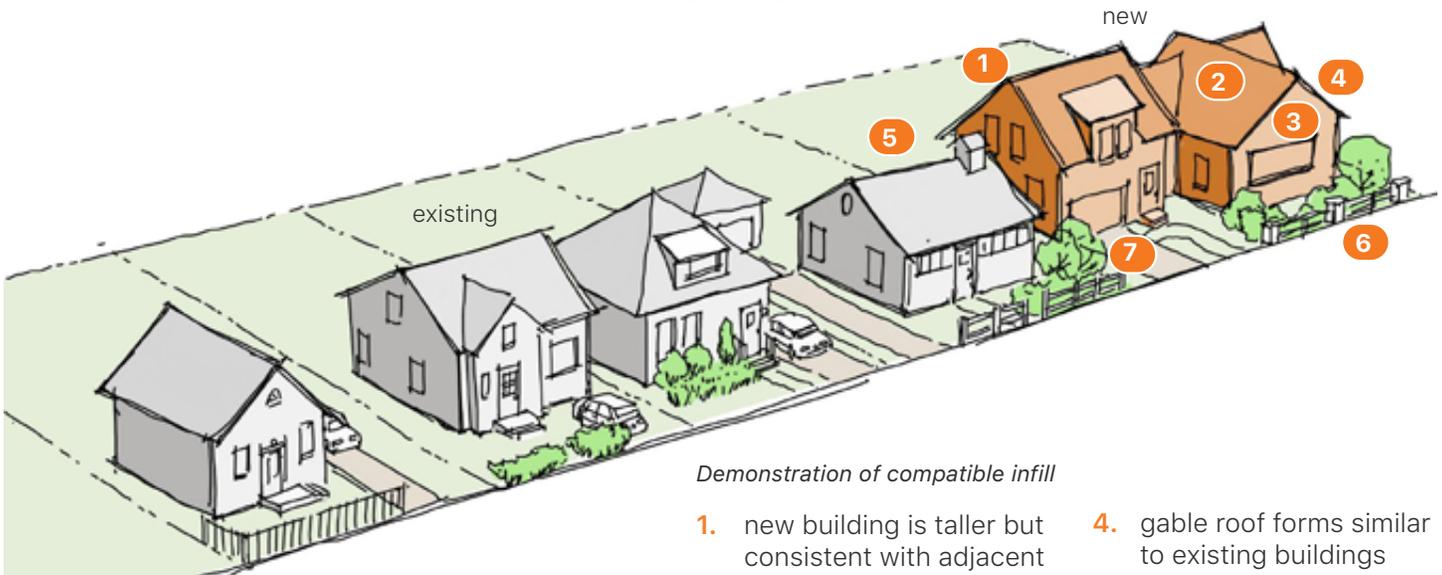
- a. similar building shape (square, rectangular, L-shaped, etc.);
- b. roof lines with similar massing, pitches and articulation (gable, hipped, shed, flat, use of dormers, etc.);
- c. similar principal building massing elements (bays, projections, first floor height, building height, entry features, etc.);
- d. similar architectural features (porches, stoops, chimneys, columns, frieze boards, etc.);
- e. important datum lines (cornice, base courses, string courses, window alignment, bays, etc.);
- f. similar proportions (bays, windows, garage, etc.).



Example of new development that is compatible because of set back, massing, sloped roof form, material, and entrance prominence. Difference in architectural style is not as important.



Port Dover • Compatible new development



Demonstration of compatible infill

1. new building is taller but consistent with adjacent buildings
2. massing of new, larger building is subdivided into smaller volumes that relate to the existing buildings on the street
3. the portion of the new building at the street edge has a similar proportion and width to the existing buildings
4. gable roof forms similar to existing buildings
5. second storey is partially within roof massing, similar to existing buildings
6. New landscaping defines a front yard zone similar to existing yard treatments
7. garage is set back

14 Articulate facades to reflect the rhythm and proportion of solids/voids, walls/windows, of neighbouring dwellings.

15 Ensure the elevation design reflects that of the adjacent homes. The horizontal expression of the windows and door should be similar to surrounding homes and their vertical rhythm should reflect those of adjacent homes.

16 Design semi-detached unit facades as one elevation.

17 Ensure facade details throughout all building elevations are consistent with their intended architectural expression.

18 Avoid mixing historic architectural elements with other architectural styles elements.

19 Historical designs should only be undertaken by registered, qualified architects who have experience in designs of that historic period.

20 Contemporary designs may be considered provided they exhibit consistency with the massing and articulation guidelines in this section.

21 Traditional designs may be considered where they are found in the neighbourhood.

Materials

22 Building materials should reflect and complement the existing materials in the area and should be high quality, durable and easily maintained.

23 The materials selected should be consistent for a building's facade and any walls that are publicly visible.

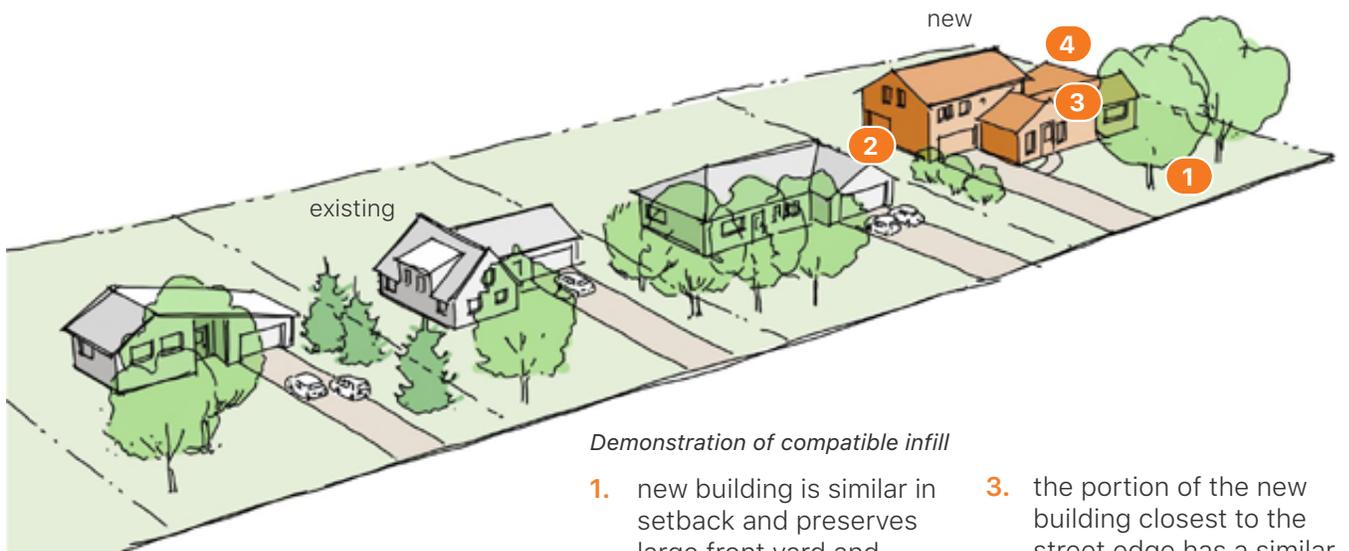
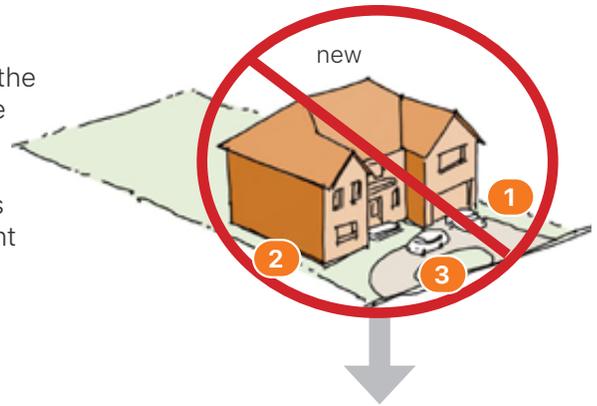
24 Recommended building materials include brick, stone, wood, glass and concrete; one or two of these materials should be selected as base materials and may be complemented by a wider range of accent materials.

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Incompatible infill

New building is dissimilar to the well-defined character of the existing context:

1. small front yard setback
2. small side yard setbacks
3. driveway dominated front yard

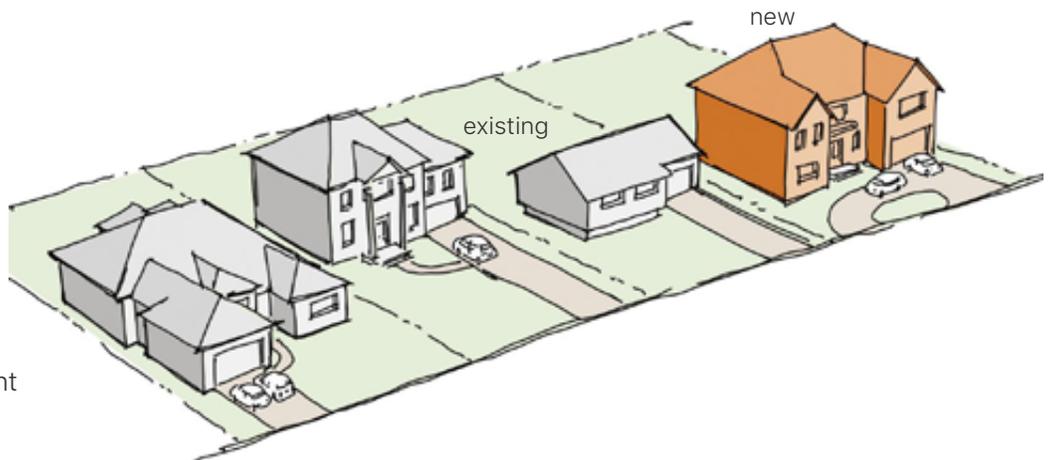


Demonstration of compatible infill

1. new building is similar in setback and preserves large front yard and mature trees
2. new building is similar in side yard setback
3. the portion of the new building closest to the street edge has a similar low profile to the existing buildings; taller portions are set back
4. similarity in massing and roofline elements

Compatible infill

The same building as shown at top may be compatible infill in a context that is less well defined and exhibits smaller setbacks, more varied or larger massing, less defined landscaping, few trees, and more recent architectural styles.



4 Future Neighbourhoods

4.1 Introduction

This chapter will be most relevant to low-rise housing forms such as single detached, semi-detached, townhouses, and stacked townhouses. New neighbourhoods may also include mixed uses and other non-residential uses, and these are covered by other sections of these guidelines.

Greenfield neighbourhoods in Port Dover will continue to be defined by the natural features that surround them and be connected to the overall community through their respective main spines, and green space and trails.

4.2 Design Guidelines for Greenfield Neighbourhoods

Site Organization

Lotting

- 1 Provide a transition in lot sizes, setbacks, massing, and grading that complements the existing and planned context.
- 2 Promote a variety of lots and building forms along streetscapes.

Placement & Orientation

- 3 Create consistent edges along public streets and open spaces.
- 4 Promote multi-storey buildings that contribute to a sense of enclosure along the street, particularly at locations.
- 5 Minimize the visual impact of long blocks; turn lots located on the end of blocks 90-degrees to face the perpendicular road, where appropriate. Consider a variety of lot facing conditions, in addition to flankage lots, along long stretches of road.

- 6 Orient lots and built form to front streets that are primary neighbourhood entry locations from the surrounding arterial and collector road pattern. Orient lots and built form to address primary internal neighbourhood streets such as collector roads.
- 7 Orient dwellings to face the public realm including streets, pedestrian connections and open space, to increase casual surveillance and foster a sense of increased safety.
- 8 Limit townhouse blocks to 8 units/modules.
- 9 For fronting townhouse buildings facing open spaces or common private lanes/ roads, provide a minimum 15.0m separation distance between buildings (front to front).
- 10 Avoid property and lot configurations that lead to front yards facing or being adjacent to rear yards or other non-frontal areas such as driveways, parking lots, or servicing areas.

Access & Parking (AP)

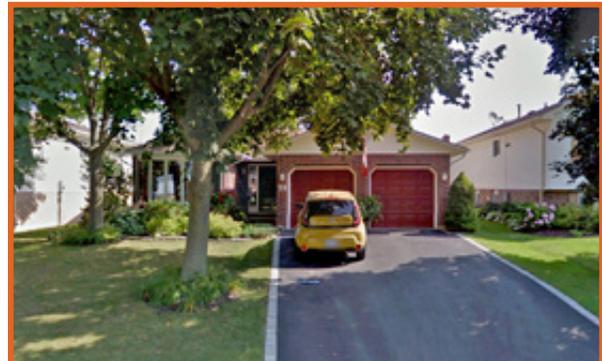
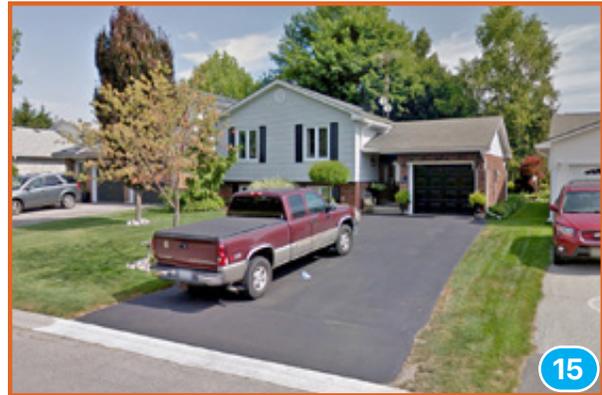
- 11 Locate driveways away from 'T' intersections and s, and on lots, encourage driveway / garage access from the side street.
- 12 Locate driveways away from parks and open spaces.
- 13 Pair driveways at a common property line, where possible, to allow for greater opportunities for landscaped/grassed areas along the streetscape and front yards, and allow sufficient space for on-street parking.
- 14 Provide a minimum 6m separation between driveways where driveways are not paired to allow for street tree planting and on-street parking.

15 Minimize the visual impact / presence of front integrated garages on the streetscape:

- a. Recess the front wall of the garage from the main building wall;
- b. Limit front integrated garages to 2 cars and ensure that the overall width of the garage doors do not exceed 50% of overall width of the house;
- c. Ensure the driveway width at the street is not greater than the width of the garage, to a maximum of 6.0m;
- d. Provide glazed door panels on all garage doors;
- e. Second storey portions of the house above the garage should not be stepped back from the front wall of the garage more than 1.5m.

16 For townhouses, provide a walkway from the front entrance of dwellings to the sidewalk.

17 For townhouses, design walkways to be shared between adjacent townhouse units through the use of a common landing between units, leading to a singular walkway.



Port Dover • Garage width

15



16



Garbage / Utilities

18 Minimize the visual impact of garbage and utility areas on the street by planning for integrated garbage and utility solutions, for example:

- a.** deeper garages;
- b.** side yard garbage bin storage screened by a gate or enclosure;
- c.** consolidated garbage facilities for townhouse blocks screened with architectural and landscape elements;
- d.** locating utility metres, vents, HVAC equipment, etc. in the interior side or rear yard, integrating them with the design of the building, or screening them with walls and landscaping.



Fencing

19 Provide high quality wood privacy fencing on all lots where the rear yard or private amenity space is exposed to the street / public space:

- a.** Include an access gate;
- b.** Locate the fence entirely within private property.

20 Provide wood privacy fencing for along the rear lot line of lots backing onto non-residential uses, and locate the fence within the non-residential use side.

Building Design

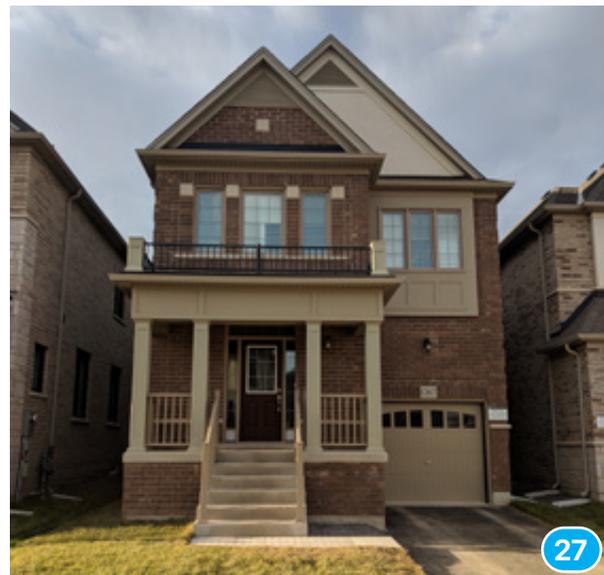
Massing & Elevation Articulation

- 21** Incorporate a variety of architectural expressions including designs, models and elevations along the streetscape, including contemporary designs that may be located alongside 'traditional' forms, subject to design, massing and context.
- 22** Discourage designs that incorporate different/disparate architectural expressions and stylistic elements within a single dwelling or building.
- 23** Ensure facade details throughout all building elevations are consistent with their intended architectural style.
- 24** Discourage design elements that are superficial, decorative, or ornamental add-ons that are intended to evoke an architectural style that is not reflected in the principal building design and massing, for example, faux Victorian scrollwork on an otherwise contemporary design.
- 25** Take design cues from local architectural expressions.



Entry Elements

- 26** Orient front entries to the street, or any adjacent publicly accessible open space that has pedestrian activity such as a walkway or park.
- 27** Use entry elements such as porches, arches, generous overhangs and massing elements such as a cantilevered upper storeys or recesses, to articulate front elevations at entrances.
- 28** Ensure steps are designed as an integral components of the elevation.
- 29** Ensure porches are sized to create usable space.
- 30** Provide municipal street numbers (address) that are visible and legible from the street.



Port Dover • Entry emphasis

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Windows

- 31 Maintain consistent window treatment throughout individual buildings.
- 32 Locate windows to maximize daylight and reduce need for indoor lighting.

Roofs

- 33 Encourage a variety of roof forms such as cottage or hipped roof, front gabled, side gabled, cross gabled, mansard and flat roofs; ensure roof forms are consistent with the architectural style of the dwelling.



Port Dover • Window consistency

- 34 For townhouse blocks, emphasize individual units through the articulation of roof lines (e.g. variations in roof slopes at end units, dormers, differing roof pitches, etc.) while maintaining a consistent roof style throughout the same block.

- 35 Avoid decorative dormers that do not have habitable space within them.

- 36 Locate stacks, gas flues and vents on the rear slope of the roof where possible.

- 37 Encourage skylights and solar panels, where present, to be designed as integrated components of the building or roof and away from public view.



Port Dover • Roof line variety

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Materials

38 Ensure materials reflect and complement the architectural style of the building.

39 For traditional styles, provide a variety of high quality and complementary wall cladding materials including brick masonry, stone, stucco, and cementitious siding.

40 For contemporary styles include materials such as brick masonry with smooth finishes, high quality stone cut to larger calibre units, corrugated steel panelling, marble, metal, concrete and metal roofing.

41 On exposed elevations, ensure material changes occur at a change in massing or has a substantial material return.

42 Ensure colours are consistent and coordinated throughout a building design and match the architectural style.





Example of priority lot locations.

- C** - lots in prominent locations
- OS** - parks/open space lots
- T** - T lots at the end of a view corridor
- E** - elbow lots

Priority Lots

Priority Lots are located in prominent locations and/or are highly visible from the public realm, including:

- Lots in prominent locations;
- Parks / Open Space Lots;
- Window Street Lots;
- 'T' and 'Elbow' Lots; and
- End Units (in the case of townhouse blocks).

43 Provide highly articulated elevations that include changes of plane, substantial window openings and upgraded architectural detailing and materials.

44 Include gables, dormers and/or bay windows, and decorative panels/louvres, where appropriate.

45 Provide wrap around windows, porches and other architectural treatments at conditions. Locate active living spaces at the /exterior side elevation.

46 Design treatment of the exterior side elevation should be equal to that of the front elevation.

47 Use stone or other upgraded materials as the main building material for gateway dwellings.

48 Provide landscaping and upgraded lot fencing.

49 Locate driveways / garages away from the terminus view; pair the side yards of visible lots.

50 Consider flankage entrances on lots.

5 Commercial & Institutional Uses

5.1 General Guidelines for All Commercial and Institutional Sites

The following design guidelines apply to buildings that have non-residential uses in them. They aim to create vibrant and street focused built form that is compatible with its surroundings and coordinated, pedestrian-scaled public spaces that contribute to place-making.

Site Organization

- 1 Provide for appropriate transitions to adjacent neighbourhoods and different land uses (i.e. setbacks, landscaping, location of servicing and parking areas).
- 2 Create a pedestrian-scaled, permeable and connected internal layout (block and street pattern) and arrange buildings to create comfortable and protected pedestrian spaces that have a sense of enclosure. A sense of enclosure is created when both sides of a street (or the edges of a public space) are well-defined by building fronts.
- 3 Integrate existing topography and natural features into the development, and minimize alteration to both, wherever feasible.
- 4 Provide a connected street and open space network, including a pedestrian/cyclist system that encourages active transportation.
- 5 Organize the site to enhance wayfinding including buildings as gateways and landmarks, public spaces as focal points, streetscapes that frame significant views.

Placement & Orientation

- 6 Locate buildings along primary streets, at or near the street line.
- 7 Avoid large areas of surface parking between the main building wall and the street; limit front yard setbacks to 6.0m.





- 8 At corners, locate the longer building wall parallel to the primary street frontage.
- 9 Orient buildings to face the public realm, in particular any adjacent / adjoining streetscape, pedestrian connection and open space.
- 10 Arrange buildings to frame views / vistas, parks and open spaces.
- 11 Arrange buildings to allow for patios and spill out areas which animate the site/street.
- 12 Locate active uses at the base of buildings and on all elevations fronting onto public spaces (i.e. streets and open spaces).

Access & Parking

- 13 Provide a safe, clear and accessible site circulation system for pedestrians, cyclists and vehicles, including visible access points and connections to the surrounding street network, public sidewalks, and parking areas.
- 14 Minimize interruptions to the sidewalk and potential conflict between vehicles, cyclists and pedestrians; provide barrier free, landscaped pedestrian connections from, to and through parking areas.
- 15 Provide prominent and easily accessible entry points to the site.
- 16 Provide direct access to at-grade uses from sidewalks and parking areas.
- 17 Locate parking areas away from the street frontage, preferably where they are screened from view by the building massing, or use landscaping and other structures to screen them.
- 18 Ensure CPTED principles are applied to parking areas including clear views within the parking areas, multiple points of pedestrian and/or vehicular entry, well-defined pedestrian routes, and the ability to be overlooked by adjacent buildings.

19 Design surface parking to minimize environmental impact by reducing parking lot/garage size, considering shared parking facilities with adjacent buildings and providing preferential parking for fuel efficient vehicles.



20 Avoid large areas of surface parking:

- a. Disperse surface parking throughout the site;
- b. Utilize low impact design principles, permeable paving materials, and reduce heat island effect through light materials or canopy coverage;
- c. Incorporate landscaping within parking areas (aim for 20 to 30 percent of the parking area).

21 Parking structures should be integrated with and/or behind principal buildings.

22 Parking structures along street/public frontages should be lined with active uses at grade and include enhanced articulated elevations.



23 Provide accessible and secure bike racks and parking at retail, commercial, and employment area developments, as well as at key nodal locations to promote purposeful cycling.

Servicing & Loading

24 Locate garbage/recycling, loading and service areas to the rear or side yards, away and fully screened from public view, abutting residential areas, major roads and open spaces.



25 Integrate garbage/recycling functions within buildings wherever possible.

26 Incorporate garbage storage bins that can be accessed for garbage pick up into the principal building design; ensure food waste is stored in climate controlled rooms.



27 Provide on-site recycling facilities for handling, storing, and separation of recyclables for large developments, such as multi-unit residential buildings, employment and office buildings, and institutional or public buildings.

28 Loading/garage doors should not face the public street / space.

Landscaping

29 Provide an enhanced public realm interface along the street, including landscaped areas, open space, gathering areas in association with front door(s) and/or walkways.

30 Design landscaping within the private areas and public interface to be coordinated and to enhance the character of the development and the neighbourhood.

31 Design landscaping to reinforce the structure of the site with a focus on creating a safe, comfortable and animated pedestrian environment – including streets, edges, gateways, transitions, public spaces, and building entrances.

32 Design landscaping to enhance and contribute to the broader environment including ecological function, stormwater management functions, urban forest, and bio-diversity.

33 A comprehensive strategy for planting, built features, fencing, walls, paving, lighting signage and site furnishings shall be provided.

34 Distinguish walkways from driveways through a material change and/or planted/sodded edge.

35 Use high-quality, durable materials for paving, walls, planters, site furniture, shade structures, etc.

36 Design fences and walls to be coordinated with building designs.

37 Use berms in landscape strips to minimize views/noise from adjacent uses, parking, loading and service areas.

Building Design

Massing & Elevation Articulation

38 Ensure massing and design is compatible with and transitions to the surrounding neighbourhood character.

39 Encourage multi-storey building designs wherever feasible and appropriate.

40 Incorporate prominent building massing and special architectural elements at intersections, s and gateways.

41 Encourage a range of design expressions to promote architectural variety.

42 Provide a high-degree of articulation on building elevations that face onto streets and public spaces, through design elements such as changes in plane, fenestration, projections, relief, horizontal and vertical elements.

43 Establish a rhythm of minor breaks or wall articulation along the facade, distinguishing one unit (retail) or building component from the next. When selecting the rhythm, scale and proportion, take cues from adjacent buildings.

44 For buildings located at s, design building elevations to equally address the two main street frontages; prominent massing, height, architectural elements and detailing should be used to emphasize these locations.

45 For sites adjacent to highways, provide the same degree of building articulation on all exposed elevations, in addition to the primary elevation.





- 46** Avoid blank, uninterrupted walls and false frontages along streets and open spaces.
- 47** Coordinate the design of ancillary buildings and structures with that of the principal building(s); height, massing, architectural details, lighting, signage, materials, and colours.
- 48** Provide main building entrances in prominent and highly visible locations, and oriented to primary streets.
- 49** Ensure building entrances are accessible, safely and clearly connected to the sidewalk and parking areas.
- 50** Concentrate the highest degree of articulation at entrances and along main building elevations.
- 51** Ensure elevations along streets include a significant amount of glazing.
- 52** Provide weather protection elements at entrances and along highly pedestrian edges.
- 53** Incorporate architectural elements to enhance the pedestrian environment – canopies, overhangs, awnings, projecting display windows, arcades, colonnades, etc.; these elements should be designed as integral parts of the building in terms of form, style, materials, colours, etc.
- 54** Screen roof top mechanical equipment from view through the use of architectural screens, parapet walls and/or integration into the design of the building.

Materials & Lighting

- 55** Coordinate building materials among buildings on a site and ensure they reflect, complement and enhance the building's architectural style and use.

56 Use high-quality, durable exterior building materials; avoid reflective and mirrored spandrel glass.

57 Provide a high level of visual transparency (glass) at eye level for lobbies, main frontages and prominent entrances.

58 Provide an overall lighting strategy that coordinates site, building and landscape lighting to ensure pedestrian safety and comfort.

59 Lighting design should minimize light spill over into residential neighbourhoods.

60 Consider lighting powered by alternate energy sources such as solar power.

Signage

61 Provide an overall signage strategy that coordinates the site and buildings within a multi-tenant site.

62 Integrate signage to the building design and ensure it complements the building's elevation, animate the ground level and enhance the streetscape.

63 Signage should be consistent in design with respect to materials, size, location on a building, lettering and lighting, while also allowing some flexibility for tenant branding.

64 Avoid neon signs, rooftop signs and visual clutter.

65 Limit the number of monument /pylon signs on a site and coordinate their design with that of the buildings.

66 Monument/pylon signs should be coordinated in design with the buildings.

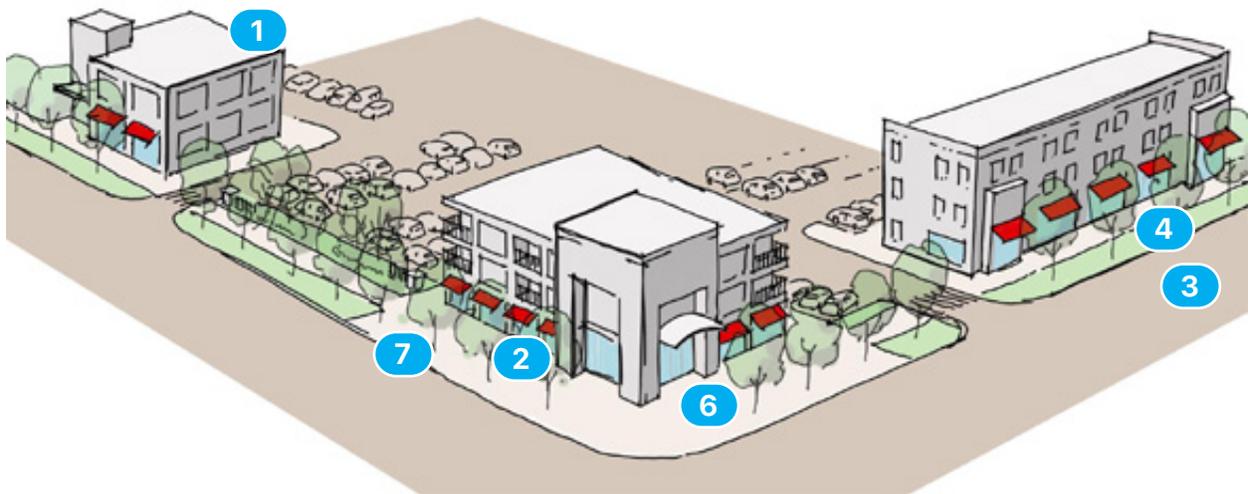


5.2 Mixed Use

This section applies to new mixed use development outside of the downtown. This can include the redevelopment and intensification of commercial sites, and the provision of new neighbourhood focal areas in greenfield development.

- 1 Provide active uses at ground level such as retail, commercial, restaurant, professional and medical services, or office along a minimum of 75% of the built frontage of buildings facing the site's primary street. Generally, the only other ground level use along primary streets should be entrances to the other, above-grade uses in the building.
- 2 Establish direct building frontage along a minimum of 50% of a site's adjacent right(s) of way.
- 3 The primary building facade and main front entrances shall orient to and be accessed from a public street sidewalk.

- 4 Where there is more than one adjacent street, orient primary building facades to the street with higher pedestrian potential.
- 5 For corner buildings, locate main entrances at or near the corner.
- 6 Sidewalks should be wider and provide opportunity for patios and retail display.
- 7 Site organization and building design should follow the Non-residential design guidelines. Parking should generally be located at the rear of buildings.



5.3 Retail

This section applies to sites where one (or more) commercial/retail users exceeds 3,000 square metres of building area for that individual user/premises.

- 1 The largest buildings should be located at the rear of sites.
- 2 Locate smaller buildings along street edges and at s, and ensure they address streets with a high quality building expression including windows.
- 3 Seek to establish direct building frontage along a minimum of 35% of a site's adjacent right(s) of way. Where this cannot be achieved, provide an enhanced pedestrian-oriented landscape treatment along the street edges including:
 - a. continuous sidewalks;
 - b. direct connections between sidewalks and internal walkways that lead to building entrances;
 - c. double row of street trees;
 - d. minimum 3.0 metre wide landscape zone along the street with low plantings;
 - e. hard landscape elements that define gateways and enhance pedestrian comfort such as seating, low walls, pergolas, columns and gateway features.
- 4 Site organization and building design should follow the General Guidelines for All Sites in this section.



5.4 Drive-through Facilities

- 1 Within larger developments, locate drive-through facilities at mid-block locations with queueing and drive-through lanes at the side or rear yards.
- 2 For sites that contain two or more drive-through facilities, ensure clear separation of their respective driveways and queue lanes.
- 3 Provide separate entrances/exits for drive-through facilities and the site.
- 4 Locate queue lanes (and intercom stations) away from residential areas and outdoor amenity areas.
- 5 Where possible, consider double drive-through lanes that merge into a single queue lane for pick-up.
- 6 Avoid locating queueing and drive-through lanes between the street and the building; for exceptions where this condition occurs provide a minimum 4.5m separation between the street and the drive-through / queue lanes that is landscaped, including plantings, fences and walls to screen these areas from public view.
- 7 Provide queue lanes to accommodate the following minimum number of vehicles:
 - a. 10 vehicle spaces for restaurants;
 - b. 8 vehicle spaces for financial institutions;
 - c. 3 vehicle spaces for other uses, such as pharmacies.
- 8 Provide a 2.0m minimum separation between queue lanes and parking areas, with the use of raised medians, planting, fences and walls.



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- 9 Avoid pedestrian routes that cross driveways and queue lanes; if they must cross these areas, they should be located to minimize potential conflict, and should be designed to prioritize pedestrians, through the use of clear pavement markings, special pavement, signage and other cues to ensure safety.
- 10 Separate payment and pick-up windows where possible.
- 11 Block spill over of vehicle headlights onto adjacent residential properties, public streets and public spaces.
- 12 Provide weather protection for payment / pick-up windows.

5.5 Institutional/Community Use Sites and Buildings

These buildings / sites have a focal role within the community and should demonstrate the highest level of design considerations and use of quality material.



- 1 Buildings should be sited prominently to anchor / gateway locations, and/or terminate vistas.
- 2 Principal buildings should be sited close to the primary street with building presence along at least 60% of the street frontage.
- 3 Massing and articulation of buildings should be considered in the context of creating 'landmarks' within the community.
- 4 Locate vehicle drop off and parking areas away from the street frontage, preferably at the sides of the principal building.
- 5 Where drop off and parking areas are located along the street, they should be designed as part of an enhanced public realm (continuous paving, rolled/flush curbs, street furniture, seating, and planting).

6 Gateways

6.1 Introduction

The Gateways are located at the boundary along the most important approach roads to Port Dover. They are points of transition from the agricultural surroundings to the urban area, and are important in forming the first impression of Port Dover. Development within gateway areas should establish a high quality image for the community, provide a sense of arrival, and reinforce the community's character. For commercial or institutional uses to be located in a Gateway, refer to guidelines for non-residential uses included in Chapter 6.

6.2 Guidelines for Gateways

- 1 One or more buildings located adjacent to the principal arrival road should be designed as landmarks with unique architectural treatments. The landmark building(s) should frame the arrival to Port Dover by being located close to the road right of way.
- 2 Building massing and design in a Gateway visible from the principal arrival road should reinforce the importance of the area through taller buildings, higher roofs, and/or prominent visual features such as a taller massing element withing the building design, gables, dormers, significant front entrances, use of transparency, and upgraded material use.
- 3 Buildings should address the principal arrival road with high quality 'frontal' appearance inclusive of windows, entrances, and sidewalk connections (if a sidewalk is present along the principal arrival road).
- 4 Rear yards with long privacy fencing is not appropriate at gateway locations.
- 5 Generous landscaping along the principal arrival road should highlight local natural ecologies through choice of plant material and design layout.
- 6 Planting should provide views to landmark building(s), screen parking areas, and create



a 'green corridor' along the principal arrival road.

- 7 A vertical element in the landscape design is encouraged, to act as a focal point and landmark, with sufficient scale to be seen from a distance.
- 8 High quality signs for municipal identity, retail/commercial uses and wayfinding can also contribute to a sense of gateway. They should be designed to be compatible with the built form and landscape character, and of a scale to be seen and read in the landscape.
- 9 Consider traffic calming elements within the right of way such as unit paving, raised crosswalks, table top intersections, narrowed vehicular lanes, and on-street parking.

7 Parks & Stormwater Management

7.1 Parks

Powell Park

Powell Park is the focal public space Downtown. It is a unique park in Port Dover with an important cultural heritage story. It has a variety of amenities, including seating, picnic tables, a gazebo, walking paths, public art, monuments, children's play, waste receptacles and lighting. The current character and function of the park should be maintained.

- 1 Maintain open park frontage on all four sides with direct sidewalk and crosswalk connections to all surrounding sidewalks.
- 2 Enhance the children's play function. This could include dedicated play equipment, or playful elements that serve dual purposes.
- 3 Consider special treatment, temporary or permanent, for Market Street East and Park Street, that visually extend the park environment and allow the parking and/or street areas to be used for park events.
- 4 Expand opportunities to reference Port Dover's history.
- 5 Consider adding more opportunities for accessible activities, picnicking and seating over time.



Port Dover • Powell Park

Silver Lake Park

Silver Lake Park is owned by the Port Dover Lions Club. Purchased in 1992, the park is the site of a soccer field, home to the Silver Lake Market and the location for many activities, celebrations and festivals. Rowing, dragon boat racing, canoeing and kayaking occur in the south end of the park, organized by various groups. There has been interest in repairing Misner Dam and revitalizing Silver Lake since 2010 with goals to:

- Revitalize the Provincially Significant Wetlands in the north end;

- Create a smaller Silver Lake in the south end suitable for water based recreation;
- Create a series of silt traps in the Silver Lake Basin with truck access to protect the river below Misner Dam, and the harbour; and
- Provide public access to both areas for recreation and environmental purposes.

Vista Parks

Vista Parks are intended to be located at the interface between Port Dover's community fabric and the natural heritage system or water's edge. They are often located at the end of a road as it approaches the lake, one of the river valleys, a woodlot, or other natural feature. Vista Parks provide a window and access to the natural feature and/or water's edge. Vista Parks may be trail access locations, a small park, visual amenities managed as ecological amenities, or a combination. They are important in defining the unique sense of place for Port Dover and complementing the parks and active transportation network.



- 6** Locate Vista Parks to enhance connectivity and clearly define access and views to the natural heritage system, river valleys and Lake Erie.



- 7** Design Vista Parks as natural parks, organizing its elements to harmonize with the surrounding landscape and incorporate the use of natural materials.

- 8** Incorporate low maintenance, native plantings and ensure appropriate landscaped transition to buffer enhancement areas and natural heritage features.



- 9** Where Vista Parks are of sufficient size, consider additional park elements such as children's play, seating, shade, gateway features, wayfinding signage, waste and recycling receptacles, lighting and gathering spots, as appropriate.

A lake front Vista Park with trail connection, seating, an overlook area, and wayfinding signage.

- 10** Vista Parks along Lake Erie should include a sign identifying clear public access and an accessible hard surface and seating area for enjoyment of the lake views, at a minimum.

- 11 Buildings adjacent to Vista Parks should treat the elevation facing the park as a frontal elevation, and should consider an entrance facing the park.

Neighbourhood Parks in Greenfield Neighbourhoods

New neighbourhood parks are multi-purpose community open space amenities located at or near the centre of neighbourhoods. They act as community focal points, and can be associated with a school, natural feature, or commercial or higher density residential uses.

- 12 Locate parks centrally within neighbourhoods, ideally within a 400 to 800m radius (5 to 10 minute walk) to most residences on the neighbourhood.
- 13 Connect parks to the larger open space system through public streets, trails, and natural features.
- 14 Locate and design parks to support, complement, and buffer the natural heritage system.
- 15 Provide significant publicly accessible frontage adjacent to parks, in the form of a public road, a school, or the natural heritage system.
- 16 Ensure neighbourhood parks have significant frontage on adjacent streets to promote views and reinforce their focal nature. Encourage street frontages on 3 sides (preferable configuration), and provide a minimum of 2 sides fronting onto streets.
- 17 Locate neighbourhood parks adjacent to school sites, where appropriate, to allow for shared amenities, such as parking lots and recreational play fields.
- 18 Ensure the neighbourhood park public street frontage is between 50 to 80m when bordering a school or residential area.





- 19 Consider community gardens in neighbourhood parks to further encourage social interaction and to provide access to locally grown produce.
- 20 Ensure the park system includes a variety of elements ranging in size, scale and function, both passive and active recreation.
- 21 Provide a range of physical activity spaces for children and adults to promote physical activity in different age groups.
- 22 Maintain existing, healthy trees and other vegetation on site.
- 23 Incorporate new trees and landscaping within parks to contribute to the urban tree canopy and buffer natural areas.
- 24 Ensure bicycle and pedestrian routes to parks are accessible, safe, and visible.
- 25 Incorporate Crime Prevention through Environmental Design (CPTED) principles into the design of parks to ensure clear views into and out of surrounding areas, including:
 - a. Adequate lighting;
 - b. Front buildings overlooking public spaces, especially playgrounds which should be highly visible to public streets and/or houses to enhance safety;
 - c. Proper signs and design for ease of access and egress; and,
 - d. Mix of activity to encourage constant use of the space.
- 26 Provide lighting to be Dark Sky/Nighttime Friendly compliant. Where feasible, incorporate LED or solar powered lighting.
- 27 Direct lighting for sports fields away from the natural heritage features and design lighting to minimize disturbance to adjacent properties.
- 28 Consider opportunities for renewable energy use such as solar powered lighting for

natural trails, park pathways and other public spaces to reduce electric energy supply in the public realm.

- 29 Provide wayfinding signage that has a high level of clarity, visibility, and visual interest; is made of high quality materials; and aids pedestrians, cyclists and drivers in navigating the area, especially at night.

Parkettes

Parkettes are small parks that complement neighbourhood parks. They provide small scale outdoor amenities such as seating, children's play and small grassed flex spaces.

- 30 Locate parkettes within a 3 to 5 minute walk of most residents (200m radius) and include passive recreational features for the immediate neighbourhood.
- 31 Locate parkettes to achieve significant public exposure and access. Urban design options include surrounding the park with streets, or dwellings fronting directly onto the parkette to create visually attractive 'edges' to these spaces and ensure there are clear sight lines from surrounding buildings to the public space.
- 32 Ensure the parkette design complements and enhances the surrounding public realm by integrating the landscape treatment (built form features, site furniture and landscape elements) within adjacent streetscapes and public space areas.
- 33 Consider designs that complements / harmonize with adjacent greenlands / natural heritage areas; use natural, sustainable materials.



7.2 Stormwater Management Facilities



Stormwater management facilities (SWM) are part of the Port Dover's infrastructure, and perform the required function of collecting and treating runoff and controlling flood potential in neighbourhoods. These facilities also form part of the Natural Heritage and Park network, providing opportunities for passive recreation and nature interpretation.

- 1 Provide walking trails, seating nodes and low-maintenance naturalized plantings on table land areas of the SWM Block.
- 2 Combine trails with maintenance paths where possible.
- 3 Connect walking trails to the broader trails network.
- 4 Design stormwater management facilities using ecological principles that emphasize naturalization of aquatic and terrestrial planting and grading systems to enhance the ecological function, reduce maintenance cost, and beautify the facility.



8 Active Transportation

8.1 Introduction

Trails for pedestrians and cyclists enhance the connectivity of the community, provide low-impact transportation choices, and promote active, healthy lifestyles. Port Dover seeks to provide a linked active transportation network throughout the community, complemented by, and interlinked with, the road network.

There has been much interest in extending the Lynn Valley Trail along the south edge of Silver Lake, along the river edge to the harbour pier. The Conceptual Active Transportation Network locates a water's edge trail.

This chapter includes general guidelines for Active Transportation based on the work of the County's Integrated Sustainable Master Plan.

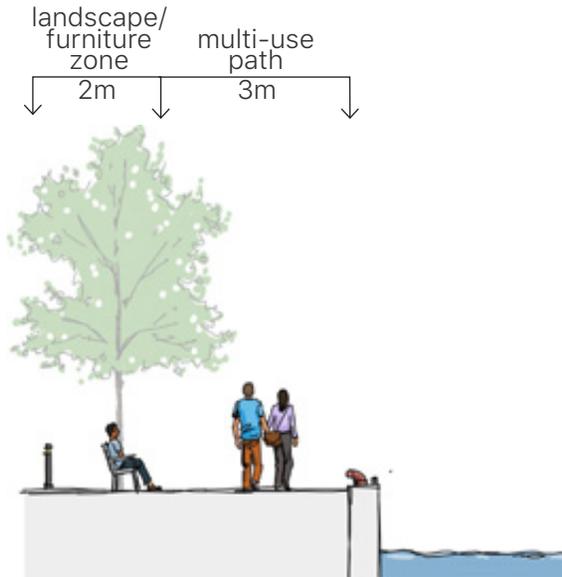
8.2 Guidelines for Active Transportation

- 1 Identify public land that can be used for access and/or recreation and enhancing its accessibility through signage and capital improvements. An example could include providing seating and a walking path along the Lake Erie shoreline where a municipal right of way extends to the water.
- 2 Provide new public space or trails where development occurs. An example could include creating a walkway along the Lynn River.
- 3 Manage vegetation along the water's edge to open views.
- 4 Provide benches along the pedestrian trail.
- 5 Support cycling by providing more infrastructure such as bike lock-up areas, bike repair areas, and covered bike parking.

Trails

- 6 Encourage active transportation and physical activity through the provision of a linked system of walking and cycling trails that provide residents with access and mobility options to local destinations.
- 7 Expand the network of trails throughout Port Dover where feasible.
- 8 Design trails to accommodate a range of users and abilities and be barrier-free, where appropriate.
- 9 Provide trail entrances at the intersections of trails with the street R.O.W and coordinate their design with that of the adjacent streetscapes and open spaces.
- 10 Provide benches and waste and recycling receptacles at trail heads and at regular intervals along the route.
- 11 Consider special treatments at trail head entrances including high quality features such as landscaping, decorative paving pattern, interpretive or directional signage, or wider pathway widths.
- 12 Ensure trails are minimum 2.5m wide, and in areas of high pedestrian traffic, wider where possible.
- 13 Ensure pedestrian trails located in environmentally sensitive areas consist of low impact materials such as natural earth, woodchips, or mown strips.
- 14 Provide lighting for pedestrian safety along busy urban trails, but minimize the disturbance on natural heritage habitats.
- 15 Provide wayfinding signage and/or trail markers throughout the trail network and clearly sign trails regarding permitted uses and speed.





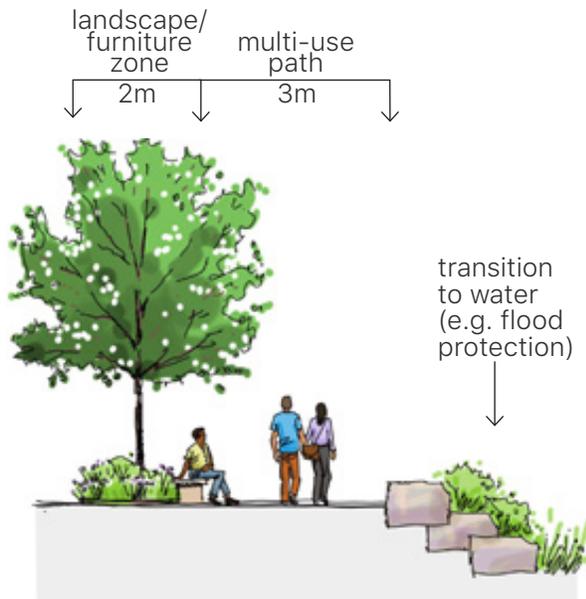
5m wide public realm at water's edge – seawall condition

- 16 Use native, non-invasive species that can contribute to the urban tree canopy along trails abutting natural features and coordinate planting design to shade trails.

Water's Edge Trail

- 17 The water's edge trail should have a minimum width of 3m wherever possible.

- 18 Provide a minimum 2m landscape/furniture zone in association with the trail. The landscape/furniture zone can accommodate planting, trees, seating, lighting, bike racks, signage, and trail connections. Additional land required for grading or hazard lands should not be part of the landscape zone.



5m wide public realm at water's edge – landscape condition



9 Streets

9.1 Introduction

Streets are the largest component of public space in communities and as such play a critical role in establishing a beautiful and high quality image in Port Dover. The street network includes the Provincial highway, Primary streets comprised of existing and potential arterial and collector streets, local streets (including Green/Vista Streets) and special streets (Main Street, other commercial streets and Walker Street).

9.2 General Guidelines for Streets

Street Network

- 1 The functional hierarchy of the street network should be enhanced by streetscape design; a variety of different streetscape character types should be provided within new neighbourhoods.
- 2 Incorporate traffic calming measures such as on-street parking, reduced lane widths, public laneways, raised intersections, and/or traffic circles to reduce vehicular traffic speeds and to ensure safe walking and cycling environments.

Walkability

- 1 Provide continuous sidewalks, or equivalent provisions for walking, on both sides of the road.
- 2 Provide large canopy deciduous trees on both sides of the street.
- 3 Explore opportunities to create more space between the building face and the roadway to accommodate pedestrians areas, trees, landscaping and spill-out space for businesses.
- 4 Add traffic calming elements such as enhanced crosswalks or mid-block pedestrian crossings.



- 5 Building entrances, sidewalks and crosswalks should be barrier-free and accessible.
- 6 All pedestrian clearways should be designed to a minimum width of 2.0m, with additional space in commercial areas for street furniture, trees, and spill-out space for businesses.
- 7 Use distinctive feature paving, alternative pavement markings or materials to minimize the conflict between vehicles and pedestrians and to enhance pedestrian crossings visibility and quality. At minimum, provide crossings identified with distinctive painted lines.



Street Furniture

- 8 Streetscape elements, such as street furniture, refuse and recycling containers, newspaper boxes, should be coordinated, clustered and not impede paths of travel.
- 9 Provide a coordinated and consistent family of street furnishings within distinct areas of Port Dover.
- 10 Coordinate above- and below-ground utilities to avoid visual clutter in the streetscape and to minimize conflict with street trees. The provision of street trees should take precedence.

Street Landscaping

- 11 Existing street trees and planting beds should be retained and maintained where possible/appropriate.
- 12 Street trees should be provided to develop, over time, a strong mature canopy and create a healthy and attractive streetscape environment.
- 13 Hanging baskets, seasonal planters and other landscape features which add warmth and visual interest to the streetscape should be installed and maintained in key areas.

- 14** Trees and landscaping should not obscure views and sight lines.
- 15** Provide planting materials to add visual interest all year.
- 16** Introduce green infrastructure, such as bioswales, within the public right-of-way to enhance ground water infiltration and improve water quality as part of a comprehensive water management plan.

Lighting

- 17** Decorative and pedestrian-scaled lighting should be used to enhance the streetscape experience, to animate streetscapes at night, and for safety and pedestrian comfort.
- 18** Existing lighting elements should be maintained, and as development occurs, extended to new block frontages.
- 19** Spotlighting and decorative lighting should be used to highlight landscape and architectural features, landmark buildings and signage.
- 20** Lighting should be designed using energy efficient sources and to avoid light pollution, spillover and glare.

Signage and Wayfinding

- 21** Coordinated directional signage should be provided to improve accessibility and wayfinding for residents and visitors.
- 22** Signage and wayfinding should be designed to be similar to the overall theme of the streetscape and architectural character of buildings, and should be unique to Port Dover.
- 23** Signage and wayfinding should be designed for clarity and visibility (not blocked by vegetation) and where possible, information should be consolidated on one panel or post.

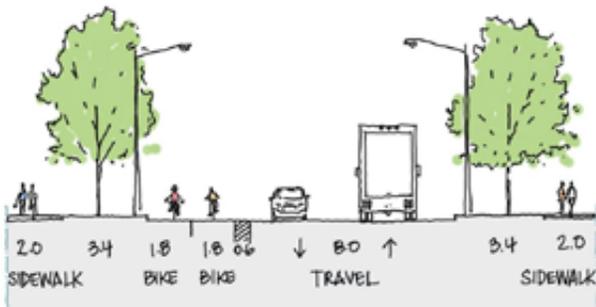
- 24 Signage placement should anticipate snow pile up and be located to be legible where possible throughout the year.

9.3 Street Typologies

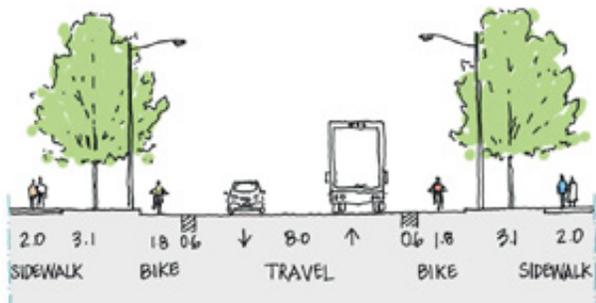
Primary Streets (Arterials and Collectors)

Primary Streets connect the neighbourhoods of Port Dover and its surrounding context. They should be planned to accommodate all modes of travel. New Primary Streets in new neighbourhoods should be planned as integral components of the bike network; many new Primary Streets will be suitable as cycling routes. The selection of uni- or bi-directional facilities will depend on the context of the street.

- 1 Provide sidewalks on both sides of the street.
- 2 Provide a softscape boulevard with large canopy street trees on both sides of the street.
- 3 Provide cycling lanes as appropriate.



Primary Street with bi-directional bike lanes and buffer on one side of the street. Dimensions are to be confirmed through appropriate engineering analysis.



Primary Street with uni-directional bike lanes and buffer on both sides of the street. Dimensions are to be confirmed through appropriate engineering analysis.

Special Streets

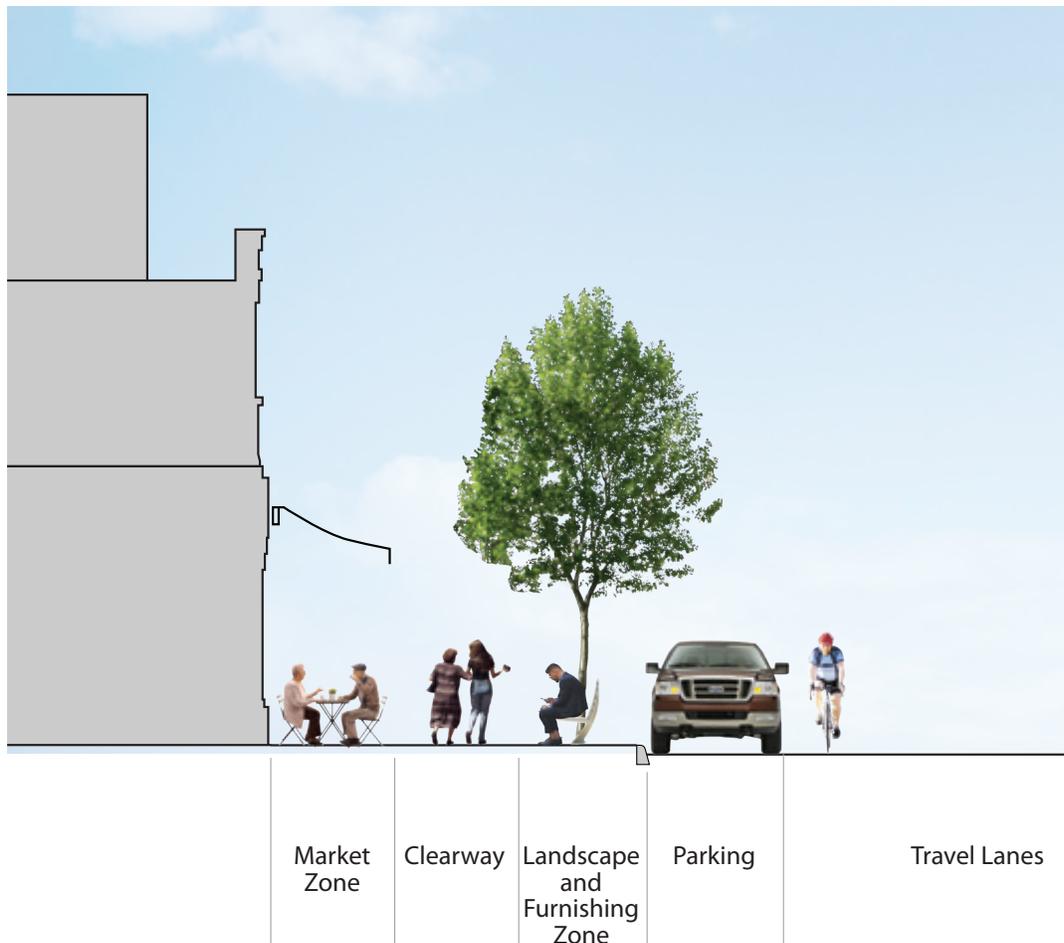
Commerical Streets

Streets in Port Dover that support commercial-retail activity, such as Main Street, side streets in Downtown, and potential future mixed use nodes, should be designed to support pedestrian activity as a priority.

- 1 Provide a 2.0m minimum pedestrian clearway on all sidewalks.
- 2 Include a 'Market Zone' along commercial-retail frontages, either within the right of way or within the setback. The Market Zone is a paved extension of the sidewalk for commercial spill out such as patios, signs and merchandise display.
- 3 Designate a zone adjacent to the vehicular travel way for landscaping and furnishing, inclusive of lighting, seating, trees and signage.

Business Spill-Out Spaces

- 4 Outdoor areas with spill-out space from businesses, such as sidewalk cafés and patios, are encouraged as they help to animate the street.
- 5 Spill-out spaces should be located along the street sidewalk edge or within the spaces between buildings. These spaces should relate to the street and be open to public view.
- 6 Sidewalk cafés and patios should be designed and located so as not to impede pedestrian movement.
- 7 Decorative fencing and patio furniture should be used to add interest in the streetscape and complement the design of the building.



Main Street

Main Street is the traditional heart of downtown and the focus of commercial activity and street life. It must accommodate and balance a lot of infrastructure and activities within its right of way, from walking to driving to parking, and signs, lighting, furniture, planting and other street elements. Increasing the flexibility of Main Street to be used in different ways can enhance its role and function as the downtown's focal point.

While Main Street is classified as an Arterial Road, it must be designed to support its role as the heart of downtown, and not as a standard arterial road.

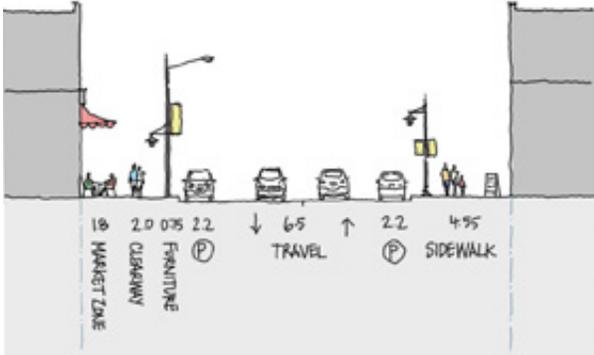
If Main Street is selected as an active transportation route with cycling (sharrows), it is recommended that on-street parking be removed on one side, in order to maintain sufficient sidewalk space as the premier pedestrian-oriented space in Port Dover.

- 1 Provide sidewalks on both sides of the street, paved from curb edge to building face, that accommodate:
 - a. a minimum 2.0m pedestrian clearway;
 - b. a market zone adjacent to the building face that can host sidewalk cafes or other retail spill-out;
 - c. a curbside furniture zone for lighting, signs, seating and other amenities.
- 2 Where there is additional space within the right of way, or the adjacent building is set back, use the space as an extension of the sidewalk.
- 3 Consider zones of on-street parallel parking that can be re-purposed for commercial or public uses, either seasonally or for events.

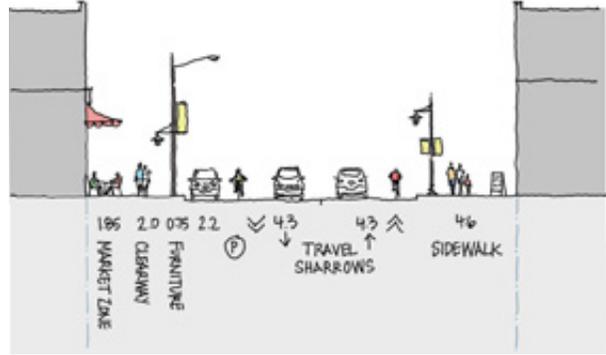


Examples of a permanent flexible parking lane that can be re-purposed seasonally using movable bollards. The flex space is used for parking in some seasons, usually winter, and for patios and public seating in others.

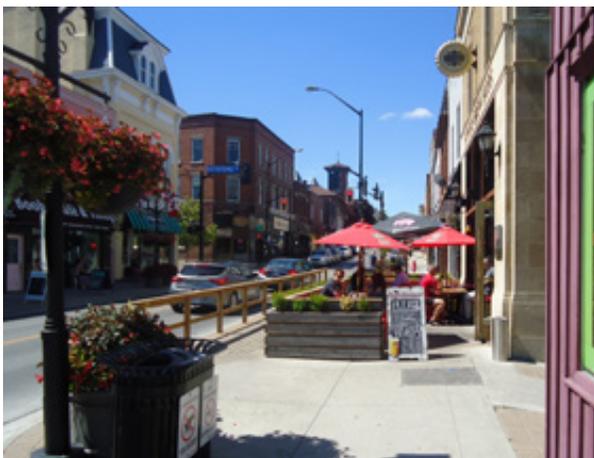
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Main Street (and other commercial streets) without bike sharrows. Dimensions are to be confirmed through appropriate engineering analysis.



Main Street (and other commercial streets) with bike sharrows. Dimensions are to be confirmed through appropriate engineering analysis.



Examples of temporary sidewalk diversion to accommodate a patio along the building edge. The temporary sidewalk is located within the on-street parking lane, and raised to curb height for better accessibility.



Examples of temporary patios and parklettes located within the on-street parking lane.

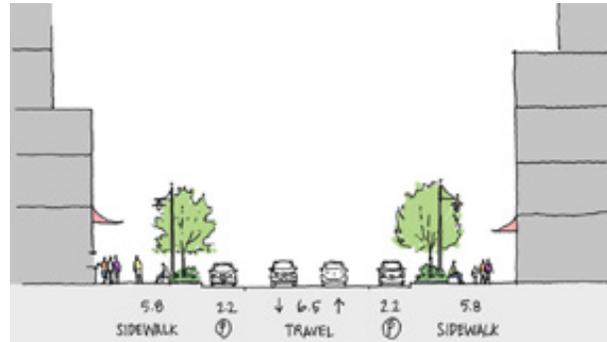
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Walker Street

Walker Street is an important character-defining street in Port Dover, providing commercial activity and access to the beach. New development has the potential to intensify the street as a pedestrian destination and transform the right of way. The right-angled parking that has existing on Walker Street is not conducive to a pedestrian environment.

While part of Walker Street is classified as a Collector Road, it must be designed to support its role as the character-defining street of the waterfront area, and not as a standard collector road.

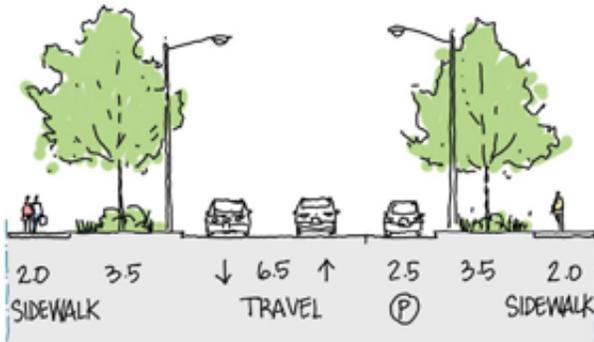
- 1 Provide wide sidewalks along both sides of Walker Street through reductions in the vehicular space.
- 2 Sidewalks should be paved from curb edge to building face.
- 3 Include generous landscaping and seating along the length of Walker Street.



Walker Street can have generous sidewalks by providing parallel parking on both sides of the street. Dimensions are to be confirmed through appropriate engineering analysis.



Walker Street today



Dimensions are to be confirmed through appropriate engineering analysis.

Local Streets

Local Streets will comprise the majority of the streets in new neighbourhoods. They should be planned with a greater emphasis on the pedestrian environment.

- 1 Provide sidewalks on both sides of the street.
- 2 Provide a softscape boulevard with large canopy street trees on both sides of the street. Consider Low Impact Development measures with the boulevards such as stormwater management and enhanced planting.

Green/Vista Street

- 1** Provide 'Green / Vista Streets' - These streets should be oriented to visually connect new neighbourhoods to the surrounding natural context and rural landscapes. They should be designed as pedestrian oriented streets that connect parks and open spaces to one another and to the natural heritage system.
- 2** Green / Vista Streets should consider incorporating wider boulevards to allow for wider sidewalks, a double row of street trees, bio-retention swales, and naturalized planting instead of sod.
- 3** Green / Vista Streets should include upgraded front elevations for all dwellings on either side of the street.
- 4** Green / Vista Streets should incorporate visually impactful street trees distinct in their size, form and fall leaf colour.

10 Green Infrastructure and Building

While sustainability is an overarching objective throughout the Guidelines, this section provides guidance on green infrastructure and building practices and helps achieve the broad sustainability principles of the Secondary Plan.

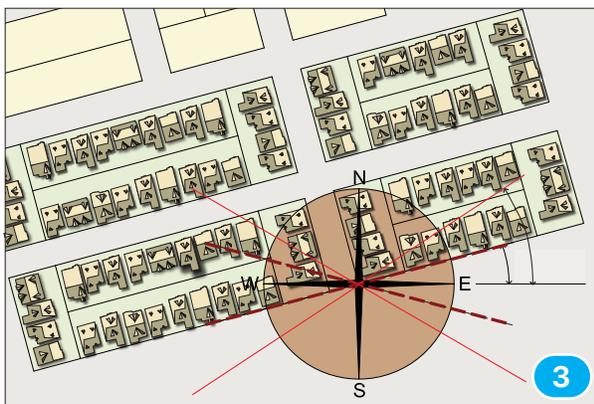
As part of the strategy to achieve a high level of sustainability in regards to the reduction of energy, water and waste, the Green Infrastructure and Building Guidelines apply to both the private and public realm.



10.1 Energy Conservation

Provide for the reduction of energy use and consider the inclusion of alternative energy sources.

- 1 Where feasible, provide alternative community energy systems such as district energy, geo-exchange, sewer heat recovery, and/or inter-seasonal thermal energy.
- 2 Consider reducing demand for energy from the grid and encourage renewable energy production. Renewable energy sources that could be employed may include the use of solar thermal and photo voltaic equipment, and/or wind power. Proposed alternative energy source(s) could be used in combination with energy from the grid.
- 3 Encourage passive solar orientation to permit enhanced energy efficiencies by creating optimum conditions for the use of passive and active solar strategies. The integration of passive building systems is enhanced with buildings oriented to maximize the potential for sunlight and natural ventilation.



- 4 Where feasible, implement street and block alignment within 25 degrees of geographic east-west to maximize passive solar orientation of buildings front and rear windows.
- 5 Consider constructing all low and medium density residential buildings to be Solar Ready, built with all the necessary piping and equipment that would be needed to install a rooftop solar power system.

- 6 Consider the purchase of energy from renewable resources available from local utility/energy providers.
- 7 Reduce heat absorption through the use of cool roofs that are designed to reflect more sunlight and absorb less heat than a standard roof. Cool roofs can be made of a highly reflective type of paint, a sheet covering, or highly reflective tiles or shingles. Consider cool roofing material with a minimum initial solar reflectance of 0.65 and minimum thermal emittance of 0.90.
- 8 For a low sloped roof, typical of commercial and institutional buildings, the cool roof Solar Reflectance Index (SRI) value should be 0.64 and for steep sloped roofs, typical of residential, the SRI value should be 15.
- 9 Mitigate heat island impacts through the use of paving material with high solar reflectance, strategic use of deciduous trees or preserve existing trees as part of a free cooling strategy to help with evapotranspiration and shading of sidewalks and hard surface areas in summer and solar access in winter.
- 10 Charging stations that would supply electricity for electric vehicles are encouraged in Draft Plans/Site Plans. Charging stations could be provided in parking areas of mixed-uses, institutional uses, or within parking garage structures.
- 11 Grade related residential unit driveways are encouraged to be paved with light-coloured material to reduce the heat island effect.



10.2 Water Use and Management

The benefits of high performance, compact, mixed use projects include reduction in household water consumption and water utility costs, as well as the protection of the natural water supply. Compact development reduces impervious surfaces and makes it easier to protect natural areas which are the most important steps a community can take to maintain water quality.



- 1** Encourage the implementation of Low Impact Design Standards that emphasize the use of bio-swales, innovative stormwater practices, constructed wetlands, at-source infiltration, greywater re-use system, and alternative filtration systems such as treatment trains.
- 2** Implement a comprehensive rainwater and water recharge strategy in conjunction with required stormwater management facilities.
- 3** Implement strategies for stormwater retention and run-off such as:
 - a.** Retain stormwater on-site through rainwater harvesting, on-site infiltration, and evapotranspiration;
 - b.** Consider the inclusion of third pipe greywater systems and rain water harvesting for watering lawns and gardening, to reduce demand on potable water use;
 - c.** Direct flow to landscaped areas and minimize the use of hard surfaces in order to reduce the volume of run-off into the storm drainage system;
 - d.** Store snow piles away from drainage courses, storm drain inlets, and planted areas; and,
 - e.** Use infiltration trenches, dry swales and naturalized bioswales adjacent to parking areas to improve on-site infiltration.

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- 4 Introduce green infrastructure, such as bioswales, within the public right-of-way to enhance ground water infiltration and improve water quality as part of a comprehensive water management plan.
- 5 Encourage the use of porous or permeable pavement instead of standard asphalt and concrete for surfacing sidewalks, driveways, parking areas, and many types of road surfaces as a stormwater run-off management strategy.
- 6 Implement a rainwater harvesting program to provide the passive irrigation of public and/or private greenspace, including absorbent landscaping, cisterns, rain barrels, underground storage tanks, infiltration trenches, etc.
- 7 Implement xeriscaping using native, drought-tolerant plants, a cost-effective landscape method to conserve water and other resources on a residential and community-wide level.
- 8 Where feasible, implement curb cuts along sidewalks and driveways to allow water to flow onto planted zones or infiltration basins.
- 9 Consider the installation of subsurface basins below parking lots to enable stormwater to be stored and absorbed slowly into surrounding soils.



10.3 Material Resources and Solid Waste

Assist in the reduction and diversion of waste from landfills and increase measures for recycling and reuse.

- 1 Consider the use of recycled/reclaimed materials for new infrastructure including roadways, parking lots, sidewalks, unit pavings, curbs, water retention tanks and vaults, stormwater management facilities, sanitary sewers, and/or water pipes.
- 2 Incorporate strategies that emphasize targets for a higher diversion rate in recycling for the plan area.
- 3 Reduce waste volumes through the provision of recycling/reuse stations, drop-off points for potentially hazardous waste, and centralized composting stations.
- 4 Consider incorporating existing heritage buildings in situ through retention, restoration, and adaptive reuse to avoid further construction waste.
- 5 In large buildings, such as multi-unit residential buildings and institutional or public buildings, provide on-site recycling facilities for handling, storing, and separation of recyclables.
- 6 Recycle and/or salvage at least 50% of nonhazardous construction and demolition debris and locate a designated area on site during construction for recyclable materials.

10.4 Air Quality

In order to minimize the air quality and climate change impacts associated with new growth, the following measures are encouraged.

- 1** Reduce the impact of air pollution by encouraging the development of 'complete' communities that are characterized by greater densities placed at neighbourhood centres, mixed use nodes, or near transit facilities; mixed land uses; mix and diversity of housing types; connected and walkable road patterns, and are designed to encourage active transportation.
- 2** Encourage and promote alternative modes of transportation such as public transit, walking, and cycling. Provide transit within a 200 to 400 metre (3 to 5 minute) walking distance of residential development.
- 3** Ensure the separation of sensitive land uses from air pollutant sources through land use planning and zoning. Refer to the Ministry of the Environment guidelines.
- 4** To promote transit ridership, programs such as developer-sponsored transit passes at reduced-costs for each residential unit or employee are encouraged.
- 5** Provide the minimum number of parking spaces to minimize the impact of car parking:
 - a.** Mixed use developments should include shared use of parking among uses that have different peaking characteristics;
 - b.** Design parking areas so they are not the primary visual component of a neighbourhood;
 - c.** Reduce the parking ratio required in areas that are served by transit; and,
 - d.** Dedicate priority parking spaces for carpool, ride sharing, and ultra low emission vehicles - 5% of total parking space.



10.5 Lighting



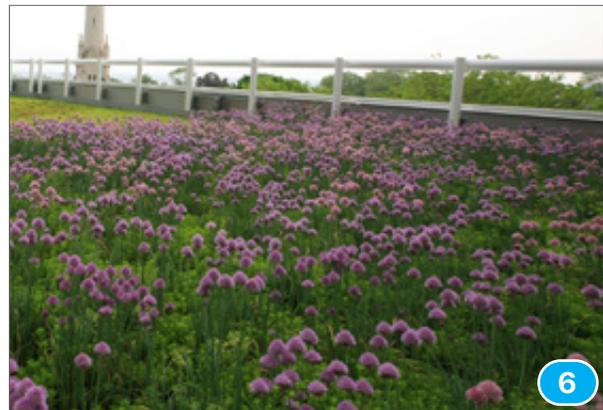
- 1 Promote Dark Sky/Nighttime Friendly compliant practices to minimize light pollution and the intrusion of unwanted lighting on natural areas.
- 2 Consider high efficiency street lighting to reduce energy use.
- 3 Consider opportunities for renewable energy use to reduce electric energy supply in the public realm, such as solar powered lighting for natural trails and park pathways.



10.6 Green Buildings/Green Sites

Promote innovative programs to encourage the design and construction of energy efficient green buildings and sites.

- 1 Consider third-party certification and rating programs, such as LEED® for New Development (ND).
- 2 Consider innovative residential development designs which contribute to affordability and energy and natural resource conservation.
- 3 Consider building(s) that are LEED® Certified or recognized or accredited by a third-party certification program i.e. Energy Star, LEED H, LEED NC, LEED for Schools, BREAM, etc.
- 4 Green roofs are encouraged for high-density residential, office buildings, as well as, public, institutional buildings to minimize surface runoff, reduce urban heat island effect, provide noise insulation, and improve local air quality.
- 5 Encourage synergies between buildings and site management practices that conserve water, reduce waste, and are energy efficient.
- 6 Provide green roofs for 80% of all high density development. In high-density residential buildings, design roofs as amenity areas.
- 7 Develop a heat island reduction strategy for community and public buildings to install green roofs with 50% coverage, remainder covered with light coloured material. Light coloured roofs have a high solar reflectance, which reduces energy costs and reduces urban heat island effect.



8 Promote Energy Efficiency:

- a.** Where feasible, provide alternative community energy systems such as geothermal, sewer heat recovery, or inter-seasonal thermal energy; and,
- b.** Development plans and building design shall provide opportunities for south facing windows and building orientation to maximize potential for passive and active solar energy.

9 Promote Water Efficiency:

- a.** All buildings comply with Ontario's Building Code required water fixtures efficiency;
- b.** Building uses Low Impact Development strategies to deal with on-site run-off and heat island effects;
- c.** Building's landscaping is water efficient and drought resistant by using native planting materials; and,
- d.** Pre-design for grey-water pipe infrastructure.

10 Promote Green Materials:

- a.** Incorporate waste reduction work plans and construction best practices that reduce construction waste;
- b.** Incorporate green building material standards to reduce impact on the environment and ensure materials are purchased/obtained from a responsible ethical sources; and,
- c.** Materials sourced from certified local businesses.

10.7 Stewardship and Education

- 1 Create a well-documented master plan including illustrations that promote sustainable aspects of the development.
- 2 Include environmental builder specifications in all subcontracts.
- 3 Produce detailed sales and promotion materials that feature conservation aspects of the development.
- 4 Develop subdivision covenants that establish ground rules for the maintenance of shared open lands and individual lots.
- 5 Create a Homebuyer's Environmental Instruction Guide that explains the unique environmental aspects of the subdivision/site and special maintenance considerations.
- 6 Include an owner/tenant education package at the time of purchase or rental regarding household activities to improve energy and water efficiency, access to transit, location of recycling station, etc. Coordinate with existing municipal and regional information.

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Appendix II: Cultural Heritage Resources

Appendix II: Cultural Heritage Resources

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Legend

■ Designated Property



Appendix III: Urban Forestry Guidelines

The Urban Forest

Trees are an important part of our towns and cities, and we are only beginning to recognize the tremendous value of a healthy urban forest. Trees perform infrastructural roles including stormwater management through root uptake and respiration, urban heat island attenuation through shading, provision of habitat for avian and insect populations, and increase resiliency in the face of climate change. Trees beautify the environment, provide psychological benefits and promote physical health in people. The economic value of a tree increases exponentially with age, with a cumulative value of \$160,000+ per tree. Over 50 years one tree will produce \$31,250 of oxygen, \$37,500 of recycled water, and \$31,500 worth of erosion control (US Forest Service, ISA, 2013). A fundamental goal of these design guidelines is to promote an expanded urban forest that exhibits health and resiliency.

Careful attention to the growing requirements of trees is intended to ensure that tree life expectancy cycles are considered in increments of 50 to 100 years. That is, even the fastest growing and shortest lived trees will thrive for 50 years, and the slower growing and longer lived trees will have an opportunity to last a century.

There are many factors that contribute to successful trees, but foremost is their soil conditions: sufficient soil volume, quality soils that are not compacted, and access to air and water. The best place to grow trees are in protected natural heritage areas. In lower density neighbourhoods where space is plentiful, soil condition requirements are easier to meet. But in Port Dover's older, more urban areas, where space is more intensively used for roads, buildings and sidewalks, deliberate care must be taken to meet the soil needs of trees.

Diversity

Biodiversity is founded on ecological principles with a host of benefits, but of particular importance is the establishment of resiliency. The International Society of Arboriculture's (ISA) best practices promote species selection based on the following guidelines:

- no more than 30% from the same family;
- no more than 20% from the same genus; and
- no more than 10% from the same species.

The purpose of this allocation is to prevent the same kind of monoculture that devastated the trees of our towns and cities including the American Chestnut, American Elm, and Ash trees. Many species today are under threat from pests and diseases, and diversity will help to ensure the urban forest is more resilient and remains green when pests and diseases inevitably strike.

It is important to look forward towards issues which may impact trees which are currently keystone species in the landscape, such as oak and maple. These species may be subject to pests and pathogens in the near future such as Oak Wilt, Gypsy Moth, and Asian Longhorn Beetle. These species should still be planted, but must not be over planted. Monoculture plantings of a single species should not be used so future pathogens do not cause widespread mortality.

Promoting Carolinian species is a unique way that Port Dover can increase urban forest diversity and resiliency against climate change and northward moving pathogens.

While the ISA percentages are a goal of the urban forest system, it will be difficult to achieve in the Downtown and Urban Waterfront, where there are fewer trees to choose from of sufficient hardiness and adaptation to urban conditions.

Tree Preservation

Existing trees are the greatest asset of the urban forest, and new developments in Port Dover should attempt to preserve healthy mature trees where they are not located within proposed building envelopes. Opportunities to preserve trees should be looked at early in the approvals process to increase the likelihood of success. When existing trees must be removed, replacement tree plantings should strive to provide equivalent benefits to the community. The Council of Tree & Landscape Appraisers provides a guide to plant appraisal which can be a useful tool for determining the value of existing trees when preservation or replacement plantings are not possible.

Existing tree preservation should prioritize large mature shade trees, trees which may have heritage value, native trees, and trees which can be managed as a group. In particular, the preservation of trees located at the perimeter of lots can often provide buffering between properties.

Tree preservation planning should consider the extent of the tree above and below ground, and the understanding that development can impact the roots of trees on adjacent properties. The critical root zone (CRZ) can be understood as a measured circle around a living tree that represents the minimum rooting area that is essential for its structural integrity and capability to remain alive and upright. The CRZ should be determined in consultation with an International Society of Arborists (ISA) certified arborist or other qualified person. However, a tree protection zone (TPZ) can be modelled as a function of the trunk diameter at breast height (DBH) and used as a guide for protection trees during development. The ISA recommends a standard TPZ of 30cm of offset per 2.54cm of diameter.

Monitoring

It is recommended that Carolinian species are incorporated into the plantings of parks and natural areas, and monitored to collect data on their performance. Producing an urban forest inventory is one way to understand the

strengths and vulnerabilities of the local tree canopy, but will require an investment of staff and other resources.

Planting Considerations

Soil

The biomass of urban trees that we see above grade, which includes the trunk, branches and leaf volume, is approximately equal to the biomass below ground, which includes the root network. Therefore, soil volume for canopy trees should be 30 cubic metres per tree, or 20 cubic metres per tree if soil volumes are shared among more than one tree. Soil volume should be within 1.4 metres of the surface. Volumes less than 14 cubic metres will not support a long-term functional tree (James Urban, Making Space for Roots: Tree Planting Options in Urban Areas, 2009. <http://www.isa-arbor.com/myaccount/myeducation/resources/ceu-april10.pdf>).

Tree roots are opportunistic, in that they will fill the available space. This means that there is a lot of flexibility in the shape of the soil area, such as, long linear boulevard spaces between the sidewalk and a street, or even in soil cells underneath sidewalks.

It is recognized that 30m³ of soil volume will not be achievable in all locations. 20m³ of soil volume will still support a large tree, though at a lesser growth potential. At reduced soil volumes, different types of trees need to be considered. There are very few instances where trees are not possible.

Planting Conditions

Open lawn areas will comprise the majority of planting conditions along streets and parks in Port Dover. Generally, there will be no issue in achieving the required soil volumes that promote healthy tree growth. However, in measuring available soil volume, only contiguous areas of lawn or soil should be counted. Sidewalks, walkways and other infrastructure, because of the compaction of the sub-grade granular underneath them, usually create barriers for tree roots.

In the Downtown and Urban Waterfront there will typically be less space available for trees. In very constrained areas, structural soil cells can achieve required soil volumes by allowing uncompacted soils to extend under the paved surfaces, areas that would otherwise be unavailable to tree roots. These engineered techniques transfer the weight of the sidewalk, street furniture and vehicles to the ground underneath a tree's required uncompacted soil volume. Structural soil cells have significant benefits both for the long term health of the urban forest, and for increasing growth rate and reducing 1-5 year stunting common to urban locations.

Open planting beds are appropriate for areas with less pedestrian traffic. This provides space for multiple trees to be co-located within a consolidated soil volume. Open planting beds with flush curb edges allow storm water to irrigate the soil area.

Irrigation systems are not generally recommended, however, it is critical to the long term health of trees to provide weekly watering during the first two years following transplant when feeder root establishment occurs. This should be accompanied by appropriate fertilization.

Soil pH levels are important for nutrient uptake and appropriate levels vary depending on species (Soil Management for Urban Trees - Best Management Practices, 2014). Soil Organic Matter content should be 3-10% (Soil Management for Urban Trees - Best Management Practices, 2014). Soil can be amended at time of planting with micorrhizal inoculant.

Roots require air and therefore soil must be well drained. Soil should drain within 24 hours of rainfall events. Appropriate mitigation is required if soil does not drain. It is too late to do this after construction is complete.

Planting Details

Temporary tree guards can be used after tree transplant to protect trees from snow removal operations, bikes, and vandalism. Tree guards should be removed after approximately 5 years or when the tree reaches 30cm diameter at breast height. Tree guards should not touch the tree or constrain growth in any fashion. Tree grates and paving details must allow adequate space for future trunk flare. Trees should be planted with the trunk flare above finished grade (top of rootball should be planted 2 to 4 cm above grade to allow for settlement), and ensure the trunk is plumb. Planting Typology

When the canopies of mature trees interconnect overhead, the value of the urban forest in performing infrastructural benefits is significantly enhanced. Tree spacing is recommended at 10 to 15 metres on centre, or closer, depending on species growth rate expectations.

Species Selection

Tree species selection should be performance based, with the right tree matched to the right location. Trees provide the most benefits to the community when they can grow for many years and attain their natural mature size. The selection of trees for individual sites must give careful consideration to the existing conditions and potential barriers to their success, including soil type, moisture, available growing space above and below ground, and proximity to sidewalks, roads and utilities. Some planting sites, like urban boulevards surrounded by paving, are extremely harsh, and only suitable for a short list of resilient tree species, while planting sites in parks or garden beds may support many more tree species. As not all trees are suitable for all locations, tree species should be selected which possess the characteristics that most closely meet the environmental conditions of each site, with the gradient of diversity increasing as the quality of the planting site increases.

Port Dover's climate is among the most temperate in Ontario, and its location within the Canadian Carolinian Zone creates the opportunity to support an urban forest with more diversity than most other locations in Ontario. The Canadian Carolinian Zone is a unique and fragile ecosystem, with the warmest average annual temperatures, the longest frost-free seasons, and the mildest winters in Ontario. Its boundary is based on the northern limit of key species which are typically only found in more southern regions, and the term 'Carolinian' refers to its similarity to the forests found in the Carolinas in the southern United States.

This unique climate means that Port Dover could have success using tree species which have little documented use in other municipalities, but are suited to the local climate.

Trees with Different Growth Rates

Growth rate stratification is based on the fact that different species grow at different rates and have different life expectancies. Generally, fast growing species are shorter lived than slow growing species. By mixing both types throughout the urban area, it ensures that a green canopy is achieved as quickly as possible, and that it remains green over time, because when the fast growing species reach the end of their lives, the slower growing species will have reached their mature size.

Natural Areas

In naturalized areas, particularly in proximity to shorelines and forested areas, native species should be planted. Invasive species which have negative impacts do the most harm when they can escape cultivation into the habitat of native species which they are able to outcompete. In particular, the species of trees for planting near riparian areas should be carefully considered.

Native species often do better in undisturbed soil, and tend to be less tolerant of the imported and compacted soil that is often found in streetscapes and new subdivisions. Many native

tree species which do not have suitable qualities for parks or streets, but are valuable habitat or food sources for pollinators and wildlife can be planted in or adjacent to naturalized areas. For example, some native species produce an abundance of fruit that can stain pavements at certain times of the year. These species should still be planted, as they are important to wildlife lifecycles, but they should be sited within an awareness of their characteristics.

Pin cherry is a good example of this type of native tree; the fruit is valuable to birds, and the bright red berries and spring flowers are attractive; however, it is a short-lived and weak wooded tree with is prone to black knot fungus. It is best planted in locations where it can freely spread by suckers and naturalize into colonies.

Refer to local conservation authorities for more extensive species planting list.

Waterfront Areas

Plantings in waterfront areas should take into account local water tables and floodplains. Many tree species which are urban and drought tolerant are intolerant of standing water. Some species which are tolerant of flooding, like silver maple, river birch, poplars and willows, have other characteristics which can be problematic, like weak wood or poor disease resistance. These species should only be used if they can be located in areas where their vulnerability to storm damage will not cause safety hazards.

Some desirable native trees like red maple, hackberry, swamp white oak and black gum can do well in wet conditions, but adaptation to flooding will depend on the source of seed stock, so this requirement should be communicated with the supplier, and locally adapted specimens grown in flood prone habitats should be sourced.

Some non-native trees, like Dutch-Elm Disease resistant hybrid elms, and hybrid soft maples are appropriate for waterfront areas, but care should be taken to avoid using any species with invasive qualities near riparian areas.

Roadside Plantings

Rural and peri-urban roadside plantings are typically subject to stress such as road salt spray, wind, compacted soil and drought. Native species from the urban boulevards list are appropriate for roadside plantings, however, if plantings can be set back further from the road the planting palette can be expanded to include native species with a lower salt and pollution tolerance from the general list, such as sugar maples.

Species Lists

The following tables provide an overview of suggested planting for Port Dover's natural areas, parks and streets. It should be considered as a starting point, to be refined through observation and local knowledge of tree health and performance in Port Dover's context.

The trees are organized in three groups:

Urban Boulevards, Constrained Conditions

- these trees are suitable for planting on sites with a high degree of impermeable surfaces and/or low opportunities for planting areas. These areas can include downtown's commercial streets, like Main Street, where the right of way is almost continuously paved. Constrained soil volumes, compaction from vehicles and pedestrian traffic, road salt and other stressors create difficult growing conditions. Species must be tolerant of these stressors. By necessity, there are a higher number of non-native trees in this group in order to provide the diversity and performance needed to achieve an urban canopy.

Sub-urban Boulevards, Parks - these areas typically have significantly improved growing conditions and as a result a much greater variety of trees that can thrive. Trees designated for Urban Boulevards should be assumed to be suitable for Softscape Boulevards and Parks. Trees designated for Softscape Boulevards should be assumed to be suitable for Park as well, but not Urban Boulevards. Trees designated Parks should be assumed to be sensitive, and should be considered on a species by species basis for

open lawn, and planting beds. Only native trees are suitable for naturalized areas, and the local conservation area can provide a more extensive list of acceptable species.

Trees to be Avoided - these trees are known to be problems, because they are invasive (they take over natural areas and compromise their ecological function), they are weak wooded and pose safety hazards in our urban areas, or they harbour invasive pests; sometimes all three.

Urban Boulevards, Constrained Conditions

Tree Species	Native Range	Comments and Notes	Size
<i>Acer ginnala</i> Amur Maple	Non-Native	Compact form/red & yellow fall colour/ lots of seeds/tends to sucker/specify single stem form	Small
<i>Acer griseum</i> Paperbark Maple	Non-Native	Compact form, exfoliating bark. Prefers moist soil.	Medium
<i>Acer x freemanii</i> Hybrid Soft Maple	Native to Ontario	Tolerant of urban conditions. Caution: Many cultivars of <i>Acer rubrum</i> and <i>A. saccharinum</i> exist under the name <i>Freemanii</i> , each with different characteristics	Large
<i>Acer myabei</i> Myabei Maple	Non-Native	Compact form	Medium
<i>Acer tataricum</i> Tatarian Maple	Non-Native	Compact form/red & yellow fall colour. Specify single stem form.	Small
<i>Amelanchier x grandiflora</i> Serviceberry (specific cultivars) 'Robin Hill' 'Autumn Brilliance'	Native to Ontario (cultivar)	Tolerant of urban stress, flowering. Specify single stem form.	Medium
<i>Amelanchier laevis</i> Serviceberry (specific cultivars) 'Spring Flurry' 'Cumulus'	Native to Ontario (cultivar)	Tolerant of urban stress, flowering. Specify single stem form.	Medium
<i>Carpinus caroliniana</i> Blue beech or Musclewood	Native to Ontario	Difficult to transplant. Keep away from road salt & spray, likes wet soil thin bark and sculptured trunk	Medium
<i>Celtis occidentalis</i> Common Hackberry	Native to Ontario	Tolerant of urban conditions, and poor drainage. Requires pruning for general form.	Large
<i>Cockspur hawthorn</i> <i>Crataegus crus-galli</i>	Native to Ontario	Compact, white flowers in spring. Tolerant of urban conditions	Small
<i>Cornus alternifolia</i> Alternate-leaf Dogwood	Native to Ontario	Use local winter hardy material only. Specify single stem	Medium
<i>Gymnocladus dioicus</i> Kentucky Coffeetree	Native to Ontario	Tolerant of urban conditions. Male trees drop large seed pods.	Large

Urban Boulevards, Constrained Conditions

Tree Species	Native Range	Comments and Notes	Size
<i>Ginkgo biloba</i> Ginkgo Tree	Non-Native	Tolerant of urban conditions. Plant male cultivars to avoid fruit litter. This species is very slow growing, and takes a long time to provide canopy cover. However, it can be useful in constrained conditions.	Large
<i>Gleditsia triacanthos var. inermis</i> Thornless Honey Locust 'Shademaster' 'Skyline'	Native to North America (cultivar)	Tolerant of urban conditions. Provides a filtered shade/susceptible to defoliation by leafhopper/susceptible to canker and other pests and diseases. Prone to over-use.	Large
<i>Liquidambar styraciflua</i> Sweetgum	Native to Eastern North America	Carolinian species, uncommon as street tree in Ontario.	Large
<i>Nyssa sylvatica</i> Black Gum	Native to Ontario	Carolinian species, tolerant of urban conditions, but uncommon as a street tree in Ontario. Tolerates poor drainage.	Large
<i>Quercus bicolor</i> Swamp White Oak	Native to Ontario	Grows in wetter conditions than most oak species. Tolerates urban conditions, but best located away from potential exposure to salt spray (eg. Wide boulevards)	Large
<i>Quercus muehlenbergii</i> Chinquapin Oak	Native to Ontario	Attractive tree, especially in old age. Limited planting, due to possibility of Oak Wilt entering Ontario	Medium
<i>Quercus robur</i> English Oak	Non-Native	Needs well drained soil/difficult to transplant/large size at maturity	Large
<i>Sophora japonica</i> Japanese Pagoda Tree	Non-Native	Showy flowers, produces seed pods, tolerant of urban conditions, but uncommon as a street tree in Ontario.	Medium
<i>Syringa reticulata</i> Japanese Tree Lilac (Ivory Silk)	Non-Native	Good white summer flower excellent small specimen Prone to over-use	Small
<i>Tilia cordata</i> Littleleaf Linden 'Glenleven' 'Greenspire'	Non-Native	Aphid & borer problems; suckers from base; messy species	Medium

Urban Boulevards, Constrained Conditions

Tree Species	Native Range	Comments and Notes	Size
<i>Tilia Americana</i> Basswood	Native to Ontario	Prefers deep moist fertile soil will grow on drier heavier soil needs large space	Large
<i>Ulmus japonica x Ulmus wilsoniana</i> Accolade Elm Princeton Morton Morton Glossy Frontier	Non-Native, Hybrid species	Tolerant of urban conditions and moist soil. These cultivars are currently shown to have good resistance to Dutch Elm Disease, but this is something in flux and should be constantly reassessed.	Large
<i>Ulmus americana</i> (DED resistant cultivars) Elm 'Homestead' 'Pioneer' 'Jefferson'	Native to North America (cultivar)	Tolerant of urban conditions and moist soil. These cultivars are currently shown to have good resistance to Dutch Elm Disease, but this is in flux and should be constantly reassessed.	Large

Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<i>Abies balsamea</i> Balsam Fir	Native to Ontario	Park	Dense, conical form. Prefers moist, cool locations	Large
<i>Acer griseum</i> Paperbark Maple	Non-Native	Park	Compact form, exfoliating bark. Prefers moist soil.	Medium
<i>Acer nigrum</i> Black Maple	Native to Ontario	Softscape Boulevard	Lots of seed for winter interest/rare/needs moist soil	Large
<i>Acer pseudoplatanus</i> Sycamore Maple	Non-Native	Softscape Boulevard	Very pollution and salt tolerant. Cankers can cause high maintenance	Large
<i>Acer rubrum</i> Red Maple 'October Glory' 'Red Sunset'	Native to Ontario	Softscape Boulevard	Green summer foliage & yellow to red fall colour. Tolerates wet soil/ poor drainage.	Large
<i>Acer saccharinum</i> Silver Maple	Native to Ontario	Softscape Boulevard	Fast growing softwood maple; Maintenance issues as tree nears maturity due to weak wood. Tolerates wet soil/ poor drainage.	Large
<i>Acer saccharum</i> Sugar Maple	Native to Ontario	Softscape Boulevard	Upright form/fall colour varies/prefers good drainage/shallow roots/salt sensitive	Large
<i>Acer x freemanii</i> Hybrid Soft Maple	Native to Ontario	Hardscape Boulevard	Caution: Many cultivars of <i>Acer rubrum</i> and <i>A. saccharinum</i> exist under the name <i>Freemanii</i> , each with different characteristics	Large
<i>Aesculus glabra</i> Ohio Buckeye	Native to Ontario	Park	Showy flowers. More resistant to anthracnose than other chestnut species.	Medium
<i>Aesculus hippocastanum</i> Horsechestnut 'Baumannii'	Non-Native	Park	Good spring flower with no fruit/limit use due to disease susceptibility	Large
<i>Aesculus flava</i> Yellow Buckeye	Native to North America	Park	Showy flowers. More resistant to anthracnose than other chestnut species.	Medium
<i>Amelanchier arborea</i> Downy Serviceberry	Native to Ontario	Naturalizing	Showy flower & fruit/ tolerant of wet & dry soil	Medium

Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<i>Amelanchier laevis</i> Smooth Serviceberry	Native to Ontario	Softscape Boulevard	Multi-stem specimens by prior approval only	Small
<i>Amelanchier canadensis</i> Shadblow Serviceberry	Native to Ontario	Softscape Boulevard	Difficult to maintain single stem. Four-season interest. Tolerates moist soil	Medium
<i>Asimina triloba</i> Pawpaw	Native to Ontario	Park	Large fruit has food value to humans	Large
<i>Betula alleghaniensis</i> Yellow Birch	Native to Ontario	Park	Interesting bark features and good fall colour	Large
<i>Betula papyrifera</i> White Birch	Native to Ontario	Park	Interesting bark features and good fall colour. Susceptible to Bronze Birch Borer, use in limited circumstances.	Large
<i>Betula populifolia</i> Grey Birch	Native to Ontario	Park	Ornamental bark. Susceptible to Bronze Birch Borer, use in limited circumstances.	Large
<i>Betula nigra</i> River Birch	Native to North America	Park	Ornamental bark. Susceptible to Bronze Birch Borer, use in limited circumstances. Tolerant of poor drainage.	Large
<i>Carpinus caroliniana</i> Blue beech or Musclemwood	Native to Ontario	Softscape Boulevard	Difficult to transplant. Keep away from road salt & spray, likes wet soil thin bark and sculptured trunk	Medium
<i>Carpinus betulus</i> European Hornbeam	Non-Native	Park	Difficult to transplant. Keep away from road salt & spray	Large
<i>Catalpa speciosa</i> Northern Catalpa	Native to North America	Park	Pods can be messy	Large
<i>Carya cordiformis</i> Bitternut Hickory	Native to Ontario	Park	Difficult to transplant due to large tap root. Messy fruit	Large
<i>Carya glabra</i> Pignut Hickory	Native to Ontario	Park	Difficult to transplant due to large tap root. Messy fruit	Large
<i>Carya laciniosa</i> Shellbark Hickory	Native to Ontario	Park	Difficult to transplant due to large tap root. Messy fruit. Sensitive	Large

Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<i>Carya ovata</i> Shagbark Hickory	Native to Ontario	Park	Difficult to transplant due to large tap root. Messy fruit	Large
<i>Carya tomentosa</i> Mockernut Hickory	Native to North America	Park	Difficult to transplant due to large tap root. Messy fruit. Can tolerate dry sites but prefers deep fertile soil.	Large
<i>Celtis occidentalis</i> Common Hackberry	Native to Ontario	Hardscape Boulevard	Requires pruning for general form. Very hardy. Tolerant of poor drainage.	Large
<i>Cercidiphyllum japonicum</i> Katsura Tree	Non-Native	Softscape Boulevard	Specify single or multi-stem. Difficult to transplant. Thin bark. Needs supplemental water.	Large
<i>Cercis canadensis</i> Redbud	Native to Ontario	Softscape Boulevard	Seeds readily. Suitable for lawns but not formal boulevard due to low branching.	Medium
<i>Chamaecyparis nootkatensis</i> 'Pendula' Weeping Nookta cypress	Native to North America	Park	Drooping branches add landscape appeal	Medium
<i>Cladrastis kentukea (lutea)</i> Yellowwood	Native to North America	Park	Few problems/use local seed sources or stock only/prune early. Sensitive	Large
<i>Cornus alternifolia</i> Alternate-leaf Dogwood	Native to Ontario	Softscape Boulevard	Use local winter hardy material only. Specify single stem	Medium
<i>Cornus florida</i> Eastern Flowering Dogwood	Native to North America	Park, Naturalizing	Endangered. Susceptible to dogwood anthracnose. Plant in limited quantities only. Symbol of Norfolk County	Small
<i>Cornus kousa</i> Kousa dogwood	Non-Native	Park	Resistant to dogwood anthracnose; berries have human food value	Small
<i>Cornus racemosa</i> Grey dogwood	Native to Ontario	Hardscape Boulevard	Specify single stem. Tolerant of poor drainage.	Small
<i>Corylus americana</i> American Hazel	Native to Ontario	Park, Naturalizing	Shrub or bush forming. Valuable food source for wildlife	Small

Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<i>Corylus colurna</i> Turkish Hazel	Non-Native	Park	Good form/ difficult to transplant/ winter interest/ needs supplemental water	Large
<i>Crataegus (varieties)</i> Hawthorns	(Dependent on species)	Softscape Boulevard	Thornless & disease resistant varieties only. <i>Crataegus monogyna</i> is invasive, not to be used	Medium
<i>Fagus sylvatica</i> European Beech	Non-Native	Park	Needs moist soil/different leaf colours with varieties/sensitive to activity within root zone/leaves persist through winter/thin bark	Large
<i>Ginkgo biloba</i> Maidenhair tree (Male cultivar only)	Non-Native	Hardscape Boulevard	Good yellow fall colour, thin bark. Tolerant of city conditions & pollution. Slow growing but very large at maturity, virtually pest and disease free	Large
<i>Gleditsia triacanthos var. inermis</i> Thornless Honey Locust 'Shademaster' 'Skyline'	Native to North America	Hardscape Boulevard	Provides a filtered shade/susceptible to defoliation by leafhopper/susceptible to canker and other pests and diseases	Large
<i>Juglans cinerea</i> Butternut Walnut	Native to Ontario	Park, Naturalizing	Messy fruit, needs large area. Fungal disease severely affects this species.	Large
<i>Juglans nigra</i> Black Walnut	Native to North America	Park	Messy fruit/needs large area.	Large
<i>Juglans regia</i> Persian Walnut	Non-native	Park	Messy fruit/needs large area.	Large
<i>Liquidambar styraciflua</i> Sweetgum	Native to Eastern North America	Softscape Boulevard	Carolinian species, uncommon as street tree in Ontario.	Large
<i>Liriodendron tulipifera</i> Tulip tree	Native to Ontario	Softscape Boulevard	Good flowers and yellow fall colour/local sources/moist well drained soil/very large tree most appropriate for lawn areas/ somewhat weak wooded	Large

Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<i>Maackia amurensis</i> Amur Maackia	Non-Native	Softscape Boulevard	Small, round headed tree/slow growing/ summer, flowering/bronze coloured bark. Not for use near naturalized areas.	Small
<i>Magnolia acuminata</i> Cucumber tree	Native to Ontario	Park	Status: Endangered	Medium
<i>Malus</i> Flowering & Domestic Crab Apple	(Dependent on species)	Park	Maintenance problems. Disease & insect problems. Tolerates most soils. Choose persistent fruit-holding, or poorly-fruited types.	Small to Medium
<i>Malus coronia</i> Wild Crabapple	Native to Ontario	Park		Large
<i>Nyssa sylvatica</i> Black Gum	Native to Ontario	Hardscape Boulevard	Carolinian species, tolerant of urban conditions, but uncommon as a street tree in Ontario. Tolerant of poor drainage.	Large
<i>Ostrya virginiana</i> Hop Hornbeam or Ironwood	Native to Ontario	Softscape Boulevard	Mainly an understory species	Medium
<i>Phellodendron amurense</i> Amur corktree	Non-Native	Softscape Boulevard	Good winter texture in bark, lots of black berries. Not appropriate near naturalized areas.	Medium
<i>Physocarpus</i> Common Ninebark	Native Ontario	Park, Naturalizing	Shrub or bush forming growing almost as wide as it does tall. Fast growing.	Small
<i>Picea abies</i> Norway spruce	Non-native	Park	Fast growing. Requires a lot of space. Shade tolerant.	Large
<i>Picea glauca</i> White spruce	Native to Ontario	Park	Slower growth rate than Norway spruce. Tolerates wide range of soil types	Large
<i>Pinus strobus</i> White Pine	Native to Ontario	Park	Locate with care in boulevards, due to possible sight line and access issues when mature (bushy) Avoid Ribes (alternate host for white pine blister rust)	Large

Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<i>Platanus occidentalis</i> Sycamore	Native to Ontario	Softscape Boulevard	Frost cracks on trunk/attractive peeling bark/fruit can cause problems/very large at maturity – reserve for large lots and lawn areas	Large
<i>Platanus x acerifolia</i> London Planetree	Hybrid of <i>Platanus occidentalis</i> (N. America) and <i>Platanus orientalis</i> (Europe), so has no native range	Softscape Boulevard	Prone to frost cracks on trunk. Attractive peeling bark. Fruit can cause problems. Very large at maturity – reserve for large lots and lawn areas	Large
<i>Populus ssp.</i> Balsam Poplar, Eastern Cottonwood, Large-tooth Aspen, Trembling Aspen	Balsam Poplar, Eastern Cottonwood, Large-tooth Aspen: Native to Ontario	Park, Naturalizing Not permitted in Boulevard	Wood is light, soft and weak, breaks easily in storms. Drops flowers, fruit, twigs and branches; tolerant of poor drainage	Large
<i>Populus ssp.</i> Dwarf varieties (eg. <i>Populus balsamifera</i> 'Paskapoo')		Park	Limited numbers may be considered in Boulevards on a trial basis	Medium
<i>Prunus spp</i> Flowering Cherry <i>Prunus sargentii</i> <i>Prunus kwanzan</i> <i>Prunus x yedoensis</i>	(Dependent on species; most popular flowering cherries are non-continental)	Softscape Boulevard	Weeping cankers; prone to fungal infections. For use in limited circumstances	Small
<i>Prunus Americana</i> American plum	Native to Ontario	Park	Somewhat thorny. Not Suitable for boulevards	Small
<i>Prunus nigra</i> Canada plum	Native to Ontario	Park	Somewhat thorny. Not suitable for boulevards	Medium
<i>Prunus pensylvanica</i> Pin Cherry	Native to Ontario	Park; naturalizing	Excellent flowers with no fruit. Single stem to be specified. Weeping cankers. Not specimen tree.	Small

Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<i>Prunus serotina</i> Black Cherry	Native to Ontario	Park; naturalizing	Interesting bark, messy fruit. Better in lawns than in formal boulevard.	Large
<i>Prunus virginiana</i> Choke Cherry	Native to Ontario	Park; naturalizing	Green spring foliage & red in summer/bark. Tends to split. Black knot. Not specimen tree.	Small
<i>Ptelea trifoliata</i> Hop Tree	Native to Ontario	Softscape Boulevard	Easily grown in average, dry to medium, well-drained soils in part shade to full shade. Tolerates full sun. One of two native larval host plants for the rare Giant Swallowtail butterfly. Adaptable to wide range of growing conditions.	Medium
<i>Quercus alba</i> White Oak	Native to Ontario	Softscape Boulevard	Needs moist soil/fruit maintenance/needs large space at maturity	Large
<i>Quercus ellipsoidalis</i> Northern Pin Oak	Native to Ontario	Softscape Boulevard	Vulnerable to iron deficiency	
<i>Quercus macrocarpa</i> Bur Oak	Native to Ontario	Hardscape Boulevard	Large size at maturity – reserve for large lots and lawn areas fruit drop. Difficult to transplant; requires good soils	Large
<i>Quercus robur</i> 'Fastigata' Fastigate English Oak	Non-Native	Park	Needs well drained soil holds leaves through the winter difficult to transplant very upright in form – reserve for sites with specific need for this form	Large
<i>Quercus robur</i> English Oak	Non-Native	Park	Needs well drained soil/difficult to transplant/large size at maturity	Large
<i>Quercus muehlenbergii</i> Chinquapin Oak	Native to Ontario	Softscape Boulevard	Attractive tree, especially in old age. Limited planting, due to possibility of Oak Wilt entering Ontario	Medium
<i>Quercus rubra</i> Red Oak	Native to Ontario	Hardscape Boulevard	Limited planting, due to possibility of Oak Wilt entering Ontario. Needs sandy loam soil difficult to transplant more salt tolerant and faster growing than other oaks	Large
<i>Quercus shumardii</i> Shumard Oak	Native to Ontario	Park	Listed as 'special concern' in Ontario. Prefers moist, well-drained soils.	Large

Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<i>Quercus velutina</i> Black Oak	Native to Ontario	Park	Limited planting, due to possibility of Oak Wilt entering Ontario. Needs well drained soil/difficult to transplant/large size at maturity	Large
<i>Rhus ssp.</i> Staghorn Sumac, Smooth Sumac, Fragrant	Native to Ontario	Park; naturalizing	Spreads quick, freely suckers from roots creating wide spreading colonies. Tolerates dry sterile soils	Small
<i>Sassafrass albidum</i> Sassafrass	Native to Ontario	Park, Naturalizing	Prefers sandy soils	Medium
<i>Sophora japonica</i> Japanese Pagoda Tree	Non-Native	Hardscape Boulevard	Excellent white flower green stem when young. Limit use due to messy characteristics	Large
<i>Staphylea trifolia</i> Bladdernut	Native to Ontario	Park, Naturalizing	Shrub or bush forming, usually found in lowlands or along streams.	Small
<i>Syringa reticulata</i> Japanese Tree Lilac (Ivory Silk)	Non-Native	Hardscape Boulevard	Good white summer flower. Excellent small specimen. Prone to over-use	Small
<i>Tilia Americana</i> Basswood	Native to Ontario	Hardscape Boulevard	Prefers deep moist fertile soil. Will grow on drier heavier soil. Needs large space	Large
<i>Tilia cordata</i> Littleleaf Linden 'Glenleven' 'Greenspire' 'Greenglobe'	Non-Native	Hardscape Boulevard	Aphid & borer problems; suckers from base	Medium
<i>Tilia tomentosa</i> Silver Linden	Non-Native	Softscape Boulevard	Heat and drought tolerant.	Medium
<i>Tilia x euchlora</i> Crimean Linden	Non-Native	Softscape Boulevard	Fruit messy suckers from base. For use in limited circumstances	Medium
<i>Tsuga canadensis</i> Eastern Hemlock	Native to Ontario	Park	Prefers moist soils and sheltered areas. Tolerates shade	Large

Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<i>Ulmus Americana</i> Elm 'Homestead' 'Pioneer' 'Jefferson'	Specific cultivars hybridized for disease resistance	Hardscape Boulevard	Tolerant of urban conditions and moist soil. These cultivars are currently shown to have good resistance to Dutch Elm Disease, but this is in flux and should be constantly reassessed.	Large
<i>Zelkova serrata</i> Japanese Zelkova 'Green Vase' 'Village Green'	Non-Native	Softscape Boulevard	Rapid growth Narrow branch angles promote fork split. Frost susceptibility when young	Large

Trees to be Avoided

Tree Species	Native Range	Comments and Notes	Size
<i>Acer platanoides</i> OR <i>Acer x platanoides</i> Norway Maple (many cultivars) or Norway maple cross	Non-Native	Surface roots conflict with and turf/girdling roots/aphid and wilt problems. Invasive.	Medium
<i>Acer negundo</i> Manitoba maple	Native to North America	Weak-wooded	Large
<i>Ailanthus altissima</i> Tree of Heaven	Non-Native	Invasive, host for spotted lantern fly	Large
<i>Alnus glutinosa</i> European Alder	Non-Native	Tolerant of wet & dry soil. Invasive tendencies checked by dry sites.	Medium
<i>Caragana arborescens</i> Siberian Pea-shrub	Non-Native	Toxic	Small
<i>Eleagnus angustifolia</i> Russian Olive	Non-Native	Invasive	Medium
<i>Fraxinus spp.</i> Ash (all species)	Native to Ontario	Susceptible to Emerald Ash Borer.	Large
<i>Morus spp.</i> Mulberry	Non-Native	Invasive	Large
<i>Paulownia spp.</i> Princess Tree	Non-Native	Invasive (within the United States)	
<i>Pyrus calleryana</i> Callery Pear 'Chanticleer' 'Bradford'	Non-Native	Fireblight problems, weak wooded	Small
<i>Robina pseudoacacia</i> Black locust	Non-Native	Invasive	Large
<i>Sorbinia spp.</i> Mountain Ash	Non-Native	Fireblight problems	Medium

Trees to be Avoided

Tree Species	Native Range	Comments and Notes	Size
<i>Ulmus pumila</i> Siberian Elm	Non-Native	Invasive	Large
<i>Chionanthus virginicus</i> Fringe tree	Non-Native	Alternate host for Emerald Ash Borer	Medium