



Working together with our community

## Public Hearings Committee – October 05, 2021

Subject: OPNPL2020104 - County-led Official Plan Amendment to adopt a new Secondary Plan for Port Dover with supporting guidelines and to make other associated policy changes within the Norfolk Official Plan, including the Lakeshore Special Policy Area.

Report Number: CD 21-102  
Division: Community Development  
Department: Planning  
Purpose: For Information

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### Executive Summary:

The County is undertaking a Secondary Plan to provide a clear and detailed planning framework for Port Dover's waterfront, downtown and future residential development within the urban boundary. The Secondary Plan will be used to guide municipal stewardship of the lake, river and natural heritage areas, create a vision for downtown and a framework to evaluate the appropriateness of development applications utilizing Urban Design guidelines for the urban area.

The Planning Partnership was retained by Norfolk County to prepare a Secondary Plan for Port Dover in March of 2020 in response to the implementation of an Interim Control By-law (ICBL). The Secondary Plan is intended to provide direction for the revitalization of the waterfront and Central Business District as well as a framework for residential development. The Secondary Plan will address leading edge development standards and urban design.

The planning process, to date, has included a number of virtual engagement events and four sessions of the Port Dover Secondary Plan Working Group (Working Group). The Working Group provided feedback directly to the Planning Partnership and staff regarding the draft plan and accompanying urban design guidelines. The public has had the opportunity to submit feedback directly to staff or through the project website. Comments received by the time of writing the report are attached in Appendix F.

This report is being presented as part of the statutory public meeting required by the *Planning Act*. The Draft Secondary Plan and accompanying Design Guidelines are attached as Appendix A. A presentation of the proposed Secondary Plan contents will be provided by The Planning Partnership. A future comprehensive report will be provided by staff containing a recommendation for Council consideration.

## Discussion:

### Public Meeting Details

A public meeting is a statutory requirement in accordance with the *Planning Act*, and is intended to allow members of the public to submit written or oral comments in relation to the proposed amendment to the Norfolk Official Plan. Additionally, any person may make written submissions at any time prior to County Council making its final decision on the application.

### Site Characteristics

Port Dover is located on the northern shore of Lake Erie along Highways 6 and is the second largest urban area in Norfolk County. The community has a permanent population of approximately 6,000 residents and welcomes thousands of seasonal tourists annually. It is a well-known and long-established tourist destination for Ontarians and Americans.

The Interurban Water Supply Study is well underway and Council has considered options to improve the water supply issue within Port Dover.

Port Dover has a long and storied cultural history. This foundation resulted in Port Dover growing into a tourist destination, with popular beaches and supportive commercial space activities.

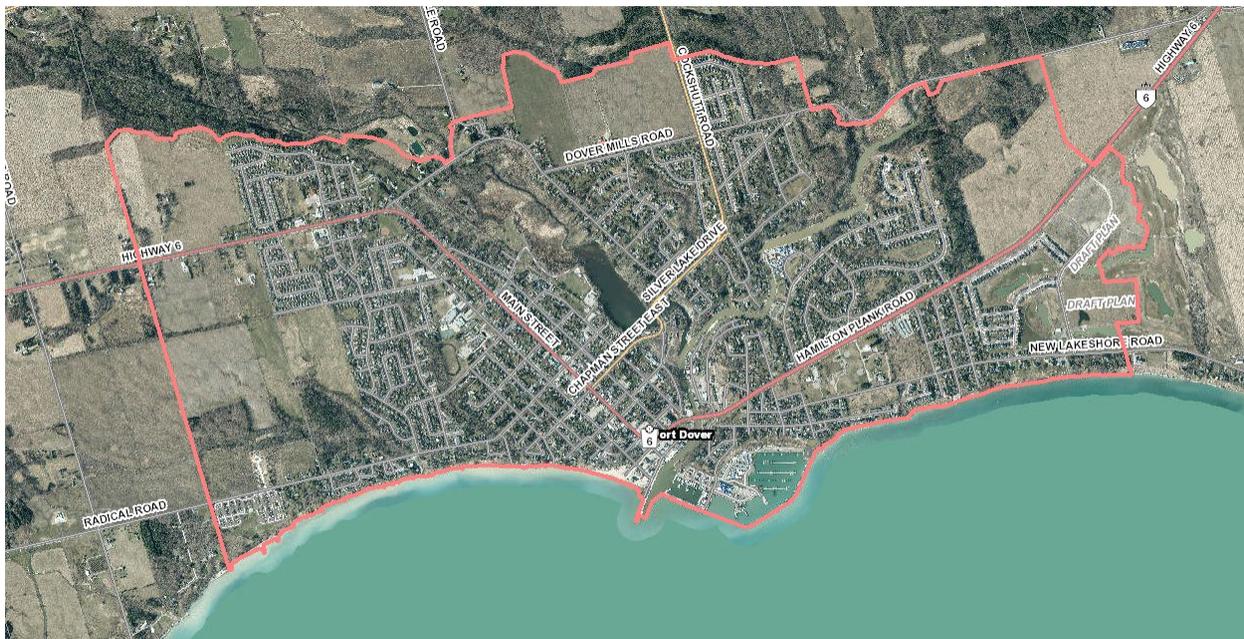


Figure 1. Port Dover, 2020 Air Photo

## Official Plan Amendment Summary

The County is undertaking a Secondary Plan to provide a clear and detailed planning framework for Port Dover's waterfront, downtown and future residential development within the urban boundary. The Secondary Plan will be used to guide municipal stewardship of the lake, river and natural heritage areas, create a vision for downtown and a framework to evaluate the appropriateness of development applications utilizing Urban Design guidelines.

In support of the current application, the following documents have been prepared by The Planning Partnership:

- Proposed Port Dover Secondary Plan including Urban Design Guidelines, dated September 2021 (Attached as Appendix A);
- Draft 1 – Port Dover Secondary Plan, including Urban Design Guidelines, dated July 2021; and
- Background Report, dated July 31, 2020 (Attached as Appendix B).

All submission materials are available online at:

<https://www.norfolkcounty.ca/government/planning/port-dover-secondary-plan/>.

Proposed policy changes within the Norfolk Official Plan, including the Lakeshore Special Policy Area and associated Community Design Guidelines and previous master plans may evolve as further review is completed and public input is considered. A list of Official Plan sections that are anticipated to be amended include:

### Norfolk Official Plan:

- Section 6.5.2 Port Dover Urban Area
  - 6.5.2.2 Port Dover Waterfront Special Policy Area
  - 6.5.2.3 Silver Lake Special Policy Area
  - 6.5.2.4 Lynn River/Black Creek Special Policy Area
- Section 7.16 Urban Waterfront Designation
  - 7.16.3 Port Dover – Bucks Orchard Beach Park Site Specific Policy Area
    - 7.16.3.2 Port Dover Wharf Site Specific Policy
- Section 10 Schedules
  - Part I (Official Plan)
    - Schedule A – Community Structure
    - Schedule B – Land Use
    - Schedule E – Transportation
    - Schedule I – Active Transportation
  - Part II Lakeshore Special Policy Area Secondary Plan
    - Schedule F – Community Structure
    - Schedule H – Neighbourhood Planning Areas

- Section 11 Lakeshore Special Policy Area Secondary Plan and associated Community Design Guidelines and Waterfront Master Plan
  - o Intent is to remove Port Dover from being subject to policies of the Lakeshore Special Policy Area Secondary Plan and associated Community Design Guidelines

### **Planning Considerations:**

From a legislative perspective (*Planning Act* section 22(2.1.1)), a secondary plan is an extension of the official plan that contains policies and land use designations that apply to multiple contiguous parcels of land intended to provide more detailed policy direction than was provided prior to the approval of the plan.

The Port Dover Secondary Plan contains a detailed policy framework and long term vision to consider. It is proposing new land use designations and permitted uses, height and density maximums, gateway locations, introduces the Block Plan process for future neighbourhood areas and provides comprehensive urban design and forestry guidelines.

The *Planning Act* also imposes a two year period where no person or public body can apply to amend the secondary plan before the second anniversary of the first day any part of the plan came into affect without a declared resolution from Council permitting a specific request, a class of requests or these requests generally.

One goal of developing the Secondary Plan was to simplify the policy framework, as it applies to Port Dover, to ensure that the policies provide clear direction regarding the long term vision for the community, development industry, staff and Council. The Secondary Plan contains new objectives and goals regarding building a successful community that may be beneficial to apply to the Norfolk County Official Plan.

The Secondary Plan proposes new land use designations and permitted uses that require further analysis and discussion to ensure that the proposed uses are successful within the community and that the long term vision can be implemented through the Zoning By-law.

The Secondary Plan proposes differentiating between existing residential uses and future neighbourhoods. These new land use designations require additional analysis to determine the differences between the existing land use policies of the Norfolk County Official Plan and the policies proposed through the Port Dover Secondary Plan to ensure that Port Dover does not become an anomaly within Norfolk County.

For example, the permitted use of neighbourhood centre within the Future Neighbourhood designation requires further review and analysis to understand how to successfully implement the concept. Amendments to the Zoning By-law to incorporate new zones or defined permitted uses may be required to ensure the successful implementation of the Secondary Plan.

In addition to above, The Planning Partnership is proposing to include a Significant Woodlands land use designation, which would be unique to Port Dover. The Norfolk County Official Plan identifies Significant Woodlots using an overlay to land use designations.

As a result of the Block Plan process proposed for the future Development Areas as identified on Schedule A: Community Structure of the Secondary Plan, planning staff will be required to develop additional procedures to implement this policy direction.

A summary of planning considerations including applicable provincial legislation and policy as well as relevant Official Plan policy is summarized in Appendix A. Further analysis and review will be contained in subsequent comprehensive recommendation report for Council to consider.

### **Consultation:**

#### **Notice Provisions**

Pursuant to the requirements of the *Planning Act R.S.O. 1990, C. P. 13* (“*Planning Act*”), a notice of the statutory public meeting was advertised in the Port Dover Maple Leaf newspaper as well as the Norfolk and Tillsonburg News 20 days in advance of the Public Meeting. Attached as Appendix D is the newspaper advertisement published in both newspapers.

The project website has been updated with the information regarding the Statutory Public Meeting and associated documentation. The project distribution list was emailed on September 13, 2021 providing the meeting details and information regarding how to participate.

#### **Technical Circulation Comments**

The during the preparation of the Secondary Plan, draft versions were circulated to a Staff Technical Committee, Port Dover Working Group and external agencies for review and comment. Planning staff consolidated the agency comments and provided feedback to The Planning Partnership to consider during the preparation of the proposed Secondary Plan attached to this report as Appendix A.

The proposed Secondary Plan has been circulated to the Staff Technical Team and comments will be contained and or/reflected in the subsequent report prepared for Council consideration.

Norfolk County is also reviewing and commenting on the Port Dover Secondary Plan as a landowner.

## Engagement

To date the following engagement has occurred during the preparation of the draft Port Dover Secondary Plan. Workshop events can be watched using the Port Dover Secondary Plan website or the Norfolk County YouTube channel:

- May 13<sup>th</sup> and May 14<sup>th</sup>, 2020 Council Interviews
- One-on-one Conversations with existing applicants within Port Dover included:
  - o June 2, 2020 – OPNPL2017009, ZNPL2018311& 28TPL2018312
  - o June 2, 2020 – ZNPL2019293, SPPL2017056 & SPPL2019059
  - o June 8, 2020 – OPNPL2019014 & ZNPL2020019
  - o July 7, 2020 – G Douglas Vallee, various applications
  - o July 14, 2020 – 28TPL2017317
- June 11, 2020 – Four topic based workshops
  - o Downtown
  - o Natural and Cultural Heritage
  - o Transportation
  - o Waterfront
- July 15, 2020 – Meeting with Port Dover Waterfront Preservation Association
- October 29, 2020 Workshops
  - o Downtown
  - o Waterfront
  - o Future Residential Areas
- July 12, 2021 Workshops
  - o River and Lakefront
  - o Downtown
- July 15, 2021
  - o Natural Features
  - o Residential Neighbourhoods
- Port Dover Secondary Plan Working Group (Notes attached in Appendix E)
  - o Meeting 1 – June 25, 2020
  - o Meeting 2 – September 20, 2020
  - o Meeting 3 – April 19, 2021
  - o Meeting 4 – July 21, 2021
- Staff attended the following meetings:
  - o Heritage Committee Meeting – Monday July 19, 2021
  - o Port Dover Board of Trade Meeting – Wednesday July 28, 2021

Additional consultation will occur with the development industry, Tourism and Economic Development Advisory Board and the Port Dover Lions Club prior to the comprehensive recommendation report being presented to Council for decision.

### **Working Group Comments**

The Working Group meeting notes have been attached as Appendix E. Key considerations include height, density, retaining the character of downtown, ensuring public access to the waterfront and rivers is provided, fostering opportunities for creative economic development which celebrates Port Dover's sense of community.

### **Public Comments**

All of the comments received from the public are attached as Appendix F and are summarized below.

The Port Dover Waterfront Preservation Association has been actively involved throughout the Secondary Plan process. Below is a summary of the correspondence attached in Appendix F. The association stressed that public access to the waterfront, including that of Silver Lake, the Lynn River, Black Creek and Lake Erie are integral to the future development of Port Dover. Encouraging the creation of a trail/pedestrian connection from the Lynn Valley Trail, around Silver Lake down to the shore of Lake Erie along Black Creek would add additional recreational opportunities within the community and enhance the preservation work being completed within and around Silver Lake.

There is an opportunity for Norfolk to utilize land currently owned by the County to help achieve and enhance the proposed trail from Silver Lake down to Lake Erie. Currently, Norfolk County leases 212 Nelson Street, a former Public Works Yard. The site is designated Urban Residential by the Official Plan. The Waterfront Preservation Association has requested that the County consider a re-designation of the property to Parks and Open Space in order to facilitate the proposed trail. The senior leadership team is currently reviewing the request and a formal recommendation from staff will be included in a comprehensive report presented to Council for a decision regarding the Secondary Plan.

The Port Dover Lions Club has also actively been participating throughout the Secondary Plan process. Staff will continue to work with the group while they prepare their formal comments regarding the Secondary Plan as their objectives include working with the Waterfront Preservation Association to maintain public access around Silver Lake, to provide opportunities for recreation and community gathering as well as protecting their development potential for the future.

Climate Action Committee of the Long Point Biosphere Reserve provided comments (attached in Appendix F) and are briefly outlined below:

- Committed to promoting and supporting actions to mitigate and adapt to climate change;

- Note positive aspects of the plan which includes the promotion of active transportation, keeping light to a minimum, the use of permeable paving;
- Interested in reducing or limited the number of drive-throughs;
- Support the planting and location of appropriate native plants;
- Note that many of the existing natural spaces within Port Dover are littered and full of invasive species which inhibits native plant growth;
- Would like to see more regarding the decrease in single-use plastics;
- The installation of water fountains/filling stations in highly-frequented areas would help reduce the reliance on a highly discarded item; and
- Consider the installation of green energy micro-grids which are excellent ways to mitigate and adapt to a changing climate.

Other comments received related to the pedestrianization of downtown, concerns regarding the infill development policies being too restrictive, the identification of parking issues and discrepancies regarding the Draft Schedules.

Comments received in regards to specific parcels of land are under review by staff and responses will be included in the comprehensive recommendation report.

**Preliminary Review**

<b>Key Items</b>		<b>Preliminary Review</b>
Park / Trails		<p>Port Dover has an existing network of parks and trails that connect the urban area to the rest of Norfolk County.</p> <p>There are currently 17 parks located within Port Dover. Two parks are owned and operated by the Port Dover Lions Club - Harry Misner Memorial Park, 1025 Main Street and Port Dover Lions Sliver Lake Park, 302 St. Patrick Street. Both parks provide essential recreational and open space to the public.</p> <p>Schedule B: Land Use of the draft Secondary Plan conceptually illustrates the location of future parks that may be considered through the development of future subdivisions in accordance with the parkland policies and parkland dedication by-law.</p> <p>The Lynn River Trail begins from Silver Lake and extends northwest to Simcoe and the Lake Erie Waterfront Trail extends along the southern part of Port Dover along New Lakeshore Road, through downtown connecting to Radical Road in the west.</p> <p>Schedule D: Active Transportation Plan of the draft Secondary Plan, illustrates the location of existing off road trails and paved road shoulders as well as</p>

		provide conceptual locations for proposed trails and bike lanes. Notably, the draft Secondary Plan proposes a trail that extends from the Lynn River Trail at Silver Lake and generally follows the waters edge along Silver Lake, the Lynn River and Black Creek all the way to Harbour Street.
Parking		Parking has been identified as an issue in Port Dover, particularly in the downtown and waterfront areas. The draft Secondary Plan provides enabling policies for the County to complete a comprehensive parking study in the future that will help alleviate the parking issues currently experienced.
Natural Heritage System		The Natural Heritage System has been considered throughout the development of the Secondary Plan and additional policies regarding Significant Woodlots and tree retention are proposed.
Transportation (TIS)		As each new development application is considered, staff will determine if a transportation study is required to support the application during the pre-consultation meeting. The draft Port Dover Secondary Plan, through Schedule C: Roads Plan, illustrates the existing road networks and indicates conceptual locations for roads in the Future Neighbourhood designation. It is the intent of the Block Plan process to ensure that the future neighbourhood areas are planned comprehensively and that the road network is extended in a logical manner.
Housing		The draft Secondary Plan provides for a variety of housing options, densities and heights throughout Port Dover. Gateway locations and neighbourhood centres are intended to provide interesting focal points within the community, which will help Norfolk County provide a range and mix of housing types that will help achieve the intensification targets of the Norfolk County Official Plan.

Building Height		Each land use designation indicates a maximum height for the area. For many designations the maximum height indicated is 6 storeys although there are policies that outline requirements to consider when evaluating a proposed height higher than 6 storeys. Further consideration and refinement to the height policies is required to ensure that the policies enable the appropriate review to determine if the proposed height is compatible with the surrounding context, for example the built form and/or any natural features.
Jobs / Employment		It is important to consider whether the plan provides a framework that encourages economic development in addition to providing for tourism opportunities. Another goal of the plan is to accommodate a diverse population and ensuring that there are opportunities for a variety of employment opportunities will attract residents to Port Dover.
Industrial		The draft Port Dover Secondary Plan does not propose an Industrial or Light Industrial land use designation, however, staff are exploring whether there is an opportunity to permit a limited amount of uses that are compatible in Port Dover but provide some flexibility for future development opportunities.
Commercial		<p>Commercial uses are permitted by most of the land use designations. Further review of the Existing Neighbourhood and Future Neighbourhood designations are required to ensure that small scale neighbourhood commercial uses will be permitted, like in the Urban Residential land use designation of the Norfolk County Official Plan.</p> <p>It is recommended that a policies to permit a grocery store in the Port Dover Downtown Designation be considered to support residential uses.</p>
Retail		Retail uses are permitted within the Port Dover Downtown, Port Dover Urban Waterfront, Mixed-use and Commercial land use designations. The Port

		<p>Dover Downtown and Mixed-use designations limit the scale of to retail uses to a maximum size of 3,000 square metres of gross floor area per individual retail use.</p>
<p>Roads</p>		<p>Schedule C: Roads Plan of the draft Port Dover Secondary Plan illustrates the existing road network and classifies the roads as provincial highways, arterial, collector or local roads. Potential locations of future roads and classifications are shown. The road pattern should be coordinated with the Integrated Sustainable Master Plan.</p>
<p>Servicing (W &amp; WW)</p>		<p>Recently, Council has approved an approach for Environmental and Infrastructure Services to pursue in order to increase the water and wastewater supply to Port Dover. The draft Secondary Plan aims to ensure the logical phasing of additional infrastructure before the Development Areas are constructed through the Block Plan process and general phasing policies.</p>
<p>Design / Site Plan</p>		<p>Urban Design and Forestry guidelines have been prepared to accompany the draft Secondary Plan. The guidelines are comprehensive and provide guidance for the development of the downtown area, urban waterfront, infill in the existing residential area as well as direction in regards to the design of future neighbourhoods and commercial plazas.</p> <p>As noted above in the Planning Consideration section of this report, further analysis regarding the capacity of staff to implement the direction of the proposed guidelines is required.</p>
<p>Cultural Heritage</p>		<p>The cultural heritage of Port Dover is strongly connected to Lake Erie and the built form of its downtown and older neighbourhoods.</p> <p>Appendix II: Cultural Heritage Resources, identifies the properties that have been designated under the Heritage Act. Policies and design guidelines have been developed to enhance the existing cultural heritage of Port Dover, however, further investigation</p>

		and study is required to develop a comprehensive cultural heritage strategy for Port Dover and Norfolk County as a whole.
Waterfront		Preserving and ensuring that public access to the waterfront is achieved has been highlighted as an objective of the community. Through the draft Secondary Plan, waterfront refers to both the shoreline of Lake Erie and all waterbodies within Port Dover including Silver Lake, the Lynn River and Black Creek.

**Strategic Plan Linkage:**

This report aligns with the 2019-2022 Council Strategic Priority "Foster Vibrant, Creative Communities".

**Explanation:**

The Port Dover Secondary Plan will provide a policy framework to ensure that the urban area develops in a coordinated, efficient and cost effective manner. It will enhance the strong sense of community within Port Dover, provide urban design guidance that will foster a beautiful built form and enable a variety of development opportunities.

**Conclusion:**

The purpose of this report is to summarize the planning application proposal, provide the comments received from applicable departments and agencies, summarize comments received from member of the public, and to provide general information in relation to the overall application.

A detailed recommendation report, responding to any outstanding matters, including those brought forward by members of the public, will be submitted to Norfolk County Council at a future meeting.

**Recommendation(s):**

THAT Report PD 21-102, Public Hearing report for OPNPL2020104, be received for information;

AND THAT any comments received as part of the statutory public meeting be considered in a future recommendation staff report.

**Attachments**

Attachment A – Draft Secondary Plan and Urban Design Guidelines, dated September 2021, prepared by The Planning Partnership

Attachment B – Background Study, dated July 2020, prepared by The Planning Partnership

Attachment C – Planning Considerations

Attachment D – Notice – Newspaper Advertisements

Attachment E – Working Group Minutes

Attachment F – Public Comments

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# The Port Dover Secondary Plan

DRAFT September 2021



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## 1.0 INTRODUCTION

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### 1.1 Purpose

- a) The purpose of this Secondary Plan (this Plan) is to provide a detailed land use plan and policy framework to guide future growth and development within the Secondary Plan Area. The planning horizon is to the year 2036 to match the time horizons in the current Norfolk County Official Plan.

This Plan is also intended to provide a long-term planning framework to manage growth and development well beyond 2031, and is cognizant of the planning policy framework of the Provincial Policy Statement (2020), as well as the ongoing legislative changes of the Province that affect land use planning throughout Ontario.

- b) Port Dover is the second largest Urban Area in the County. The County will support and promote the continued development of Port Dover as a significant urban waterfront community and tourism node in the County. The County will support and promote Port Dover as a sustainable waterfront urban community focused on port and lake-based activities, and containing an efficient pattern of development. In the future, Port Dover will continue to grow and to evolve into a complete community that is well-balanced and attractive.

### 1.2 Administration

- a) All development in Port Dover shall conform to the vision, guiding principles, and policies of this Plan, along with:

- i) **Schedule A: Community Structure;**
- ii) **Schedule B: Land Use Plan;**
- iii) **Schedule C: Roads Plan;** and,
- iv) **Schedule D: *Active Transportation Plan*.**

- b) In addition, Urban Design Guidelines are attached as **Appendix I**, the Cultural Heritage Resources Map is attached as **Appendix II**, and Urban Forestry Guidelines are attached as **Appendix III**. The attached Appendices do not form a statutory part of this Plan.

- i) The Urban Design Guidelines are meant to guide the preparation of detailed development plans in accordance with the vision, principles and policies for Port Dover. It is the intent of this Plan that all new development be consistent with the Urban Design Guidelines; and,

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- ii) The Cultural Heritage Resources Map identifies the properties that have been listed or designated as significant cultural heritage resources within Port Dover. Those resources are subject to the policies of Section 5.7 of the Norfolk County Official Plan.
- iii) The Urban Forestry Guidelines provide best practices for cultivating a healthy urban forest, including appropriate species selection.
- c) This Plan includes sidebar notes and graphics that are provided to provide additional explanation to a number of key policies of this Plan. Sidebar notes and graphics are not a statutory element of this Plan.
- d) This Plan is to be read in its entirety, and all relevant policies are to be applied to all applications for development. This Plan is also to be read in conjunction with all the relevant policies of the Norfolk County Official Plan, which are to be conformed with when considering any application for development, or making any land use planning decision. Where there is a conflict between the policies of the Norfolk County Official Plan and this Plan, the policies of this Plan shall apply.
- e) In addition, all development shall be consistent with the relevant policies of the Provincial Policy Statement. Where this Plan makes reference to an Act of the Legislature or an Ontario Regulation, such reference shall include its successor upon amendment or replacement.
- f) This Plan shall conform with any decisions of the Ontario Land Tribunal (OLT), or its predecessors. Where there is a conflict between the policies of this Plan and a decision of the OLT, the decision of the OLT shall prevail.

## 2.0 VISION AND PRINCIPLES

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### 2.1 Vision Statement

Port Dover will remain a healthy, sustainable and complete community growing and prospering within the context of its 18 kilometres of waterfront and recognizing and celebrating the history and character of the community. Visual and physical access to the waterfront for the public is a crucial element of the community.

The Downtown and Urban Waterfront Districts combined will remain the heart and soul of Port Dover, as well as a major tourist attraction with new development contributing to the creation of successful mixed use districts and neighbourhoods.

New development will contribute to establishing beautiful neighbourhoods, a network of green spaces and protected natural areas, a connected trail system, safe streets and a variety of shops and services for both residents and tourists.

### 2.2 Guiding Principles

- a) The Secondary Plan includes Guiding Principles' that alongside the Vision Statement to give direction on how the physical aspects of the community will be developed.

**Principle 1:** Downtown Port Dover will thrive as a destination for residents and visitors alike to access shopping, restaurants, culture and entertainment while new development will be compatible in character, and will enhance adjacent streetscapes and public spaces.

**Principle 2:** Port Dover's vibrant Urban Waterfront District will continue to be a pedestrian focused tourist destination that provides for a mixture of land uses, including residential uses. Development will be compatible in character, and shall celebrate the waterfront. Physical and visual access to the waterfront will be preserved and enhanced.

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- Principle 3:** Throughout Port Dover, the public's visual and physical access to the waterfront will be a priority and will be preserved and enhanced over time. A variety of both land-based and water-based recreational activities will be promoted adjacent to the waterfront for both residents and tourists alike.
- Principle 4:** Existing neighbourhoods throughout Port Dover will evolve over time, and are expected to include small-scale intensification projects and Additional Residential Units. All new development within an existing neighbourhood will be compatible with existing development.
- Principle 5:** New neighbourhoods will be well connected, offer a range and mix of housing types (including affordable and rental housing), a mix of appropriately scaled retail and service commercial uses and community facilities, with green space connecting to the broader network.
- Principle 6:** Retail, service commercial uses, tourist-focused facilities and community facilities will remain important components of a complete community that will help to ensure Port Dover is a community to live, work, shop and play.
- Principle 7:** Cultural heritage conservation and enhancement will play an important role when new development is proposed on a site that includes, or is in proximity to, an identified cultural heritage resource.
- Principle 8:** The health and connectivity of a network of public spaces, natural heritage features and their associated ecological functions will be protected and enhanced for the long-term.
- Principle 9:** A well-connected, multi-modal transportation network that gives priority to creating safe streets for pedestrians and cyclists while providing a balanced supply of parking will be provided throughout Port Dover. The concepts of complete streets and active transportation will be incorporated into all transportation planning decisions.
- Principle 10:** Development will be planned in a logical, efficient and cost effective manner, and will be coordinated with planning for transportation and municipal infrastructure.

## 3.0 BUILDING A SUCCESSFUL COMMUNITY

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### 3.1 Objectives for a Successful Community

- a) A successful community incorporates a range of elements that work in combination to ensure a well-functioning, beautiful and desirable place to live, to work and to play. A successful community is:
- i) ***A Complete Community*** - A complete community meets people's needs for daily living throughout an entire lifetime by providing convenient access to an appropriate mix of jobs, local services, a full range of housing, including affordable housing, public services and community infrastructure including educational and recreational facilities. A complete community has a robust open space system. Convenient access to options for Active Transportation are crucial elements of a complete community. Objectives are:
- » To accommodate a range and mix of housing types to allow for multiple choices and enhanced opportunities for residents to remain in the community for their entire lifecycle.
  - » To create a community that is walkable, with connected public gathering places, where opportunities for social interaction are increased and where commercial and community services can be provided within easy walking and cycling distances and that meet the needs of people of all ages, backgrounds, and capabilities throughout the various stages of their lives; and,
  - » To ensure easy access for all residents to a wide range of uses including shopping and restaurants, parks and open spaces, employment opportunities, educational and cultural opportunities, live-work options, mobility options, a mix and diversity of housing types, and a range of community facilities, services and amenities.

- ii) ***A Viable Community*** - A viable community has a strong economy that is market responsive, efficient and cost effective. Objectives are:
- » To attract long-term economic development opportunities by protecting for, and establishing opportunities for businesses and commercial activities focused on residents and tourists;
  - » To ensure that development is cost-effective and appropriate for the market place, including the flexibility to respond to, and ensure positive changes in the market place over time;
  - » To optimize the use of existing infrastructure investments and to promote the coordinated, efficient and cost-effective delivery of service infrastructure and community infrastructure that is appropriate for the planned urban development forms; and,
  - » To ensure that all development is sustainable and financially viable to the County over its life cycle.

- iii) ***A Beautiful and High Quality Community*** - A high quality and ultimately a beautiful community includes well designed buildings and streetscapes. A beautiful community protects its waterfronts and river banks, natural heritage features and viewscapes and includes an accessible and well-designed system of public parks and open spaces that celebrate the community, and provide opportunities for enjoyment by the entire population. A high quality community shall engender a sense of pride as a place to live and a sense of stewardship in its long-term care and maintenance. Objectives are:

- » To develop a welcoming community that encourages and supports active living, social engagement, civic pride and the creation of a sense of place and wellbeing. Fundamental to achieving this objective is the celebration of the views over the water and public access to the Lake Erie and Silver Lake shorelines and the river banks of the Lynn and Black Rivers;
- » To include gateways that clearly identify where you are, and when you have entered. Gateways help recognize entry points into the community. Gateways can include buildings, structural elements and/or landscape features;

- » To build beautiful streets and streetscapes. Streets need to accommodate all modes of transportation and be designed to be pedestrian friendly and safe. Building facades play a crucial role in defining the street edge, animating the street and creating the image and character of the community. Together the streets and the adjacent building facades create a streetscape;
- » To ensure that parks and open spaces, as well as Port Dover's waterfront, are beautiful, accessible and linked; and,
- » To require high quality architecture that transcends a theme or a specific period in time is fundamental to a beautiful community. Buildings shall be compatible with one another, but there must be a diversity of scale and a diversity of style as it may be defined through building materials, colour and architectural details.

iv) ***A Healthy Community*** - A healthy community consciously seeks to improve the health of its citizens by putting public health high on the social and political agenda. Physical, social and mental wellbeing are the necessary components of public health, including access to health care, healthy food, clean air and water, and opportunities for physical activity. A fundamental element of a healthy community is the inclusion of active transportation. Active transportation refers to any form of human-powered transportation - walking, cycling, using a wheelchair, scooters, inline skating or skateboarding. Objectives are:

- » To plan for an active transportation system that is highly integrated and connected within the community, and to transportation systems that serve the broader region;
- » To design the community around pedestrian activity with a substantial number of destinations, including parks, cultural and community facilities, shopping and restaurant opportunities within walking distance to promote walking and cycling that encourages daily physical activity;
- » To ensure that the appropriate level of infrastructure and amenities are provided along active transportation routes to ensure pedestrian comfort, and enjoyable and safe environments through which to travel; and,

- » To plan for “age-in-place” facilities within the community that anticipate changing housing needs for an aging population.

v) **A Sustainable Community** - Sustainability is commonly referred to as activities that meet present needs without compromising the ability of future generations to meet their own needs. A sustainable community is environmentally and socially healthy and resilient. It meets the challenges of climate change, and other environmental issues through integrated solutions rather than through fragmented, incremental approaches that meet one objective at the expense of the others. A sustainable community manages its human, natural and financial resources equitably and takes a long-term view - one that is focused on both present and future generations. Sustainability success relies upon having specific and measurable targets for indicators related to energy, water, carbon and waste. Objectives are:

- » To protect and enhance local and regional ecosystems and biological diversity;
- » To promote the responsible use of resources to ensure long-term sustainability, reduce greenhouse gas emissions, and reduce demands for energy, water and waste systems;
- » To demonstrate leadership in sustainable forms of green building design and technology, including the incorporation of renewable and alternative energy sources; and,
- » To require a transportation system that reduces the reliance on the automobile as the primary mode of transportation and promotes active transportation.

vi) **A Resilient Community** - A resilient community can effectively respond to emergencies because it has a plan in place, responsibilities assigned and facilities available. Natural or human made disasters are considered and the necessities of life are provided, particularly for those who are most at risk. Access to power, food, water and health care is ensured, while emergency services are equipped to operate and provide assistance in all conditions. Objectives are:

- » To ensure access to power, food, water and health care services during and immediately following a disaster event; and,

- » To ensure that health care and emergency service facilities are located within the community, and to ensure that they are adequately equipped to operate and provide assistance in all conditions.

## 3.2 Policies for a Successful Community

### *Promoting the Economy*

- a) Ongoing and enhanced economic development is a fundamental prerequisite to Port Dover's future success and sustainability. The attraction of real estate investment and tourism are key elements of the vision for the Port Dover community.
- b) The County will plan for a strong and healthy economy and anticipate changing economic trends. To help attract and retain a diverse and skilled labour force, the County will support a strong and healthy economy by:
  - i) Promoting ongoing opportunities for the expansion and diversification of Port Dover's retail and service commercial sector;
  - ii) Constructing, upgrading and maintaining high quality municipal infrastructure systems and community facilities;
  - iii) Facilitating efficient and convenient transportation options for the movement of people and goods; and,
  - iv) Supporting options for live/work units and planning for an appropriate range of Home-based businesses.
- c) The County will support tourism as an integral part of economic development. The County acknowledges that tourism will continue to bring economic benefits to Port Dover. Tourism is supported by having a healthy, livable and diverse community, which includes visual and physical public access to the waterfront, to the parks system and other outdoor leisure and recreational opportunities, which are connected by an integrated active transportation system. To further develop water-based tourism, opportunities shall be identified to:
  - i) Improve physical and visual public access to the waterfront;
  - ii) Improve existing marina facilities; and,
  - iii) Encourage year-round access, parking, accommodation facilities and related commercial uses to support water-based tourism activities.
  - iv) Ensuring an Adequate Housing Supply/Affordable Housing

d) Providing a range of housing types includes not only various forms, sizes and tenures, but also includes affordable and special needs housing. Residents of all ages, income levels and abilities rely on a range and mix of housing types to offer a meaningful place to grow and a safe and secure place to live. An appropriate range of housing choices contributes to the overall health and well-being of communities.

e) The County shall ensure a diversity of housing options are available to residents of all ages, abilities and incomes. The County will work with the private sector and other stakeholders to provide a full range of housing types to meet the projected demographic and market requirements of the current and future residents of Port Dover. The County may become directly involved in the supply of housing through land acquisitions and development partnerships.

f) The County will develop an affordable housing implementation framework to help achieve the affordable housing target of a minimum of 25% of new housing units in Port Dover. The County shall promote the supply of new affordable housing in a variety of locations, dwelling types and tenures. Policies for the provision of affordable housing in Port Dover are as follows:

- i) Affordable housing must include a mix and range of building types, lots sizes, unit types/sizes, and tenures to provide opportunity for all household types, including larger families and residents with special needs;
- ii) Identifying optimal sites, including publicly-owned lands for affordable housing, early in the development process to maximize affordable/accessible housing funding opportunities in consultation with the building industry, non-profit agencies and other stakeholders. This includes:
  - » Working with the development industry to achieve affordable/accessible housing targets;
  - » Encouraging the development of intrinsically affordable housing, which includes modest amenities, standard materials, minimal details and flexibility within units; and,
  - » Support for the development of additional residential units as a key component of intensification throughout Port Dover's existing neighbourhoods.

- g) Further, the County will consider innovative financial arrangements, tools, policies and partnerships to encourage the private sector and other stakeholders in the development and maintenance of non-profit and affordable/accessible housing, such as:
- i) Pre-zoning identified sites;
  - ii) The allocation of sewage treatment and water supply capacity;
  - iii) Community Improvement Plans;
  - iv) Grants, or other financial incentives, funded through the Community Benefits Charge By-law; and,
  - v) Reduced municipal fees and charges.

### ***Promoting Sustainability and Climate Change Mitigation***

- h) The County will use its array of planning tools to help guide development and redevelopment to be more sustainable. The County may consider the use of the Community Benefits By-law, Community Improvement Plans and associated incentive programs to assist with the implementation of sustainable development design standards, including the following initiatives:
- i) Maximizing vegetation to support improved air quality;
  - ii) Reducing greenhouse gas emissions;
  - iii) Maximizing opportunities for the use of renewable energy systems; and,
  - iv) Promoting the use of active transportation and reduced vehicle trips.

The County shall work to improve County-wide transit services, carpooling and other traffic demand management measures to reduce greenhouse gas emissions from transportation. The County may prepare Air Quality and Climate Change Reports to monitor it's progress towards reducing emissions of air pollutants and greenhouse gases, and to increase awareness of air quality and climate change.

### ***Conserving Water***

- i) The County shall promote water conservation by encouraging for all new development:
- i) Achieving 10% greater water efficiency than the Ontario Building Code and to encourage, through appropriate incentive programs, 20% greater water efficiency than the Ontario Building Code; and,

- ii) Including the installation of rainwater harvesting and re-circulation/reuse systems for outdoor irrigation and outdoor water use. In addition, other water conservation opportunities to be considered include the use of water efficient and drought resistant plant materials in parks, along streetscapes and in public and private landscaping, including;
- iii) Avoidance of turf grass areas, and when required, install drought resistant sod; and,
- iv) Reduce the impact caused by new development in the natural hydrological cycle by installing permeable driveway and parking lot surfaces.

## ***Conserving Energy***

- j) The County supports development that minimizes energy consumption, optimizes passive solar gains through design, and makes use of renewable, on-site generation and district energy options including but not limited to solar, wind, biomass, and geothermal energy. As such:
  - i) The County will promote reducing energy consumption in all County-owned, maintained and operated facilities and equipment., which shall be designed to at a minimum, LEED Silver Certification or similar standards to reduce energy consumption and incorporate renewable energy sources; and,
  - ii) Existing County buildings will be retro-fitted to implement more sustainable design construction features wherever possible; and,
  - iii) The County will encourage all new development to include energy efficient building design and practices and targets, including:
    - » Grade-related (3.5 storeys or less) residential buildings achieve a performance level that is equal to a rating of 83 or more when evaluated in accordance with Natural Resources Canada's EnerGuide for New Houses: Administrative and Technical Procedures;
    - » Medium density and high density residential (3.5 storeys and greater) and non-residential buildings be designed to achieve 40% greater efficiency than the Model Energy Code for Buildings, 1997; and,
    - » Non-residential and mixed use buildings shall be designed to achieve 25% greater energy efficiency than the Model National Energy Code for Buildings, 1997.

## ***Preserving and Enhancing the Waterfront***

- k) The County will preserve and enhance the public's visual and physical access to the waterfront in the following ways:
  - i) Where development is proposed, the provision of public access to the waterfront shall be pursued through:
    - » Parkland dedication required under the Planning Act, or purchases from funds allocated through the cash-in-lieu of parkland dedication; and/or,
    - » Negotiations through the development approval process; and,
  - ii) By pursuing initiatives outside of the development approval process, such as:
    - » Gifts/Bequests;
    - » Land exchanges;
    - » Purchases;
    - » Easement agreements;
    - » Partnering with individual landowners; and/or,
    - » Partnering with community organizations or service clubs.

## ***Protecting and Enhancing the Natural Heritage System***

- l) The County shall protect and enhance the natural heritage system within Port Dover. The relevant policies of Section 3.0 Sustainable Natural Heritage of the Norfolk County Official Plan shall be applied when considering all development within Port Dover.

## ***Forest Resource Enhancement***

- m) The County will preserve, protect, manage, replace and, where appropriate acquire, tree stands, hedgerows, woodlands and forested areas within Port Dover. It is a policy of this Secondary Plan that the County achieve a minimum of 40 percent tree canopy cover within Port Dover by 2031. To this end, the County shall:
  - i) Implement a Tree Protection By-law, which will include a tree replacement ratio where tree removal is unavoidable;
  - ii) Implement street tree and naturalization programs to increase urban canopy cover; and,

- iii) Require the planting of trees in all public works projects.

### ***Promoting Local Food Production***

- n) The creation of opportunities for local food production is supported by the County. Development plans and building designs shall provide opportunities for local food growing and production through:
  - i) Community gardens;
  - ii) Edible landscapes;
  - iii) Small scale food processing (i.e. community kitchens, food co-ops, community food centres);
  - iv) Food-related home occupations/industries; and,
  - v) A farmer's market.

### ***Conserving Cultural Heritage Resources***

- o) Cultural Heritage Resources include archaeological resources, built heritage resources and cultural heritage landscapes. The combination of human-made buildings and structures as well as the natural landscape create an area that is valued by the community. The listed and designated cultural heritage resources within Port Dover are identified in **Appendix II**. The County's policies for the conservation of cultural heritage resources are included in Section 5.7 of the Norfolk County Official Plan.

### ***Ensuring Compatible Development***

- p) The principle of compatible development will be applied to all applications for development within Port Dover to ensure the sensitive integration of new development with existing built forms and landscapes in a way that enhances the image and character of the community. The definition of compatible development is as follows:

"Compatible development is not development that is either the same as, or even similar to development in the vicinity. Compatible development is development that enhances the character of the existing community, without causing any undue, adverse impact on adjacent properties."

- q) Within the framework of compatible development, the County shall support contemporary architecture and design approaches that interpret and enrich living culture, and thereby creates a 21st century cultural heritage for future generations. To ensure development is compatible, when considering any application for development within Port Dover, particular attention shall be paid to the following:

- i) The landscape, built form and functional character of the surrounding community is enhanced;
- ii) No undue, adverse impacts are created on adjacent properties in the vicinity;
- iii) The height and massing of nearby buildings is appropriately considered, and buffers and/or transitions in height and density to adjacent properties are implemented, where necessary;
- iv) On-site amenity space is provided and it is reflective of the existing patterns of private and public amenity space in the vicinity;
- v) Streetscape patterns, including block lengths, setbacks and building separations are maintained; and,
- vi) Utility, infrastructure and transportation system capacity exists to serve the proposed development and there are no undue adverse impacts on the County's sewer, water, storm water management and transportation systems.

### ***Crime Prevention Through Environmental Design***

- r) The County will promote building and site design that assist in the reduction of the incidence of crime through the implementation of Crime Prevention Through Environmental Design (CPTED) principles including:
  - i) Adequate lighting;
  - ii) Clear sight lines, allowing view from one end of the walkway to the other;
  - iii) Appropriate landscaping, but avoiding landscaping that might create blind spots or hiding places;
  - iv) Adequate fencing and fenestration;
  - v) Clear signage that delineates permitted use and speed; and,
  - vi) Streetscape and building design that promotes 'eyes on the street'.

### ***Accessibility for Ontarians with Disabilities Act***

- s) The County has a duty to accommodate persons with disabilities that applies to all forms of development within Port Dover. The County will consider accessibility for persons with disabilities in all land-use planning and development decisions. As such:

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- i) All new and/or renovated County-owned, leased, or operated facilities, parks and open spaces, municipal infrastructure systems, and any other space that is accessible to the public, shall comply with the accessibility requirements as set out by the Ontario Building Code and the Accessibility for Ontarians with Disabilities Act; and,
- ii) All new private sector development shall incorporate universal physical access features and follow the accessibility requirements as set out by the Ontario Building Code and the Accessibility for Ontarians with Disabilities Act. Accessible private sector development shall be achieved through Site Plan Approval, enforcement of the Building Code, and the implementation of all applicable Provincial legislation and standards.

## 4.0 GROWTH MANAGEMENT

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### 4.1 Growth Projections

- a) Growth in Port Dover shall be established in the Norfolk County Official Plan, as it is amended from time to time. It is anticipated that as Port Dover builds out to its defined community boundary, it will have the capacity to accommodate between 20,000 and 25,000 residents through a combination of new neighbourhoods and residential intensification. It is expected that level of growth will create a significant demand for:
- i) Enhanced retail and service commercial facilities, as well as community facilities; and,
  - ii) Long-term and comprehensive municipal infrastructure capacity, including sewage treatment facilities and an adequate water supply.

It is also anticipated that the achievement of that level of growth in Port Dover will not occur within the time horizon of this Plan, and it will be appropriately phased in accordance with the phasing policies of this Plan.

### 4.2 Community Structure

- a) Port Dover is unique and will accommodate a varied range and type of growth and development. Overall the community is expected to be a complete community that will accommodate:
- i) A full range of housing types, including affordable and special needs housing;
  - ii) Business opportunities to provide a wide range of employment and services to residents, businesses and visitors;
  - iii) A concentration of public service facilities, including social, cultural, entertainment, health, educational and other supporting facilities; and
  - iv) an open space, natural heritage and recreational network that is integrated with open spaces throughout the County, and provides appropriate passive, natural and active areas; and,
  - v) Development on full municipal services, and an appropriate level of transportation infrastructure.

- b) Further, the County shall ensure through its planning activities that Port Dover develops with efficient land use patterns that minimize the extension of municipal service infrastructure and will sustain the community and financial well-being of the County over the long-term.
- c) **Schedule A - Community Structure** identifies, conceptually, the urban structure of Port Dover. It identifies a number of key geographic components of the community that help articulate where and how growth will occur, and that will consequently influence the community's success in achieving the identified vision for the future. **Schedule A** identifies the following components:
- i) **The Settlement Area Boundary** – The Settlement Area Boundary of Port Dover establishes the jurisdictional extent of this Secondary Plan. It is expected that all of the forecasted population and employment growth can be accommodated within the Settlement Area Boundary identified on **Schedule A**;
  - ii) **The Greenlands System** – The Greenlands System provides an important structural element throughout Port Dover, and generally identifies areas where development will be prohibited in order to protect natural heritage and hydrologic features and their ecological functions. The Greenlands System also identifies the existing parks and open spaces;
  - iii) **The Built-Up Area** – The Built-Up Area includes lands already developed with urban land uses. The Built-Up Area includes those areas within the Settlement Area Boundary where the intensification target is to be measured. The intensification target for Port Dover is and,
  - iv) **The Development Areas** – Development Areas include those vacant areas within the Settlement Area Boundary that are outside of the Built-Up Area, and include future development opportunities for Port Dover. The Development Areas are further subdivided into Development Areas 1 through 5 on **Schedule A**.
- d) Growth in Port Dover will occur through a combination of intensification within the Built-Up Area, and development within the Development Areas. These two areas have different policy frameworks to guide their planning, approval and development:
- i) **Intensification within the Built-Up Area - Schedule A** identifies the Built Boundary and the Built-Up Area of Port Dover. This Plan requires that a minimum of 25% of all new residential development within Port Dover shall occur within the Built-Up Area on an annual basis; and,

- ii) ***Growth within the Development Areas - Schedule A*** identifies the Development Areas of Port Dover. The Development Areas are expected to accommodate significant growth as they develop as primarily new neighbourhoods. Residential development in the new neighbourhoods shall achieve a minimum density target of 15 units per gross developable hectare, where gross developable hectare means the total land area, less any defined lands within the Hazard Land, Provincially Significant Wetland, and/or the Significant Woodlands Designations.
- e) All new residential development within Port Dover shall be subject to Section 5.3 of the Norfolk County Official Plan, including the requirement that all new housing shall contribute to the achievement of a minimum of 25% of all new housing to be affordable and accessible to low and moderate income households, of which at least 10% of all new units are affordable to low income households.
- f) The County shall monitor growth in Port Dover to ensure stated housing objectives are being met, and that growth is occurring in a logical and cost effective manner.
- g) Sites identified as a Gateway on **Schedule A** are intended to become identifiable entrances to Port Dover. Gateways abut, or are within the road's right-of-way associated with these key intersections, and it is expected that all corner sites associated with an identified Gateway will include:
  - i) Buildings and/or structures that reinforce the importance of the gateway location. This includes the use of high quality building materials, windows and entrances facing the streets and unified and consistent architectural detailing; and/or,
  - ii) Special landscape treatments, appropriate signage, and high quality streetscaping.

To facilitate the construction of an identified Gateway, partnerships among the County, developers and/or service clubs shall be explored. Further, the County may establish other Gateway locations throughout Port Dover to identify key Districts or features, including Gateways related to the Active Transportation System. No Amendment to this Plan is required for the County to identify and establish Gateways that are not conceptually identified on **Schedule A**.
- h) **Schedule A** identifies an Industrial Influence Area, and those lands are subject to Section 6.7.2 of the Norfolk County Official Plan.

## 4.3 Phasing Policies

- a) Fundamental to Port Dover's long-term ability to accommodate projected growth, is the provision of appropriate municipal infrastructure, including sewage treatment facilities and an adequate water supply. Norfolk County will continue to monitor available sewage treatment and water supply capacity to ensure that adequate facilities are planned and built in concert with expected development activity.
- b) All development approvals within Port Dover shall be explicitly linked to the ability of the County to provide municipal sewage treatment and water supply infrastructure, based on the capacity of both the wastewater treatment plant and the water filtration plant to accommodate growth, as well as commitments to the timing and funding of any required road, active transportation facility, public service facility, park dedication and/or storm water management facility.

These works, facilities and lands shall be provided for in subdivision and site plan agreements and development approvals shall be conditional upon the timing and funding of such facilities, works and/or lands, to the satisfaction of the County.

- c) The Town shall allocate capacity to individual development proposals on the basis of the following criteria:
  - i) Conformity with all of the relevant policies of this Plan, including, where applicable:
    - » Achievement of the required housing mix and Greenfield Density Target;
    - » Contribution to the Intensification Target; and,
    - » Contribution toward the Attainable and Affordable Housing Targets;
  - ii) Consistency with the guidelines of the Urban Design Manual, including the Green Building Technology approaches identified therein;
  - iii) The mix of land uses and the ability to accommodate, or support defined community needs, including Public Service Facilities; and,
  - iv) The status of individual development applications, with respect to:
    - » The proximity of the proposed development to existing and planned municipal water and wastewater systems;

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- » The need for, or the triggering of, capital works projects for existing and planned municipal water and wastewater system upgrades required to accommodate the proposed development; and,
- » The need for, and the timing involved, to undertake and obtain any required Environmental Assessment Approvals for municipal water and wastewater system upgrades required to accommodate the proposed development.

## 5.0 LAND USE POLICIES

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### 5.1 Land Uses Permitted in All Designations

#### 5.1.1 Public Uses and Utilities

- a) Public uses, and public and private utilities, including telecommunication facilities, are permitted in all land use designations in this Plan, subject to any regulatory requirements, such as the provisions of the Environmental Assessment Act., and in conformity with Section 8.7 of the Norfolk County Official Plan.

#### 5.1.2 Electricity Generation, Transmission and Distribution Systems

- a) Electricity generation facilities, transmission and distribution systems are permitted in all land use designations. Renewable energy systems are subject to Provincial legislation and regulations.

#### 5.1.3 Accessory Uses, Buildings and Structures

- a) Any use, building or structure which is normally incidental and subordinate to a use permitted by this Plan shall be permitted on the same lot as the permitted use, subject to the regulations of the Implementing Zoning By-law, and provided adequate water supply and waste disposal facilities are available.

### 5.2 Land Uses Prohibited in All Designations

- a) The following uses are prohibited in all land use designations in this Plan:
  - i) Uses that are noxious, polluting, or produce or store hazardous substances;
  - ii) Uses that involve the recycling and/or the storage of contaminated materials; and,
  - iii) Uses that are prohibited pursuant to the provisions of the Environmental Protection Act.
- b) The Implementing Zoning By-law shall incorporate provisions setting out those uses which are prohibited in all zone categories.

- c) Any proposed sites, or expansions to existing waste disposal sites, shall not be permitted in any land use designation.
- d) Recreational vehicle park development, other than in approved, site specific Official Plan Amendments or Zoning By-laws existing as of the date of adoption of this Plan, or in existing recreational vehicle parks, shall be prohibited in all land use designations of this Plan.

## **5.3 Land Use Specific Policies**

### **5.3.1 Low Density Residential Uses**

- a) Low density residential uses shall include single detached, semi-detached and townhouse dwelling units and shall not exceed a density of 40 units per net residential hectare.
- b) Additional Residential Units shall also be permitted in low density residential uses, subject to the policies of Section 5.3.3 of the Norfolk County Official Plan.

### **5.3.2 Medium Density Residential Uses**

- a) Medium density residential uses shall include all forms of residential buildings containing three or more dwelling units such as tri-plex and four-plex buildings, live/work buildings, street, block and stacked townhouses, and low-rise apartments. Medium density residential development shall generally shall be between 40 and 80 units per net residential hectare.
- b) Additional Residential Units shall also be permitted in street townhouse units, subject to the policies of Section 5.3.3 of the Norfolk County Official Plan.
- c) New medium density residential development may be permitted through an Implementing Zoning By-law, subject to the following conditions:
  - i) The site is within a designation that permits medium density residential development;
  - ii) The proposed development is compatible, and can be sensitively integrated with the surrounding land uses. Special measures, such as angular planes, increased building setbacks, or enhanced landscaped buffer strips may be required in order to ensure sensitive integration;
  - iii) The site is adequate in size and configuration to accommodate on-site parking facilities and amenities;
  - iv) The transportation, utilities and service infrastructure can adequately serve the proposed development; and,

- v) The community and neighbourhood amenities such as parks, open space, recreational facilities and institutional services, can adequately serve the proposed development.

### **5.3.3 High Density Residential Uses**

- a) High density residential uses shall include apartment buildings and all forms of multiple residential dwelling unit buildings having a density between 60 and 120 units per net residential hectare.
- b) New high density residential development may be permitted through an Implementing Zoning By-law, subject to the following conditions:
  - i) The site is within a designation that permits high density residential development;
  - ii) The proposed development is compatible, and can be sensitively integrated with the surrounding land uses. Special measures, such as increased building setbacks, angular planes, or landscaped buffer strips may be required in order to ensure sensitive integration;
  - iii) The site is adequate in size and configuration to accommodate on-site parking facilities and amenities;
  - iv) The transportation, utilities and service infrastructure can adequately serve the proposed development; and,
  - v) The community and neighbourhood amenities such as parks, open space, recreational facilities and institutional services, can adequately serve the proposed development.

### **5.3.4 Special Needs Housing**

- a) Special Needs Housing includes all forms of communal housing: Group Homes, Lodging Houses, Halfway Houses, Homes for Special Care and senior care facilities. Special Needs Housing includes all other types of residences licensed or funded under a federal or provincial statute for the accommodation of persons living under supervision in a single housekeeping unit and who, by reason of their age, emotional, mental, social or physical condition, require a group living arrangement for their well-being.
- b) Special Needs Housing shall be permitted in accordance with Section 5.3.3 of the Norfolk County Official Plan.

## 5.3.5 Home-Based Businesses

- a) There are two forms of home-based businesses:
  - i) Home occupations are small scale operations conducted by persons in their own home and tend to be professional or personal services; and,
  - ii) Home industries are typically carried out by the occupant of a property within accessory buildings on the property and may include artisan studios, maker spaces, small scale manufacturing, processing or repair uses with limited retail operations.
- b) Home occupations and artisan studios may be permitted in residences in accordance with the following provisions:
  - i) The use is carried out entirely within the dwelling unit, and can be appropriately accommodated within a residential structure;
  - ii) The use is clearly secondary to the primary use of the property as a residence in terms of floor space utilization;
  - iii) The property is the principal residence of the person carrying on the home occupation use;
  - iv) Outside storage of goods, materials, equipment or service vehicles such as trailers and commercially licensed vehicles related to the home occupation use shall not be permitted;
  - v) The activities associated with the home occupation use, including traffic generated and hours of operation, do not adversely affect the surrounding area;
  - vi) Adequate water supply and sewage disposal facilities are available and the requirements of the Ontario Building Code are satisfied;
  - vii) Solid waste beyond the volume normally generated by a household as defined by regional and provincial data is not permitted;
  - viii) The retail sales of any goods or wares shall not be permitted as a primary home occupation use; and,
  - ix) Compliance with on-site parking requirements and other provisions regulating home occupations in the Zoning Bylaw.
- c) The Implementing Zoning By-law may include additional provisions regulating Home Occupations.
- d) Home industries may be permitted in accordance with the following provisions:

- i) The use is carried out within an accessory building or structure separate from a residential dwelling;
  - ii) The use is clearly secondary to the primary use of the property and shall not detract from the primary use of the property;
  - iii) Only accessory retail sales of products directly produced by the home industry is permitted;
  - iv) The repair, storage or sale of motor vehicles is not considered to be a home industry;
  - v) The use must be compatible with adjacent uses; and,
  - vi) The use must be in compliance with any zoning and/or licensing provisions.
- e) The Implementing Zoning By-law may include additional provisions regulating home industries.

### **5.3.6 Live-Work Units**

- a) Live-work units have the potential to integrate small-scale service commercial, retail or office uses at-grade. Live-work units are typically in a street townhouse residential building and are subject to the associated development policies identified in this Plan. In addition, live-work units shall provide:
- i) Amenity areas and buffering with planting and/or fencing from adjacent residential dwellings; and,
  - ii) Adequate parking and drop-off/pick-up facilities.

### **5.3.7 Education Facilities**

- a) The County will work with the Boards of Education to ensure the reservation of an adequate number and distribution of school sites and related community facilities throughout Port Dover to accommodate the needs of residents. These sites and facilities shall be planned and developed in accordance with the respective policies, practices and guidelines of the School Boards.
- b) The County shall require the location of school sites to be adjacent to parks or other recreation facilities to allow for shared use of facilities and shall work with the Boards of Education to allow public use of school facilities, under appropriate agreements. The County shall also encourage the development of shared school buildings where feasible and when the Boards of Education's partnership criteria and policies can be met, to maximize the use of land and financial resources.

- c) The selection of school sites shall also consider safe connectivity between the school site and adjacent community, and the availability of community infrastructure that supports active transportation within the school catchment area.
- d) Education facilities may be permitted in any designation identified in this Plan, with the exception of Hazard Land, Provincially Significant Wetland and Significant Woodlands Designations through a Site Specific Zoning By-law Amendment. The Implementing Zoning By-law may include additional provisions regulating education facilities.

### **5.3.8 Community Facilities**

- a) Community facilities include facilities designed to meet the recreational, social, self-directed learning and cultural needs of the residents including public libraries, museums, cultural centres or other similar uses, excluding educational facilities. The County will work with community organizations to ensure that provision is made for such facilities in appropriate locations to serve the residents' needs.
- b) In determining appropriate locations for community facilities, the County shall have regard for the type of service provided by the facility, recognizing that some uses will serve a localized population, while others will serve the whole or large portions of the County, as well as tourists and other visitors.
- c) Where appropriate, community facilities will be located in community hubs to promote cost-effectiveness and facilitate service integration, access to transit and active transportation.
- d) Community facilities may be permitted in any designation identified in this Plan, with the exception of Hazard Land, Provincially Significant Wetland and Significant Woodlands Designations through a Site Specific Zoning By-law Amendment. The Implementing Zoning By-law may include additional provisions regulating community facilities.

### **5.3.9 Emergency Services**

- a) The County shall ensure the efficient and effective allocation of emergency services in a planned effort to keep pace with growth in consultation with Fire, Police and Emergency Medical Services.
- b) The County shall consult with the emergency service providers with respect to the establishment of station locations. Such stations shall have convenient access to arterial roads, a close relationship to the intended service area and shall be integrated with the surrounding development, including appropriate architectural design, landscaping and buffering from residential buildings.

- c) Emergency services may be permitted in any designation identified in this Plan, with the exception of Hazard Land, Provincially Significant Wetland and Significant Woodlands Designations through a Site Specific Zoning By-law Amendment. The Implementing Zoning By-law may include additional provisions regulating emergency services.
- d) Site plans and draft plans of subdivision will be reviewed to ensure that they are designed to accommodate fire prevention and timely emergency response.

### **5.3.10 Bed and Breakfast Establishments**

- a) Bed and breakfast establishments are permitted within any single-detached dwelling unit provided the use does not substantially alter the residential character of the property. The Implementing Zoning By-law may contain specific regulations pertaining to parking, signage and other matters associated with a permitted bed and breakfast use.

### **5.3.11 Day Care Facilities**

- a) Day care facilities may be permitted in a number of designations identified in this Plan, subject to specific regulations in the Implementing Zoning By-law and in accordance with the following policies:
  - i) The use will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads;
  - ii) The use is intended to serve and support the surrounding residential area; and,
  - iii) The site is large enough to accommodate the building, on-site play areas, parking/drop-off facilities and appropriate buffering, where required.

### **5.3.12 Small-Scale Places of Worship**

- a) New places of worship with the capacity to accommodate a congregation of less than 200 people shall be permitted in accordance with the policies of this Plan, through an Implementing Zoning By-law, subject to the following criteria:
  - i) The use will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads; and,
  - ii) The site is large enough to accommodate the building, on-site parking areas and appropriate buffering, where required.
  - iii) The use will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads;

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- iv) The site is large enough to accommodate the building, on-site parking and appropriate amenity areas and buffering, where required; and,
  - v) Direct access shall be provided to places of worship from all parts of the surrounding community through a comprehensive active transportation network.
- b) Small scale places of worship may be permitted in any designation identified in this Plan, with the exception of Hazard Land, Provincially Significant Wetland and Significant Woodlands Designations through a Site Specific Zoning By-law Amendment. The Implementing Zoning By-law may include additional provisions regulating small scale places of worship.

## 6.0 LAND USE DESIGNATIONS

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- a) The land use designations that apply within Port Dover are identified on **Schedule B: Land Use Plan**, and include:
- i) Port Dover Downtown Designation;
  - ii) Port Dover Urban Waterfront District Designation;
  - iii) Existing Neighbourhood Designation;
  - iv) Future Neighbourhood Designation;
  - v) Mixed use Designation;
  - vi) Commercial Designation;
  - vii) Major Institutional Designation;
  - viii) Parks and Open Space Designation;
  - ix) Industrial Designation;
  - x) Major Public Infrastructure Designation;
  - xi) Hazard Land Designation;
  - xii) Provincially Significant Wetland Designation; and,
  - xiii) Significant Woodlands Designation.

### 6.1 Port Dover Downtown Designation

#### 6.1.1 Intent

- a) Downtown Port Dover is the historic heart of the community and serves as a primary gathering place and location for events and festivities that give identity to the community. Downtown Port Dover has a mix of commercial, institutional and residential uses, and is anchored by Main Street. A grid pattern of streets create small blocks, making it easy to walk around and connect to the waterfront. Powell Park is a major focal point. Buildings in Downtown Port Dover exhibit a variety of materials, with brick and siding most commonly used. There are a number of buildings with a heritage designation or potential concentrated Downtown.

- b) The Port Dover Downtown Designation is intended to be an area that serves as the primary activity centre for Port Dover, and is an appropriate location for a wide range of uses, including retail, service commercial uses and restaurants, recreational, entertainment, business and professional, governmental, institutional, arts and cultural, community, employment and residential uses. Downtown Port Dover will be the focus of compact development oriented to the pedestrian scale.

## 6.1.2 Permitted Uses

- a) The diversity of land uses permitted within the Port Dover Downtown Designation shall foster the health and continued growth of the Downtown, while conserving and enhancing cultural heritage resources and community identity. Downtown Port Dover will be a focus of tourism and business activities, and will continue to grow and develop with the following permitted uses:
  - i) Retail and service commercial uses;
  - ii) Restaurants;
  - iii) Farmers' markets;
  - iv) Offices,
  - v) Accommodations for tourists and the travelling public including: hotels and motels, inns, bed and breakfast establishments, guest houses, lodge-style facilities, resort-oriented condominiums, or other similar forms of accommodation;
  - vi) Convention centres;
  - vii) Private clubs;
  - viii) Recreational and entertainment uses;
  - ix) Arts and cultural facilities;
  - x) Residential Apartments, including special needs housing;
  - xi) Small scale places of worship;
  - xii) Community facilities;
  - xiii) Institutional uses;
  - xiv) Emergency Services;
  - xv) Day Care Facilities;
  - xvi) Parking facilities at grade, or in structure; and,
  - xvii) Parks, open spaces and walkways/trails.

- b) In addition to the identified list of permitted uses, the following may also be permitted within the Port Dover Downtown Designation:
  - i) Uses accessory to any of the identified permitted uses; and,
  - ii) Pop-up uses and activities. The County may establish policies and procedures to facilitate the establishment of pop-up uses and activities.
- c) The County reserves the right, through the Implementing Zoning By-law, to further refine the list of permitted uses to ensure that new development is appropriate in the context of the adjacent and surrounding community.
- d) New Drive-through commercial facilities, as well as single detached, semi-detached and Townhouse dwellings are specifically not permitted within the Port Dover Downtown Designation.

### 6.1.3 Development Policies

- a) The County shall encourage the development, redevelopment and rehabilitation within the Port Dover Downtown Designation. Further, the County will encourage developments that increase the amount and intensity of residential apartments within the Port Dover Downtown Designation by supporting appropriate residential development and redevelopment, in accordance with the policies of this Plan and the associated Urban Design Guidelines attached to this Plan as **Appendix I**.
- b) All development within the Port Dover Downtown Designation shall be compatible with existing development in proximity. Where appropriate, historic streetscape patterns such as block lengths, building heights, setbacks and separations will be maintained.
- c) The County will encourage the establishment of a wide range of arts and cultural facilities and activities in the Port Dover Downtown Designation through:
  - i) The placement of public art and murals; and,
  - ii) The placement of wayfinding signage to direct residents and visitors to shops, services and attractions.

- d) Certain lands within the Port Dover Downtown Designation have been identified by the Long Point Region Conservation Authority as being Hazard Land. Consequently, any use of such land will be subject to the policies of Section 7.3 of the Norfolk County Official Plan. All development applications that are within the Hazard Land area within the Port Dover Urban Waterfront Designation shall be evaluated in consultation with the Conservation Authority.
- e) Linkages between the Port Dover Downtown Designation and the Active Transportation Network identified on **Schedule D** shall be created and enhanced to foster pedestrian activity and encourage tourism within the Port Dover Downtown Designation.
- f) Parks and open spaces shall be distributed throughout the Port Dover Downtown Designation, and these facilities shall be linked together and connected with the broader County network of natural and open space areas. All developments within the Port Dover Downtown Designation shall be required to provide public parkland, or cash-in-lieu of parkland. Public parkland shall be provided subject to the following:
  - i) Public open space to be dedicated shall be landscaped prior to conveyance in a manner satisfactory to the County; and/or,
  - ii) Where cash-in-lieu of parkland is accepted for development within the Port Dover Downtown Designation, the County shall allocate the funds generated for public parkland improvements within the Port Dover Downtown Designation. The funds generated shall be used to provide additional public parkland or for the aesthetic and/or functional improvement of existing public parkland areas.
- g) Where practical, the municipality may provide or cooperate with private landowners and other public agencies to provide additional open space facilities within Port Dover Downtown Designation. The development of open space facilities that can be the focus of community festivals, events and activities attractive to all segments of the community will be encouraged.
- h) Permitted retail uses shall be limited in scale to a maximum of 3,000 square metres of Gross Floor Area per individual retail use.
- i) The height, massing and layout of buildings within the Downtown Designation shall be oriented to a pedestrian scale. The maximum building height within the Port Dover Downtown Designation shall be 6 storeys, or 20 metres, whichever is less, and may be subject to the following provisions that will be more fully articulated within the Implementing Zoning By-law:

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- i) All development within the Port Dover Downtown Designation shall have a minimum height of 2 storeys; and,
- ii) All development within the Port Dover Downtown Designation shall incorporate a floor to ceiling height of the first floor of 4.5 metres.

Building heights greater than 6 storeys may be considered within the Port Dover Downtown Designation only through a Site Specific Amendment to this Plan and a Site Specific Zoning By-law Amendment, where appropriate mechanisms are employed to ensure compatibility and an appropriate transition to abutting properties. Mechanisms employed through the Site Specific planning policies and regulations may include a reduction in the permitted building height, the implementation of enhanced building setbacks, the requirement for landscape planting strips, the imposition of a step back and/or the imposition of an angular plane.

- j) All new development with frontage along Main Street, St. George Street or St. Andrew Street within the Port Dover Downtown Designation shall have at least 50% of its ground floor Gross Floor Area dedicated to non-residential uses, with a preference for retail and service commercial uses and restaurants. Residential uses shall only be permitted above the ground floor and/or on the ground floor in the rear of the building.
- k) Where residential uses are proposed in an existing building of commercial character, they shall only be permitted above the ground floor and on the ground floor in the rear of the building, provided that the street frontage is maintained for commercial uses. For the purposes of this policy, the commercial character of a building may be determined in consultation with the Chief Building Official of the County.
- l) In an existing building of residential character, residential uses and/or commercial uses shall be permitted, provided the residential character of the building is maintained. For the purposes of this policy, the residential character of a building may be determined in consultation with the Chief Building Official of the County.
- m) In the consideration of any Zoning By-law Amendment proposing to establish any of the permitted uses, the following shall be addressed:
  - i) Adequate and appropriate access to the property from a public road; and,
  - ii) The provision of adequate and appropriate parking and loading facilities.

- n) It is recognized that in the Port Dover Downtown Designation, it may not be feasible or desirable for all new developments to provide for parking on site. As such, the provision of parking spaces shall be subject to the following policies:
- i) Private and public parking lots will be encouraged at locations convenient to Main Street in Port Dover;
  - ii) On-street parking is important to the economic vitality of the Port Dover Downtown and will be maintained to the extent practical. The temporary use of on-street parking spaces for pop-up uses or outdoor cafes or eating areas may be permitted subject to the policies and procedures that may be established by Council;
  - iii) In reviewing development applications within the Port Dover Downtown, the County will recognize the importance of the Downtown mixed use context and will promote a comprehensive parking strategy that considers reduced parking standards for urban mixed use developments, or other classes of development, based on an understanding of opportunities for shared parking, on-street parking and the availability of public parking facilities;
  - iv) The County may accept cash-in-lieu of parking as an alternative to providing any required parking. Where cash-in-lieu of parking is accepted, the funds generated shall be used to provide additional public parking or the aesthetic and/or functional improvement of existing public parking areas within the Port Dover Downtown Designation; and,
  - v) Where a development cannot provide off-street parking on its site, the County may permit the provision of the required parking spaces on an alternative site, provided that the alternative site is within convenient walking distance of the proposed development, and the developer enters into an agreement with the municipality to ensure the continued availability of the alternative site as a parking area.
- o) The site plan control policies in accordance with the policies of Section 9.6.5 (Site Plan Control) of the Norfolk County Official Plan shall apply to all development applications in the Port Dover Downtown Designation.

- p) The County may undertake, by both direct municipal action and by encouraging the actions of other parties, a program of on-going improvements within the Port Dover Downtown Designation, including streetscape improvements, facilities for off-street and on-street parking, improved vehicular connections and circulation patterns, and facilities for those not traveling by private automobile, including improvements for pedestrians, cyclists, the elderly and physically-challenged, and those using taxis, transportation for the disabled and transit services.
- q) The County encourages the establishment of a Port Dover Downtown Business Improvement Area, in accordance with the Municipal Act, corresponding to the boundary of the Port Dover Downtown Designation. The County may promote a larger Business Improvement Area that combines the areas identified as within the Port Dover Downtown Designation and the Port Dover Urban Waterfront Designation.
- r) The County shall pursue the ongoing enhancement of the Port Dover Downtown in co-operation with the boards of management for the Business Improvement Areas (BIA) and other public and private interests and community groups, to enhance the efficiency, convenience, safety and appearance of the areas and the activities they accommodate. As such, the County may pass by-laws designating and delineating a Community Improvement Project Area associated with the Port Dover Downtown Designation, and may undertake Community Improvement Plans, in accordance with Section 9.5.2 (Community Improvement) of the Norfolk County Official Plan to improve public infrastructure and to stimulate private sector investment.

#### **6.1.4 Urban Design Policies**

- a) **Interpretation** - These Urban Design Policies are intended to guide new development within the Port Dover Downtown Designation in combination with the other relevant policies of this Plan and the Urban Design Guidelines attached to this Plan as **Appendix I**. It is recognized that as policies within the statutory part of this Plan, all new development within the Port Dover Downtown Designation shall conform to these Urban Design Policies. However, it is the intent of the County that progressive and innovative development is desirable within Port Dover, and the test of conformity with these Urban Design Policies may be interpreted with some flexibility, so long as the intent of these Policies is achieved, to the satisfaction of the County.

- b) **Context** - The core of the Port Dover Downtown Designation is focused along Main Street where, generally, buildings are located close to the street line and to one another, resulting in a well-defined streetwall. Commercial buildings are typically 2-storeys in height, with flat roofs. Many buildings have an historic character that create a distinct sense of place. At each end of Main Street, buildings are more widely spaced, typically one storey in height, with varying setbacks. On-street parking is replaced by vehicular lanes and parking is provided in large surface lots. These areas are more suburban in character and oriented to vehicular access. It is the objective of these Urban Design Policies to shape a more pedestrian-oriented and well-defined Main Street throughout the Port Dover Downtown Designation.

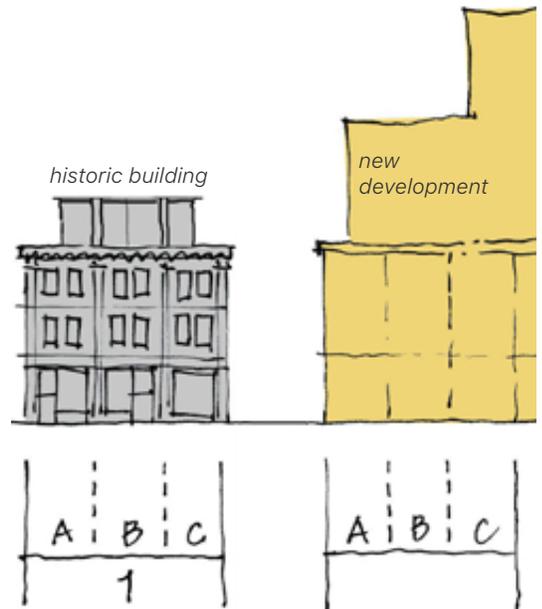
The streets around Main Street are more residential in character, although they also have commercial uses mixed in. Buildings have set backs from each other and from their neighbours while still defining the street edge. There is a mix of historic, traditional and more modern architectural styles. Parking is often accommodated in side driveways or garages set back from the street edge. Large mature trees make a significant contribution to the attractiveness of this area.

- c) **Cultural Heritage** - The County shall protect and enhance, where possible and appropriate, the historic architectural character found within the Port Dover Downtown Designation, particularly focusing on the pedestrian scale and design of the buildings and streetscapes, and the arrangement of windows and treatments on the building facades. Existing listed and designated cultural heritage resources within the Port Dover Downtown Designation are identified in **Appendix II**. Heritage-related design policies for new development within the Port Dover Downtown Designation include:

- i) The historic pattern of development in Downtown Port Dover has been characterized by ground floor commercial uses with narrow street frontages, frequent pedestrian entrances and windows that create pedestrian appeal. The design of new buildings and the redevelopment or adaptive re-use of existing buildings shall enhance the quality of the pedestrian environment by including transparent frontages, the articulation of facades and the use of quality materials at the street level;
- ii) New buildings shall relate to the traditional building stock, either through a traditional style that reflects the character of the area, or a more contemporary style that sets them apart from and highlights heritage buildings through a defined contrast and juxtaposition. Designs shall provide a clear distinction between 'new' and 'old'; and,



*New designs shall relate to the traditional building stock*



*Facade designs shall reference the articulation of neighbouring historic buildings*

- iii) Facade designs shall reference the articulation of neighbouring historic buildings, with respect to vertical and horizontal elements, including the rhythm and proportion of its main architectural elements.
- d) **General Design Policies** - The following policies apply everywhere within the Port Dover Downtown Designation:
  - i) A minimum 2-storey street wall shall be maintained facing public streets along all streets;
  - ii) Buildings shall be oriented to frame the street edge and to create a strong street wall;
  - iii) Buildings shall front directly onto public streets and other public spaces, in order to clearly define the public realm and create an attractive and safe pedestrian environment;
  - iv) Buildings at corner locations shall be sited to address the intersection, with consideration given to both street frontages;
  - v) Buildings shall align with the existing street wall. In residential areas, buildings shall generally be aligned with the setbacks of their neighbours;
  - vi) Main building entrances shall face the street and shall be clearly defined with architectural details and easily identifiable within the facade composition. Publicly accessible front entrances for stores, offices, and institutional uses shall permit barrier-free access and universal accessibility, including both visual and physical accessibility;
  - vii) Ground floor facades shall be highly transparent, including transparent windows and entrance doors, to establish a strong visual connection between the street and the interior of active ground floor uses; and,
  - viii) Facade rhythm may be established along a street wall through architectural articulation, including the use of fenestration, bands, columns, and other repeated elements.
- e) **Additional Main Street Design Policies** - The historic buildings along Main Street exhibit many urban design principles that these policies seek to reinforce. These principles are not about style, they are about the relationship of the building to the street. Additional design policies for development along Main Street within the Port Dover Downtown Designation are as follows:
  - i) Along the Main Street frontage, buildings shall provide a step-back of 2.0 metres above the street wall height - typically above the 2nd Storey;

- ii) Buildings shall be sited adjacent to the sidewalk and in line with the established street wall. They shall occupy the full width of their frontage unless there is a provision for public space or access. Building walls at the interior side lot lines will generally be blank, without windows or doors, in anticipation of future development that is also built to the same interior side yard;
- iii) Buildings fronting onto Main Street shall have active uses at grade, such as commercial, retail, office or institutional uses. A rhythm of fine-grain and narrow shop frontages shall be established;
- iv) Parking shall not be visible from Main Street. If present, it shall be located behind the building or in structure;
- v) Vehicular access to sites is discouraged along Main Street. Preferred access locations are from adjacent streets and shared laneways and driveways;
- vi) Where site access is unavoidably located along Main Street, minimize its width. Consider single lanes, one way lanes, and shared spaces that incorporate vehicular and pedestrian access together;
- vii) Sidewalks along Main Street shall be wider, with a minimum 2.0 metre unobstructed pedestrian clearway. In addition to the 2 m clearway, a landscape/ furnishing zone shall be provided adjacent to the curb for lighting, signs, seating and where feasible, planting. In addition to the clearway, additional space on the sidewalk adjacent to the building may be used as a Market Zone for outdoor patios and retail display; and,
- viii) For locations on Main Street that display a more suburban context, where more generous setbacks are provided, the space shall be used for landscaped areas, additional street tree planting, amenity areas, seating, display areas or sidewalk cafes and patios. Parking spaces shall not be located in the front yard setback space, or within the exterior side yard space on corner lots.

f) **Additional Design Policies for Residential Infill** - The following guidelines anticipate the potential for modest residential infill within the historic small lot fabric of the original survey grid within the Port Dover Downtown Designation:

- i) Avoid parking, driveways and garages along street frontages; these shall be located away from public view and preferably internal to the site, screened by buildings;



2-storey street wall

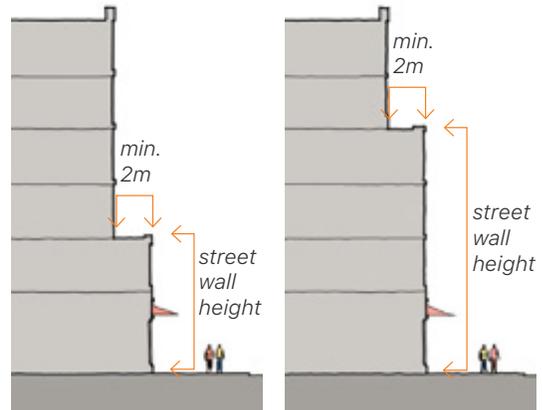


Buildings front directly onto streets and public spaces



Facade rhythm through architectural articulation

- ii) Buildings shall be placed to create a continuous street wall; the setback distance shall be determined with reference to the setback distances of adjacent buildings and provide sufficient transition from private front yards to the public street;
- iii) The massing and scale of the development shall be compatible with the existing and planned context;
- iv) Main entries shall be located on the front of the building and shall be highlighted in the architectural design;
- v) Ensure that end units display the same level of architectural detail and articulation as that of the main front elevation;
- vi) The interface between the front yard and the sidewalk shall be designed with a combination of low fencing, stone walls and/ or hedges and shrubs that enhance the character of the streetscape; and,
- vii) Detailed landscape treatments shall be coordinated with the main building materials and create a year round visually appealing presence along the street.



2 metre step-back above the street wall height

g) **Design Policies for Parking and Access** - Guidelines for parking and access within the Port Dover Downtown Designation generally relate to the design and functionality of on- and off street parking and access driveways. The objective is to provide suitable parking and driveway options while reducing traffic impacts and the negative visual impact of surface parking lots. The following design policies apply:

- i) Parking facilities shall be designed and located to minimize its impact on the streetscape and public realm to ensure that sidewalks and building facades define the street edge. As such:
  - » Parking will not be permitted between the right of way and the building face or within front yard setback areas;
  - » Side yard parking may be considered where site constraints are significant, but is generally discouraged;
  - » Rear yard parking, or encouraging patrons to park in designated parking lots, is preferred;
- ii) Parking lots shall be organized to minimize the number of potential pedestrian-vehicle movement conflicts. Where possible, parking areas shall be coordinated between multiple properties to maximize connectivity, improve traffic flow and increase the efficiency of parking;

- iii) Parking lots shall be designed to enhance the feeling of safety through techniques such as maintaining clear sight lines for natural surveillance of the parking lot as well as minimizing dark, hidden or obscured areas. Parking lot design shall consider:
  - » Planting strips and landscaped traffic islands, medians, or bump-outs shall also be provided within lots to break up the expanse of hard surface;
  - » Pedestrian-scaled lighting, walkways, landscaping, and signage to enhance pedestrian safety, movement and comfort through parking lots to the adjacent sidewalks;
  - » Pedestrian routes through parking areas shall be wide enough to accommodate comfortable travel and their walking surfaces shall be clearly differentiated from the parking areas with texture, material, colour changes or markings;
- iv) Where existing parking areas are adjacent to the sidewalk, a landscaped area shall be located between parking spaces and the sidewalk. Landscaping shall provide clear sight lines at eye level.
- v) Bicycle parking and racks shall be provided in locations that are close to building entrances but situated to avoid any conflicts for movement along pedestrian routes.
- vi) Where possible, access to parking areas shall be provided from side streets and laneways. Access to parking areas shall be defined through clearly designated entrances and exits.



*Landscaped medians and islands*



*Pedestrian-scaled walkways and landscaping enhance safety, movement and comfort*

## **6.1.5 Bucks Orchard Beach Park Site Specific Policy Area**

- a) On lands that are identified as Site Specific Policy 6.1.5 on **Schedule B**, in addition to the uses permitted in the Port Dover Downtown Designation, residential uses in accordance with the policies of the Existing Neighbourhood Designation of this Plan shall also be permitted. In the consideration of a Zoning By-law amendment to implement the policies of this Site Specific Policy Area, the following shall be addressed:
  - i) The compatibility of the proposed use with adjacent uses;

- ii) The need for special building design incorporating height limitations, setbacks, buffering and landscaping to enhance compatibility with adjacent uses;
  - iii) Adequate access to the property particularly regarding emergency vehicles; and,
  - iv) The provision of adequate parking.
- b) Site plan control shall be utilized for new development or redevelopment projects in order to address compatibility and to ensure that such development or redevelopment creates a highly aesthetic and unique area.

## **6.2 Port Dover Urban Waterfront District Designation**

### **6.2.1 Intent**

- a) The Port Dover Urban Waterfront District Designation is an important tourism and economic resource. Its historic role as a tourist destination and a commercial fishing port remains important to the success of Port Dover. It is the intent of this Plan that the Port Dover Urban Waterfront District remains a unique area that supports tourism, the commercial fishery and provides recreational, commercial and cultural opportunities for residents and tourists. To achieve this, the policies of the Port Dover Urban Waterfront District Designation shall promote:
- i) Public visual and physical access to the waterfront shorelines, and to, from and within the District through the possible establishment of a boardwalk and/or waterfront promenades;
  - ii) Attractive and diverse public and private sector development that is compatible with the character and charm of the existing community;
  - iii) The recognition of the defined Hazard Lands and the protection and enhancement of the waterfront lands.
  - iv) The protection and promotion of:
    - » The Commercial Fishing Port as an ongoing viable commercial/industrial activity;
    - » The existing residential land uses;
    - » The mixed use and street-oriented commercial district that takes advantage of its locational attributes and its importance as a tourist destination; and,

- » The facilities and services of the Recreational Marina.

## 6.2.2 Permitted Uses

- a) The Port Dover Urban Waterfront District Designation is intended to accommodate commercial facilities and services in convenient locations to serve the needs of residents and tourists. The Port Dover Urban Waterfront District will be a focus of tourism and business activities, and will continue to grow and develop with the following permitted uses:
  - i) Retail and service commercial uses;
  - ii) Restaurants and snack bars;
  - iii) Accommodations for tourists and the travelling public including: hotels and motels, inns, bed and breakfast establishments, guest houses, lodge-style facilities, resort-oriented condominiums, or other similar forms of accommodation;
  - iv) Convention centres;
  - v) Marinas and related retail, commercial, office, recreation, club house and banquet uses and facilities;
  - vi) Marine industrial uses including boat-works and other marine-related commercial uses;
  - vii) Recreational and entertainment uses;
  - viii) Arts and cultural facilities;
  - ix) Residential uses, including special needs housing;
  - x) Home-based businesses:
  - xi) Community facilities;
  - xii) Emergency Services;
  - xiii) Day Care Facilities;
  - xiv) Parking facilities at grade, or in structure; and,
  - xv) Parks, open spaces and walkways/trails.
- b) In addition to the identified list of permitted uses, the following may also be permitted within the Port Dover Urban Waterfront District Designation:
  - i) Uses accessory to any of the identified permitted uses; and,
  - ii) Pop-up uses and activities. The County may establish policies and procedures to facilitate the establishment of pop-up uses and activities.

- c) Within the Port Dover Urban Waterfront District Designation the existing Commercial Fishing Port is recognized as an ongoing viable commercial/industrial activity. On those lands identified as the Marine Industrial Priority Area, marine industrial uses shall be the primary use of the property. The extent of the Commercial Fishing Port and the regulations that manage its ongoing function shall be detailed within the Implementing Zoning By-law.
- d) The County reserves the right, through the implementing Zoning By-law, to further refine the list of permitted uses to ensure that new development is appropriate in the context of the adjacent and surrounding community.
- e) New Drive-through commercial facilities are specifically not permitted within the Port Dover Urban Waterfront District Designation.

### **6.2.3 Development Policies**

- a) The County shall encourage the development, redevelopment and rehabilitation within the Port Dover Urban Waterfront District Designation by supporting appropriate mixed use development and redevelopment, in accordance with the policies of this Plan and the associated Urban Design Guidelines attached to this Plan as **Appendix I**.
- b) Stand-alone residential developments are not permitted within the Port Dover Urban Waterfront District Designation. New residential apartments shall only be permitted as part of a mixed use building, with another permitted use located at-grade and the residential use located above grade.
- c) All development within the Port Dover Urban Waterfront District Designation shall be compatible with existing development in proximity. Where appropriate, historic streetscape patterns such as block lengths, building heights, setbacks and separations will be maintained.
- d) The County will encourage the establishment of a wide range of arts and cultural facilities and activities in the Port Dover Urban Waterfront District through:
  - i) The placement of public art and murals; and,
  - ii) The placement of wayfinding signage to direct residents and visitors to shops, services and attractions.

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- e) Certain lands within the Port Dover Urban Waterfront District Designation have been identified by the Long Point Region Conservation Authority as being Hazard Land. Consequently, any use of such land will be subject to the policies of Section 7.3 of the Norfolk County Official Plan. All development applications that are within the Hazard Land area within the Port Dover Urban Waterfront District Designation shall be evaluated in consultation with the Conservation Authority.
- f) The County shall encourage and facilitate partnership arrangements, joint ventures, and facility sharing with the Marina Board of Management, the Harbour Authority, Provincial agencies, school boards, other institutions and community groups to expand the supply of parks, open space and parking facilities within the Port Dover Urban Waterfront District Designation.
- g) All development within the Port Dover Urban Waterfront District Designation shall be compatible with existing development in proximity, in consideration of the following issues:
  - i) Building design, roof lines, density, scale and massing;
  - ii) Setbacks, signage, lighting, and buffering of existing and proposed development; and,
  - iii) The use of landscaping and the provision of benches, planters and other street furniture.
- h) Linkages between the Port Dover Urban Waterfront District Designation and the Active Transportation Network identified on **Schedule D** shall be created and enhanced to foster pedestrian activity and encourage tourism within the Port Dover Urban Waterfront District Designation.
- i) Parks and open spaces shall be distributed throughout the Port Dover Urban Waterfront District Designation, and these facilities shall be linked together and connected with the broader County network of natural and open space areas. All developments within the Port Dover Urban Waterfront district Designation shall be required to provide public parkland, or cash-in-lieu of parkland. Public parkland shall be provided subject to the following:
  - i) Public open space to be dedicated shall be landscaped prior to conveyance in a manner satisfactory to the County; and/or,

- ii) Where cash-in-lieu of parkland is accepted for development within the Port Dover Urban Waterfront District Designation the County shall allocate the funds generated for public parkland improvements within the Port Dover Urban Waterfront District Designation. The funds generated shall be used to provide additional public parkland or for the aesthetic and/or functional improvement of existing public parkland areas.
  
- j) Where practical, the municipality may provide or cooperate with private landowners and other public agencies to provide additional open space facilities within Port Dover Urban Waterfront District Designation. The development of open space facilities that can be the focus of community festivals, events and activities attractive to all segments of the community will be encouraged.
  
- k) Retail sales outlets and associated commercial uses shall be limited in size, scale and scope by the Implementing Zoning By-law.
  
- l) Outdoor storage, including commercial fishing and marina related items, including the on-land storage of boats, as a use accessory to primary marine industrial uses, or to marinas and related marine commercial facilities may be permitted, having regard to compatibility with adjacent uses and potential impact on future redevelopment. The Implementing Zoning By-law and/or Site Plan Agreement may delineate where open storage may be located together with appropriate setbacks, screening and buffering. The Implementing Zoning By-law and/or Site Plan Agreement may also delineate where the boats may be stored, and may limit the number of boats permitted to be stored at any given time.
  
- m) The height, massing and layout of buildings within the Port Dover Urban Waterfront District Designation shall be oriented to a pedestrian scale. The maximum building height within the Port Dover Urban Waterfront District Designation shall be 4 storeys, or 13.5 metres, whichever is less, and may be subject to the following provisions that will be more fully articulated within the Implementing Zoning By-law:
  - i) All development within the Port Dover Urban Waterfront District Designation shall have a minimum height of 2 storeys; and,
  - ii) All development within the Port Dover Urban Waterfront District Designation shall incorporate a floor to ceiling height of the first floor of 4.5 metres.

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Building heights greater than 4 storeys may be considered within the Port Dover Urban Waterfront District Designation only through a Site Specific Amendment to this Plan and a Site Specific Zoning By-law Amendment, where appropriate mechanisms are employed to ensure compatibility and an appropriate transition to abutting properties. Mechanisms employed through the Site Specific planning policies and regulations may include a reduction in the permitted building height, the implementation of enhanced building setbacks, the requirement for landscape planting strips, the imposition of a step back and/or the imposition of an angular plane.

- n) Where residential uses are proposed in an existing building of commercial character, they shall only be permitted above the ground floor, ensuring that the street frontage is maintained for commercial uses. For the purposes of this policy, the commercial character of a building will be determined in consultation with the Chief Building Official of the County.
- o) In an existing building of residential character, residential uses and/or commercial uses shall be permitted, provided the residential character of the building is maintained. For the purposes of this policy, the residential character of a building will be determined in consultation with the Chief Building Official of the County.
- p) In the consideration of any Zoning By-law Amendment proposing to establish any of the permitted uses, the following shall be addressed:
  - i) Adequate and appropriate access to the property from a public road; and,
  - ii) The provision of adequate and appropriate parking and loading facilities.
- q) The County will recognize the importance of the desired mixed use context and will promote a comprehensive parking strategy within the Port Dover Urban Waterfront District Designation that considers reduced parking standards for urban mixed use developments, or other classes of development, based on an understanding of opportunities for shared parking, on-street parking, and the availability of public parking facilities. In addition:
  - i) The County may accept cash-in-lieu of parking as an alternative to providing any required parking. Where cash-in-lieu of parking is accepted, the funds generated shall be used to provide additional public parking or the aesthetic and/or functional improvement of existing public parking areas within the Port Dover Urban Waterfront District Designation; and,

- ii) Where a development cannot provide off-street parking on its site, the County may permit the provision of the required parking spaces on an alternative site, provided that the alternative site is within convenient walking distance of the proposed development, and the developer enters into an agreement with the municipality to ensure the continued availability of the alternative site as a parking area.
- r) The Site Plan Control policies of Section 9.6.5 of the Norfolk County Official Plan shall apply to all development applications in the Port Dover Urban Waterfront District Designation.
- s) The County may undertake, by both direct municipal action and by encouraging the actions of other parties, a program of on-going improvements within the Port Dover Urban Waterfront District Designation, including streetscape improvements, facilities for off-street and on-street parking, improved vehicular connections and circulation patterns, and facilities for those not traveling by private automobile, including improvements for pedestrians, cyclists, the elderly and physically-challenged, and those using taxis, transportation for the disabled and transit services.
- t) The County encourages the establishment of a Port Dover Urban Waterfront District Business Improvement Area, in accordance with the Municipal Act, corresponding to the boundary of the Port Dover Urban Waterfront District Designation. The County may promote a larger Business Improvement Area that combines the areas identified as within the Port Dover Downtown Designation and the Port Dover Urban Waterfront District Designation.
- u) The County shall pursue the ongoing enhancement of the Port Dover Urban Waterfront District in co-operation with the boards of management for the Business Improvement Areas (BIA) and other public and private interests and community groups, to enhance the efficiency, convenience, safety and appearance of the areas and the activities they accommodate. As such, the County may pass By-laws designating and delineating a Community Improvement Project Area associated with the Port Dover Urban Waterfront District Designation, and may undertake Community Improvement Plans, in accordance with Section 9.5.2 (Community Improvement) of the Norfolk County Official Plan to improve public infrastructure and to stimulate private sector investment.

## 6.2.4 Urban Design Policies

a) **Interpretation** - These Urban Design Policies are intended to guide new development within the Port Dover Urban Waterfront District Designation in combination with the other relevant policies of this Plan and the Urban Design Guidelines attached to this Plan as **Appendix I**. It is recognized that as policies within the statutory part of this Plan, all new development within the Port Dover Urban Waterfront District Designation shall conform to these Urban Design Policies. However, it is the intent of the County that progressive and innovative development is desirable within Port Dover, and the test of conformity with these Urban Design Policies may be interpreted with some flexibility, so long as the intent of these Policies is achieved, to the satisfaction of the County.

b) **General Design Policies** - The following policies apply everywhere within the Port Dover Urban Waterfront District Designation:

- i) Buildings shall be oriented to frame the street edge and to create a strong street wall;
- ii) Buildings shall front directly onto public streets and other public spaces, in order to clearly define the public realm and create an attractive and safe pedestrian environment;
- iii) Buildings at corner locations shall be sited to address the intersection, with consideration given to both street frontages;
- iv) Buildings shall align with the existing street wall. Buildings shall generally be aligned with the setbacks of their neighbours. For sites with ground floor commercial, consideration shall be given to zero setback;
- v) Main building entrances shall face the street and shall be clearly defined with architectural details and easily identifiable within the facade composition. Publicly accessible front entrances for stores, offices, and institutional uses shall permit barrier-free access and universal accessibility, including both visual and physical accessibility;
- vi) Ground floor facades shall be highly transparent, including transparent windows and entrance doors, to establish a strong visual connection between the street and the interior of active ground floor uses;
- vii) Facade rhythm shall be established along a street wall through architectural articulation, including the use of fenestration, bands, columns, and other repeated elements;



*Buildings front directly onto streets and public spaces*



*Facade rhythm through architectural articulation*

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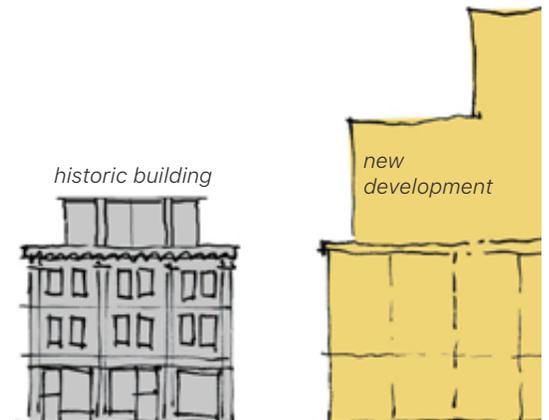
- viii) Streets shall be designed to accommodate all modes of transportation, including walking, cycling, cars and service vehicles; and,
- ix) Streets and sidewalks shall create a pedestrian environment through defined standards for landscaped areas, paving, street trees, and other appropriate street furniture, and shall form a connected system of optional routes to, from and within the area.

c) **Cultural Heritage** - The County shall protect and enhance, where possible and appropriate, the historic architectural character found within the Port Dover Urban Waterfront District Designation, particularly focusing on the pedestrian scale and design of the buildings and streetscapes, and the arrangement of windows and treatments on the building facades. Existing listed and designated cultural heritage resources within the Port Dover Urban Waterfront District Designation are identified in **Appendix II**. Heritage-related design policies for new development within the Port Dover Urban Waterfront Designation include:

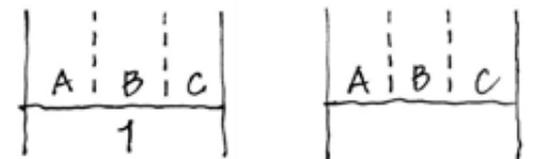


*New designs shall relate to the traditional building stock*

- i) The historic pattern of development in the Port Dover Urban Waterfront District Designation has been characterized by ground floor commercial uses with narrow street frontages, frequent pedestrian entrances and windows that create pedestrian appeal. The design of new buildings and the redevelopment or adaptive re-use of existing buildings shall enhance the quality of the pedestrian environment by including transparent frontages, the articulation of facades and the use of quality materials at the street level;
- ii) New buildings shall relate to the traditional building stock, either through a traditional style that reflects the character of the area, or a more contemporary style that sets them apart from and highlights heritage buildings through a defined contrast and juxtaposition. Designs shall provide a clear distinction between 'new' and 'old'; and,
- iii) Façade designs shall reference the articulation of neighbouring historic buildings, with respect to vertical and horizontal elements, including the rhythm and proportion of its main architectural elements.



d) **Design Policies for Parking and Access** - Guidelines for parking and access within the Port Dover Urban Waterfront District Designation generally relate to the design and functionality of on- and off street parking and access driveways. The objective is to provide suitable parking and driveway options while reducing traffic impacts and the negative visual impact of surface parking lots. The following design policies apply:



*Facade designs shall reference the articulation of neighbouring historic buildings*

- i) Parking facilities shall be designed and located to minimize its impact on the streetscape and public realm to ensure that sidewalks and building facades define the street edge. As such:
  - » Parking will not be permitted between the right of way and the building face or within front yard setback areas;
  - » Side yard parking may be considered where site constraints are significant, but is generally discouraged;
  - » Rear yard parking, or encouraging patrons to park in designated parking lots, is preferred;
- ii) Parking lots shall be organized to minimize the number of potential pedestrian-vehicle movement conflicts. Where possible, parking areas shall be coordinated between multiple properties to maximize connectivity, improve traffic flow and increase the efficiency of parking;
- iii) Parking lots shall be designed to enhance the feeling of safety through techniques such as maintaining clear sight lines for natural surveillance of the parking lot as well as minimizing dark, hidden or obscured areas. Parking lot design shall consider:
  - » Planting strips and landscaped traffic islands, medians, or bump-outs shall also be provided within lots to break up the expanse of hard surface;
  - » Pedestrian-scaled lighting, walkways, landscaping, and signage to enhance pedestrian safety, movement and comfort through parking lots to the adjacent sidewalks;
  - » Pedestrian routes through parking areas shall be wide enough to accommodate comfortable travel and their walking surfaces shall be clearly differentiated from the parking areas with texture, material, colour changes or markings;
- iv) Where existing parking areas are adjacent to the sidewalk, a landscaped area shall be located between parking spaces and the sidewalk. Landscaping shall provide clear sight lines at eye level.
- v) Bicycle parking and racks shall be provided in locations that are close to building entrances but situated to avoid any conflicts for movement along pedestrian routes.



*Landscaped medians and islands*



*Pedestrian-scaled walkways and landscaping enhance safety, movement and comfort*

- vi) Where possible, access to parking areas shall be provided from side streets and laneways. Access to parking areas shall be defined through clearly designated entrances and exits.

## 6.2.5 Lynn River/Black Creek Special Policy Area

- a) On lands within the Port Dover Urban Waterfront District Designation identified as Site Specific Policy Area 6.2.5 on **Schedule B** includes land north of Highway No. 6 bordering on both sides of the Lynn River and Black Creek. Traditionally, marine industries have established within this area. This Special Policy Area is to be considered and applied in addition to the other policies of this Section.
- b) The marine industry is still an important component of the local economy and the Lynn River acts as the main artery for marine traffic to this area. With the increase in tourism, which is becoming more important to the economic health of the County, this area has the potential to support a variety of interests relating to tourism, recreation and innovative residential development, while at the same time recognizing that the established marine industries will remain. A large portion of the Lynn River/Black Creek Special Policy Area are considered Hazard Lands due to potential flooding and that the risk of flooding may impact existing and future land uses.
- c) New uses may be established provided they are in suitable locations where land use conflicts can be minimized and adequate access and parking can be provided. The intent of this Plan is to create a continuous link for commercial and tourism related uses between the Downtown Area and the Urban Waterfront District through the Lynn River/Black Creek Area and back to the Downtown Area through a connection along Market Street, Chapman Street and Powell Park. The Lynn River/Black Creek Special Policy Area is in transition and none of the changes in land use are expected to occur immediately. The principal method of redevelopment will be through applications to amend the Implementing Zoning By-law. In the consideration of any such application, effort shall be made to eliminate or minimize potential land use conflicts.
- d) In addition to the land uses permitted in the underlying land use designation, the following uses shall be permitted within the Lynn River/Black Creek Special Policy Area:
  - i) Marine industrial uses;
  - ii) Commercial and recreational uses catering specifically to boaters;
  - iii) Retail commercial uses catering specifically to tourism and marine recreational activities;
  - iv) Commercial accommodation including hotels, motels and bed and breakfast establishments;

- v) Restaurants;
- vi) Convention centres; and,
- vii) Residential uses, within a mixed use building, that exhibit the following characteristics:
  - » Architecture and detailing in built form and urban design that reflects a nautical or marine theme; and
  - » An orientation to the waterfront, taking advantage of the views and vistas.
- e) In the consideration of any application to amend the Implementing Zoning By-law proposing to establish a commercial or residential use, the following shall be addressed:
  - i) The compatibility of the proposed use with adjacent and neighbourhood land uses, particularly existing marine industrial uses;
  - ii) The need for special building design, setbacks, buffering and landscaping to enhance compatibility with adjacent uses;
  - iii) Adequate access to the property particularly for emergency vehicles; and,
  - iv) The provision of adequate parking and loading facilities.
- f) The County shall use site plan control, in accordance with the policies of Section 9.6.5 (Site Plan Control) of the Norfolk County Official Plan, to ensure that new development and redevelopment, is compatible with the intended character and the natural environment, and creates a highly aesthetic and unique mixed use area.

## **6.2.6 Dover Wharf Site Specific Policy**

- a) On lands within the Port Dover Urban Waterfront District Designation identified as Site Specific Policy Area 6.2.6 on **Schedule B**, in addition to the uses permitted, a 48 unit residential development shall be permitted.

## 6.3 Existing Neighbourhood Designation

### 6.3.1 Intent

- a) The Existing Neighbourhood Designation in Port Dover is expected to continue to accommodate attractive neighbourhoods which will provide for a variety of residential forms as well as neighbourhood facilities such as elementary schools, parks, places of worship and convenience commercial uses integral to and supportive of a residential environment. A variety of housing types are needed to meet the needs of a diverse population. Opportunities to provide housing for individuals or groups with special needs including the elderly and those with special physical, social or economic needs within the County will be encouraged.
- b) It is the intent of this Plan to enhance the existing neighbourhoods within Port Dover, while managing their ongoing evolution, including opportunities for sensitive intensification. It is also the intent of the County to recognize the existing neighbourhoods of Port Dover as communities that consist of primarily low density residential house forms that have limited potential to accommodate significant levels of intensification, but that are prime candidates for the introduction of additional residential units (accessory apartments) and home-based businesses.

### 6.3.2 Permitted Uses

- a) Within the Existing Neighbourhood Designation, the following uses are permitted:
  - i) Low density and medium density residential uses;
  - ii) Special needs housing;
  - iii) Home-based businesses;
  - iv) Day care facilities;
  - v) Bed and breakfast establishments;
  - vi) Community facilities;
  - vii) Small scale places of worship;
  - viii) Parking facilities at grade, or in structure; and,
  - ix) Parks, open spaces and walkways/trails.
- b) In addition to the identified list of permitted uses, uses accessory to any of the identified permitted uses are also permitted.

- c) Residential development approved in site specific Official Plan Amendments, or in Draft Plans of Subdivision approved by the County prior to the adoption of this Plan are also permitted within the Existing Neighbourhood Designation.
- d) The County reserves the right, through the implementing Zoning By-law, to further refine the list of permitted uses to ensure that new development is appropriate in the context of the adjacent and surrounding community.

### 6.3.3 Development Policies

- a) The County shall permit the development, redevelopment and rehabilitation within the Existing Neighbourhood Designation by supporting a mixture of low density housing types, in accordance with the policies of this Plan and the associated Urban Design Guidelines attached to this Plan as **Appendix I**.
- b) Medium density residential uses may also be permitted on a limited basis within the Existing Neighbourhood Designation where they assist in defining a small scale neighbourhood focus such as a park, school, place of worship or commercial use. Such submissions will be subject to a Site Specific Zoning By-law Amendment and Site Plan Control process, including the submission of a planning justification report and other required studies, to ensure that the development proposal is compatible with adjacent uses, to the satisfaction of the County.
- c) All new development shall be compatible with existing adjacent residential uses in terms of orientation, privacy, landscaping, shadow casting, and visual impact. Where new development is introduced within, or abutting an Existing Neighbourhood Designation, the County will consider additional setbacks, angular lanes and enhanced landscaping as techniques to ensure an appropriate transition/interface.
- d) Certain lands within the Existing Neighbourhood Designation have been identified by the Long Point Region Conservation Authority as being Hazard Land. Consequently, any use of such land will be subject to the policies of Section 7.3 of the Norfolk County Official Plan. All development applications that are within the Hazard Land area within the Existing Neighbourhood Designation shall be evaluated in consultation with the Conservation Authority.
- e) All development within the Existing Neighbourhood Designation shall be compatible with existing development in proximity, in consideration of the following issues:
  - i) Building design, roof lines, density, scale and massing;
  - ii) Setbacks, signage, lighting, and buffering of existing and proposed development; and,

- iii) The use of landscaping and the provision of benches, planters and other street furniture.
- f) Linkages throughout the Existing Neighbourhood Designation to the Active Transportation Network identified on **Schedule D** shall be created and enhanced to foster pedestrian activity.
- g) All developments within the Existing Residential Designation shall be required to provide public parkland, or cash-in-lieu of parkland. Public parkland shall be provided subject to the following:
  - i) Public open space to be dedicated shall be landscaped prior to conveyance in a manner satisfactory to the County; and/or,
  - ii) Where cash-in-lieu of parkland is accepted for development within the Existing Neighbourhood Designation the County shall allocate the funds generated for public parkland improvements within Port Dover. The funds generated shall be used to provide additional public parkland or for the aesthetic and/or functional improvement of existing public parkland areas.

#### 6.3.4 Urban Design Policies

- a) **Interpretation** - These Urban Design Policies are intended to guide new development within the Existing Neighbourhood Designation in combination with the other relevant policies of this Plan and the Urban Design Guidelines attached to this Plan as **Appendix I**. It is recognized that as policies within the statutory part of this Plan, all new development within the Existing Neighbourhood Designation shall conform to these Urban Design Policies. However, it is the intent of the County that progressive and innovative development is desirable within Port Dover, and the test of conformity with these Urban Design Policies may be interpreted with some flexibility, so long as the intent of these Policies is achieved, to the satisfaction of the County.
- b) **Cultural Heritage** - The County shall protect and enhance, where possible and appropriate, the historic architectural character found within the existing Neighbourhood Designation within Port Dover, particularly focusing on the pedestrian scale and design of the buildings and streetscapes, and the arrangement of windows and treatments on the building facades. Existing listed and designated cultural heritage resources within the Existing Neighbourhood Designation within Port Dover are identified in **Appendix II**. Heritage-related design policies for new development within the Existing Neighbourhood Designation within Port Dover include:

- i) New buildings shall relate to the traditional building stock, either through a traditional style that reflects the character of the area, or a more contemporary style that sets them apart from and highlights heritage buildings through a defined contrast and juxtaposition. Designs shall provide a clear distinction between 'new' and 'old'; and,
  - ii) Façade designs shall reference the articulation of neighbouring historic buildings, with respect to vertical and horizontal elements, including the rhythm and proportion of its main architectural elements.
- c) ***Design Policies for Residential Infill*** - The following guidelines anticipate the potential for modest residential infill within the existing residential neighbourhoods within the Existing Neighbourhood Designation in Port Dover:
- i) Avoid parking, driveways and garages along street frontages, which shall be located away from public view and preferably internal to the site, screened by buildings;
  - ii) Buildings shall be placed to create a continuous street wall; the setback distance shall be determined with reference to the setback distances of adjacent buildings and provide sufficient transition from private front yards to the public street;
  - iii) The massing and scale of the development shall be compatible with the existing and planned context;
  - iv) Main entries shall be located on the front of the building/unit and shall be highlighted in the architectural design;
  - v) Ensure that end units display the same level of architectural detail and articulation as that of the main front elevation; and,
  - vi) Detailed landscape treatments shall be coordinated with the main building materials and create a year round visually appealing presence along the street.

### **6.3.5 Silver Lake Special Policy Area**

- a) Within the Existing Neighbourhood Designation in Port Dover, the area along the west side of Silver Lake extending north from the vicinity of Chapman and Patterson Streets to the rear of lots fronting on Queen Street, as delineated as Special Policy Area 6.3.5 on **Schedule B**, is the Silver Lake Special Policy Area. This Special Policy Area is to be considered and applied in addition to the other policies of this Section.

- b) The Silver Lake Special Policy Area is an area in transition which has traditionally included certain industrial and open space uses. There is the potential to create a specific linkage between the Downtown Area of Port Dover with the open space areas and the Lynn Valley Trail. It is the policy of this Plan to encourage a transition to open space and recreational uses in this area.
- c) Notwithstanding the underlying land use designations, permitted uses in the Silver Lake Special Policy Area, delineated on **Schedule B**, shall be limited to:
  - i) Passive and active open space and recreational uses;
  - ii) Public and/or private commercial recreational facilities;
  - iii) Small scale commercial uses including a farmers' market;
  - iv) Limited residential uses, such as senior housing complexes, lifestyle communities, and residential care facilities; and,
  - v) Institutional uses.
- d) The County shall use site plan control, in accordance with the policies of Section 9.6.5 (Site Plan Control) of the Norfolk County Official Plan to ensure that new development and redevelopment, including redevelopment of and conversion of existing buildings, is compatible with the intended character of the areas and the natural environment, and the adjacent residential areas.

### **6.3.6 Mill Store Site Specific Policy Area**

- a) On land within the Existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.6, in addition to the uses permitted, an existing retail establishment focusing on the sale of fabrics and clothing, a farmers' market, a place of sports and recreation, and an adult training centre shall be permitted.

### **6.3.7 Restaurant Site Specific Policy Area**

- a) On land within the Existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.7, in addition to the uses permitted, a restaurant shall also be permitted to locate within the existing dwelling.

### 6.3.8 Office Site Specific Policy Area

- a) On land within the Existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.8, an administrative office shall also be permitted within the existing one-storey residential dwelling unit.

### 6.3.9 Gardening Supply Outlet Site Specific Policy Area

- a) On land within the existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.9, in addition to the uses permitted, a gardening supply outlet shall be permitted for the sale of supplies directly related to the installation and maintenance of the various landscaping backyard pond displays.

### 6.3.10 Health Clinic Site Specific Policy Area

- a) On land within the Existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.10, in addition to the permitted uses, an animal hospital, a clinic or doctor's offices, a financial institution, offices, a pharmacy, and a retail store associated with a clinic or doctor's office shall be permitted. The commercial structure or building shall have a gross floor area of up to 900 m<sup>2</sup>, where the number of commercial retail units is limited to a maximum of two units with a maximum usable floor area of 200 m<sup>2</sup> per unit. The usable floor area of a pharmacy shall be limited to a maximum of 50 m<sup>2</sup>.

### 6.3.11 Dover Coast Residential Site Specific Policy Area

- a) On land within the Existing Neighbourhood Designation within Port Dover, identified on **Schedule B** to this Plan as Site Specific Policy Area 6.3.11, Notwithstanding the policies of this Plan, golf course uses shall also be permitted.
- b) For any residential proposal on the Subject Land and on the Other Lands owned by the applicant, as identified on **Schedule B**, that are within the Industrial Influence Area, as identified on **Schedule A** to this Plan, a distance of no more than 300 metres, the following studies shall be completed and submitted to Norfolk County and Haldimand County:
  - i) Planning Justification Report;
  - ii) Air Quality Assessment; and,
  - iii) Noise Study.
- c) Each of the completed studies may be peer reviewed at the discretion of Norfolk County and/or Haldimand County and the proponent shall pay the reasonable cost of the peer review. This review may include consultation with the Province.

## **6.3.12 Shore Acres Park Site Specific Policy Area**

- a) In addition to the list of permitted uses within the Existing Neighbourhood Designation, the lands identified as Site Specific Policy Area 6.3.12 on **Schedule B**, the existing recreational trailer park shall also be a permitted use.

## **6.4 Future Neighbourhood Designation**

### **6.4.1 Intent**

- a) It is the intent of the County to promote well-designed and attractive residential neighbourhoods throughout Port Dover. The Future Neighbourhood designation will include an appropriate range and mix of housing types, parks and open space features and an array of community facilities. The Future Neighbourhood Designation will include provisions for the establishment of Neighbourhood Centres.

### **6.4.2 Permitted Uses**

- a) Within the Future Neighbourhood Designation, the following uses are permitted:
  - i) Low density, medium density and high density residential uses;
  - ii) Live-work units;
  - iii) Special needs housing;
  - iv) Neighbourhood centres;
  - v) Home-based businesses;
  - vi) Day care facilities;
  - vii) Bed and breakfast establishments;
  - viii) Community facilities;
  - ix) Education facilities;
  - x) Small scale places of worship;
  - xi) Emergency services;
  - xii) Parking facilities at grade, or in structure; and,
  - xiii) Parks, open spaces and walkways/trails.
- b) In addition to the identified list of permitted uses, uses accessory to any of the identified permitted uses are also permitted.

- c) Residential development approved in site specific Official Plan Amendments, or in Draft Plans of Subdivision approved by the County prior to the adoption of this Plan are also permitted within the Future Neighbourhood Designation.
- d) The County reserves the right, through the implementing Zoning By-law, to further refine the list of permitted uses to ensure that new development is appropriate in the context of the adjacent and surrounding community.

### 6.4.3 Development Policies

- a) All development within the Future Neighbourhood Designation shall proceed through a Secondary Plan/Official Plan Amendment Process that will establish the more detailed land use designations and associated policy framework to properly guide future development.
- b) The County shall require, prior to the approval of any Secondary Plan/Official Plan Amendment, or any associated development application (Draft Plan of Subdivision or implementing Zoning By-law) within any of the individual Development Areas identified on **Schedule A**, that are also designated Future Neighbourhood on **Schedule B**, that a Block Plan be prepared for the entire individual Development Area. The purpose of the Block Plan is to promote comprehensive planning, and to:
  - i) Identify the detailed land use and density distribution, and to ensure that the required density target is achieved;
  - ii) Confirm the boundaries of the Natural Heritage System;
  - iii) Identify the location for the Neighbourhood Centres;
  - iv) Identify the parkland system, and the active transportation network;
  - v) Identify the location for any required educational and/or community facilities;
  - vi) Identify the detailed road pattern, including Local Roads;
  - vii) Articulate the details for the provision of sewer, water and storm water management systems;
  - viii) Identify the road network and infrastructure system connections to all properties within and adjacent to the individual Neighbourhood Area; and,
  - ix) Potentially form the basis for a Developer's Group Agreement, where the identified Neighbourhood Area includes multiple landowners.

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Required Block Plans shall be adopted by the County and shall include all of the necessary supporting technical studies, as identified in the required pre-consultation process, to the satisfaction of the County. Required Block Plans shall form the basis for the subsequent approval of Draft Plans of Subdivision and implementing Zoning By-laws.

- c) The County shall permit the development of a range and mix of housing types and densities, as well as appropriate neighbourhood supporting land uses, in accordance with the policies of this Plan and the Urban Design Guidelines attached as **Appendix I**. In addition, within any of the individual Development Area identified on **Schedule A**, that are also designated Future Neighbourhood on **Schedule B**, at least one Neighbourhood Centre shall be established. Permitted Neighbourhood Centres shall be centrally located within each of the identified Development Areas. Generally, a Neighbourhood Centre shall:
  - i) Be located at an intersection, where at least one road is a collector or arterial; and,
  - ii) Be within a walking distance of 5 to 10 minutes for most of the residents of the defined Development Area.
- d) Within a Neighbourhood Centre, the mix of uses shall be compatible and sensitively integrated with the surrounding residential uses in terms of building mass, height, setbacks, orientation, privacy, landscaping, shadow casting, accessibility and visual impact. In addition to the residential uses permitted within a Neighbourhood Centre, at least one of the following additional land uses shall be required:
  - i) One local convenience retail store use up to 190 square metres of non-residential gross floor area per Neighbourhood Centre. Apartment units may be permitted above the ground floor at the rear or to the side of the local convenience store; and/or,
  - ii) Institutional and community uses which provide services to the neighbourhood.
- e) Linkages throughout the Development Areas to the Active Transportation Network identified on **Schedule D** shall be created and enhanced to foster pedestrian activity.
- f) All developments within the Future Neighbourhood Designation shall be required to provide public parkland, or cash-in-lieu of parkland. Public parkland shall be provided subject to the following:
  - i) Public open space to be dedicated shall be landscaped prior to conveyance in a manner satisfactory to the County;

- ii) The first priority for parkland dedication is the achievement of new public parks within each of the Development Areas identified on **Schedule A**; and/or,
- iii) Where cash-in-lieu of parkland is accepted for development within the identified Development Areas, the County shall use the funds to provide public parkland within the Development Area where the funds were generated, or for the aesthetic and/or functional improvement of existing public parkland areas.

#### 6.4.4 Urban Design Policies

- a) **Interpretation** - These Urban Design Policies are intended to guide new development within the Future Neighbourhoods Designation in combination with the other relevant policies of this Plan and the Urban Design Guidelines attached to this Plan as **Appendix I**. It is recognized that as policies within the statutory part of this Plan, all new development within the Future Neighbourhood Designation shall conform to these Urban Design Policies. However, it is the intent of the County that progressive and innovative development is desirable within Port Dover, and the test of conformity with these Urban Design Policies may be interpreted with some flexibility, so long as the intent of these Policies is achieved, to the satisfaction of the County.
- b) **Development abutting Natural Features** - The following policies apply to lands that abut lands within the Provincially Significant Wetlands, Hazard Land and/or the Significant Woodlands Designations, as defined on **Schedule B**. It is a policy of the County to:
  - i) Protect and incorporate the surrounding natural heritage system as an integral part of the neighbourhood's structure;
  - ii) Minimize development that encroaches into the natural heritage system and negatively impact the health and diversity of it due to noise, light pollution, debris, and unauthorized access;
  - iii) Provide frequent access points and public street frontage to promote views and accessibility to natural heritage areas;
  - iv) Create views and vistas to natural heritage features, parks and open spaces through the location, arrangement and configuration of streets and blocks;
  - v) Locate parks and open spaces prominently, with adjacency or strong connections to the natural heritage system and trail network; and,



Street arranged to provide view of natural heritage feature

- vi) Back lotting of the natural heritage features shall be minimized.
- c) **Sustainable Design Policies** - Development within the identified Future Neighbourhoods Designation, as identified on **Schedule B** shall be developed with regard to the following:
- i) The principles of LEED-ND (Leadership in Energy and Environmental Design – Canada) as they evolve; and,
  - ii) Building orientation to maximize potential for passive and active solar energy.
- d) **Neighbourhood Layout Policies** - It is a policy of the County to:
- i) Create a connected, pedestrian-oriented and highly interconnected street and block pattern, with connections to adjacent communities and to community amenities/ destinations;
  - ii) Limit blocks to no more than 180m in length; blocks that are longer than this in length shall include mid-block landscaped pedestrian links of at least 8m in width;
  - iii) Provide appropriate transition to/integration with adjacent uses;
  - iv) Changes in land use, lotting and built form shall occur along a rear lot line so that similar uses and forms shall frame both sides of a street;
  - v) Parks and open spaces shall be provided with substantial frontage on a public road. Back lotting of parks and open spaces shall be minimized;
  - vi) Locate higher density forms of development at prominent locations such as around parks, adjacent to major roads, at gateways and along special streets; and,
  - vii) Require built form that is a minimum of three storeys in height around parks and at prominent locations.
- e) **Road Pattern Policies** - It is a policy of the County to:
- i) Maximize number of connections to the major road network;
  - ii) Connect to existing road stubs;
  - iii) Provide multiple future road connections to undeveloped areas;
  - iv) Provide well-connected internal road network;

- v) Provide direct connection to natural features from internal streets;
- vi) Provide vista parks from internal streets to natural features where direct frontage is not feasible;
- vii) Locate key destinations such as retail and service commercial uses, parks and schools within 5 minute walk (400m radius) of most residents;
- viii) Use streets and public spaces to create linkages from natural heritage features and lower intensity land uses into the central area of the neighbourhood;
- ix) Ensure publicly accessible open spaces such as parks, storm water management facilities and natural heritage features have significant frontage on internal public roads;
- x) Provide higher density and mixed uses adjacent to higher order roads;
- xi) Create a transition of higher density to lower density uses;
- xii) Provide a mix of housing forms throughout the neighbourhood and along streetscapes; and,
- xiii) Locate built form and public space to create and accentuate entry points to the neighbourhood.



*Demonstration plan of road pattern policies*

## 6.5 Mixed Use Designation

### 6.5.1 Intent

- a) It is the intent of this Plan that the lands within the Mixed Use Designation develop and intensify over time with a range and mixture of land uses and higher intensity character, in a manner that is sensitive to the adjacent residential neighbourhoods. The Mixed Use Designation is expected to provide retail and service commercial uses that serve a growing local population.

### 6.5.2 Permitted Uses

- a) Within the Mixed Use Designation, the following uses are permitted:
  - i) Retail and service commercial uses;
  - ii) Restaurants;
  - iii) Offices;
  - iv) Hotels and motels;
  - v) Convention centres,

- vi) Private clubs;
  - vii) Recreational and entertainment uses;
  - viii) Arts and cultural facilities;
  - ix) Medium density and high density residential uses, including special needs housing;
  - x) Live/work units;
  - xi) Home-based businesses;
  - xii) Small scale places of worship;
  - xiii) Community facilities;
  - xiv) Education facilities;
  - xv) Institutional uses;
  - xvi) Emergency services;
  - xvii) Day care facilities;
  - xviii) Parking facilities at grade, or in structure; and,
  - xix) Parks, open spaces and walkways/trails.
- b) In addition to the identified list of permitted uses, uses accessory to any of the identified permitted uses are permitted.
- c) The County reserves the right, through the Implementing Zoning By-law, to further refine the list of permitted uses to ensure that new development is appropriate in the context of the adjacent and surrounding community.

### 6.5.3 Development Policies

- a) The County shall encourage mixed use development within the Mixed Use Designation, in accordance with the policies of this Plan and the associated Urban Design Guidelines attached to this Plan as **Appendix I**.
- b) All development within the Mixed Use Designation shall be compatible with existing development in proximity.
- c) Linkages between the Mixed Use Designation and the Active Transportation Network identified on **Schedule D** shall be created and enhanced to foster pedestrian activity.
- d) All developments within the Mixed Use Designation shall be required to provide public parkland, or cash-in-lieu of parkland. Where cash-in-lieu of parkland is accepted for development within the Mixed use Designation, the funds generated shall be used to provide additional public parkland, or for the aesthetic and/or functional improvement of existing public parkland areas.

- e) Permitted retail uses shall be limited in scale to a maximum of 3,000 square metres of Gross Floor Area per individual retail use. Further, permitted retail uses shall have a minimum of 125 square metres of Gross Floor Area per individual retail use.
- f) The maximum building height within the Mixed Use Designation shall be 6 storeys, or 20 metres, whichever is less, and shall also be subject to the following provisions that will be more fully articulated within the Implementing Zoning By-law:
  - i) Where a property within the Mixed use Designation abuts existing or proposed low density residential uses, appropriate mechanisms shall be employed to ensure compatibility and an appropriate transition to those abutting properties. Mechanisms may include a reduction in the permitted building height, the implementation of enhanced building setbacks, the requirement for landscape planting strips, the imposition of a step back and/or the imposition of an angular plane;
  - ii) All development within the Mixed Use Designation shall have a minimum height of 2 storeys; and,
  - iii) All development within the Mixed Use Designation shall incorporate a floor to ceiling height of the first floor of 4.5 metres.

Taller buildings may be considered for approval by the County at the Gateway locations identified on **Schedule A** that are also within the Mixed Use Designation on **Schedule B**.

- g) All new development with frontage along Main Street, or Highway 6 within the Mixed Use Designation have at least 50% of its ground floor Gross Floor Area dedicated to non-residential uses, with a preference for retail and service commercial uses and restaurants. Residential uses shall only be permitted above the ground floor and/or on the ground floor in the rear of the building.
- h) In the consideration of any Implementing Zoning By-law proposing to establish any of the permitted uses, the following shall be addressed:
  - i) Adequate and appropriate access to the property from a public road; and,
  - ii) The provision of adequate and appropriate parking and loading facilities. All development within the Mixed use Designation shall provide adequate parking on-site.

- i) Comprehensive block development of lands in separate ownerships will be required, to achieve well-designed and integrated development, including:
  - i) Integrated internal circulation systems;
  - ii) Co-ordinated access points, to minimize the total number of access points to abutting roads;
  - iii) Compatible building design and location, to achieve a consistent streetscape;
  - iv) Complementary landscaping plans;
  - v) Integrated parking areas; and
  - vi) Consistent signage and lighting facilities.
- j) The site plan control policies in accordance with the policies of Section 9.6.5 (Site Plan Control) of the Norfolk County Official Plan shall apply to all development applications in the Mixed Use Designation.

## 6.5.4 Urban Design Policies

- a) **Interpretation** - These Urban Design Policies are intended to guide new development within the Mixed Use Designation in combination with the other relevant policies of this Plan and the Urban Design Guidelines attached to this Plan as **Appendix I**. It is recognized that as policies within the statutory part of this Plan, all new development within the Mixed Use Designation shall conform to these Urban Design Policies. However, it is the intent of the County that progressive and innovative development is desirable within Port Dover, and the test of conformity with these Urban Design Policies may be interpreted with some flexibility, so long as the intent of these Policies is achieved, to the satisfaction of the County.

- b) **General Design Policies** - The following policies apply everywhere within the Port Dover Mixed Use Designation:

- i) A minimum 2-storey street wall shall be maintained facing public streets along all streets;
- ii) Buildings shall be oriented to frame the street edge and to create a strong street wall;
- iii) Buildings shall front directly onto public streets and other public spaces, in order to clearly define the public realm and create an attractive and safe pedestrian environment;
- iv) Buildings at corner locations shall be sited to address the intersection, with consideration given to both street frontages;



2-storey street wall



Buildings front directly onto streets and public spaces



Facade rhythm through architectural articulation

- v) Buildings shall align with the existing street wall. In residential areas, buildings shall generally be aligned with the setbacks of their neighbours;
  - vi) Main building entrances shall face the street and shall be clearly defined with architectural details and easily identifiable within the facade composition. Publicly accessible front entrances for stores, offices, and institutional uses shall permit barrier-free access and universal accessibility, including both visual and physical accessibility;
  - vii) Ground floor facades shall be highly transparent, including transparent windows and entrance doors, to establish a strong visual connection between the street and the interior of active ground floor uses; and,
  - viii) Facade rhythm will be established along a street wall through architectural articulation, including the use of fenestration, bands, columns, and other repeated elements.
- c) **Design Policies for Parking and Access** - Guidelines for parking and access within the Mixed Use Designation generally relate to the design and functionality of on- and off street parking and access driveways. The objective is to provide suitable parking and driveway options while reducing traffic impacts and the negative visual impact of surface parking lots. The following design policies apply:
- i) Parking facilities shall be designed and located to minimize its impact on the streetscape and public realm to ensure that sidewalks and building facades define the street edge;
  - ii) Parking lots shall be organized to minimize the number of potential pedestrian-vehicle movement conflicts. Where possible, parking areas shall be coordinated between multiple properties to maximize connectivity, improve traffic flow and increase the efficiency of parking;
  - iii) Parking lots shall be designed to enhance the feeling of safety through techniques such as maintaining clear sight lines for natural surveillance of the parking lot as well as minimizing dark, hidden or obscured areas. Parking lot design shall consider:
    - » Planting strips and landscaped traffic islands, medians, or bump-outs shall also be provided within lots to break up the expanse of hard surface;



*Landscaped medians and islands*



*Pedestrian-scaled walkways and landscaping enhance safety, movement and comfort*

- » Pedestrian-scaled lighting, walkways, landscaping, and signage to enhance pedestrian safety, movement and comfort through parking lots to the adjacent sidewalks;
  - » Pedestrian routes through parking areas shall be wide enough to accommodate comfortable travel and their walking surfaces shall be clearly differentiated from the parking areas with texture, material, colour changes or markings;
- iv) Where existing parking areas are adjacent to the sidewalk, a landscaped area shall be located between parking spaces and the sidewalk. Landscaping shall provide clear sight lines at eye level.
  - v) Bicycle parking and racks shall be provided in locations that are close to building entrances but situated to avoid any conflicts for movement along pedestrian routes.
  - vi) Where possible, access to parking areas shall be provided from side streets and laneways. Access to parking areas shall be defined through clearly designated entrances and exits.

## 6.6 Commercial Designation

- a) The County shall encourage the development, redevelopment and rehabilitation within the Commercial Designation, identified on **Schedule B**.
- b) The County will encourage new development within the Commercial Designation in accordance with the policies of Section 7.11 of the Norfolk County Official Plan and consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.
- c) All development within the Commercial Designation shall be compatible with existing development in proximity.

## 6.7 Major Institutional Designation

### 6.7.1 General Policies

- a) The County shall encourage the development, redevelopment and rehabilitation within the Major Institutional Designation, identified on **Schedule B**.
- b) The County will encourage new development within the Major Institutional Designation in accordance with the policies of Section 7.14 of the Norfolk County Official Plan and consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.

- c) All development within the Major Institutional Designation shall be compatible with existing development in proximity.

## **6.7.2 Former Port Dover Secondary School Site Specific Policy Area**

- a) On lands designated as Major Institutional – Site Specific Policy Area 6.7 on **Schedule B**, in addition to the permitted uses of the Major Institutional Designation, an elementary school, a day care facility, a public library and public recreational uses may be permitted.

## **6.8 Parks and Open Space Designation**

- a) The County shall encourage the development, redevelopment and rehabilitation within the Parks and Open Space Designation, identified on **Schedule B**.
- b) The County will encourage new parks and community facilities to be identified within the Parks and Open Space Designation in accordance with the policies of Sections 5.6 and 7.15 of the Norfolk County Official Plan and consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.

## **6.9 Industrial Designation**

### **6.9.1 General Policies**

- a) The County shall encourage the development, redevelopment and rehabilitation within the Industrial Designation, identified on **Schedule B**.
- b) The County will encourage new development within the Industrial Designation in accordance with the policies of Section 7.13 of the Norfolk County Official Plan and consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.
- c) All development within the Industrial Designation shall be compatible with existing development in proximity.

### **6.9.2 Silver Lake Industrial Site Specific Policy Area**

- a) On land designated Industrial– Site Specific Policy Area 6.9.2 on **Schedule B** to this Plan, a trucking operation shall not be permitted and any proposals for redevelopment shall take into account the nature of the surrounding land uses.

## **6.10 Major Public Infrastructure Designation**

- a) The County shall encourage the development, redevelopment and rehabilitation within the Major Public Infrastructure Designation, identified on **Schedule B**.

- b) The County will encourage new development within the Major Public Infrastructure Designation in accordance with the policies of Section 7.17 of the Norfolk County Official Plan and consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.
- c) All development within the Major Public Infrastructure Designation shall be compatible with existing development in proximity.

## 6.11 Hazard Land Designation

### 6.11.1 General Policies

- a) Lands identified as within the Hazard Land Designation identified on **Schedule B** shall be subject to the policies of Section 7.3 of the Norfolk County Official Plan.

### 6.11.2 Restaurant/Spa Site Specific Policy Area

- a) On land designated Hazard Lands – Site Specific Policy Area 6.11.2 on **Schedule B** to this Plan, in addition to the uses permitted, a restaurant and a spa in conjunction with the restaurant shall be permitted.

## 6.12 Provincially Significant Wetland Designation

- a) Lands identified as within the Provincially Significant Wetland Designation identified on **Schedule B** shall be subject to the policies of Section 7.4 of the Norfolk County Official Plan.

## 6.13 Significant Woodlands

- a) Lands identified as within the Significant Woodlands Designation identified on **Schedule B** shall be subject to the applicable policies of Sections 4.6 and 5.5 of the County of Norfolk Official Plan.
- b) In the consideration of development applications in which there is a net loss of any area within the Significant Woodland Designation, the County will require this loss be compensated by the developer with the replacement of trees, or cash-in-lieu of trees. The trees, or the cash-in-lieu of trees collected to purchase trees, shall be planted in a location to be determined by the County. In determining appropriate compensation, consideration shall be given to the significance and value of the ecological function that the existing tree inventory provides.

## 7.0 Networks and Infrastructure

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- a) This Plan includes **Schedule C: Roads Plan** and **Schedule D: Active Transportation Plan**. To implement the development of new roads, and active transportation facilities, the relevant policies of Sections 8.1, 8.2 and 8.3 of the Norfolk County Official Plan shall apply. Changes to the location of any potential roads, or trails that are identified on **Schedule C** or **Schedule D** may be implemented without an Amendment to this Plan, provided that the principles inherent to this Plan are achieved, to the satisfaction of the County. In addition, all roads, trails and active transportation facilities within Port Dover shall be designed to be consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.
- b) With respect to water transportation, the relevant policies of Section 8.6 of the Norfolk County Official Plan shall apply.
- c) With respect to matters related to required storm water management systems, the relevant policies of Sections 8.7 and 8.9.4 of the Norfolk County Official Plan shall apply. In addition, all storm water management facilities within Port Dover shall be designed to be consistent with the Urban Design Guidelines attached to this Plan as **Appendix I**.
- d) With respect to matters related to required utilities, telecommunications, water, and wastewater systems, the relevant policies of Sections 8.7 and 8.9 of the Norfolk County Official Plan shall apply.
- e) With respect to matters related to noise, vibration, odour and light emissions, the relevant policies of Section 8.8 of the Norfolk County Official Plan shall apply.
- f) With respect to matters related to waste management, energy supply and transmission and emergency services, the relevant policies of Sections 8.10, 8.11 and 8.12 of the Norfolk County Official Plan shall apply.
- g) With respect to matters related to capital and public works, the relevant policies of Section 8.13 of the Norfolk County Official Plan shall apply.

## 8.0 Implementation and Monitoring

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- a) This Plan applies to all planning decisions within the boundaries of the community of Port Dover, as identified on the Schedules to this Plan. To implement and monitor this Plan, all of the relevant policies of Section 9 of the Norfolk County Official Plan shall be applied.

This Plan promotes significant reliance on the principles and policies related to Urban Design. In addition, Urban Design Guidelines have been established and are attached to this Plan as **Appendix I**. To assist the County with the implementation of the principles, policies and guidelines related to urban design the County will utilize the complete application requirements of Section 9.7 of the County Official Plan and may establish:

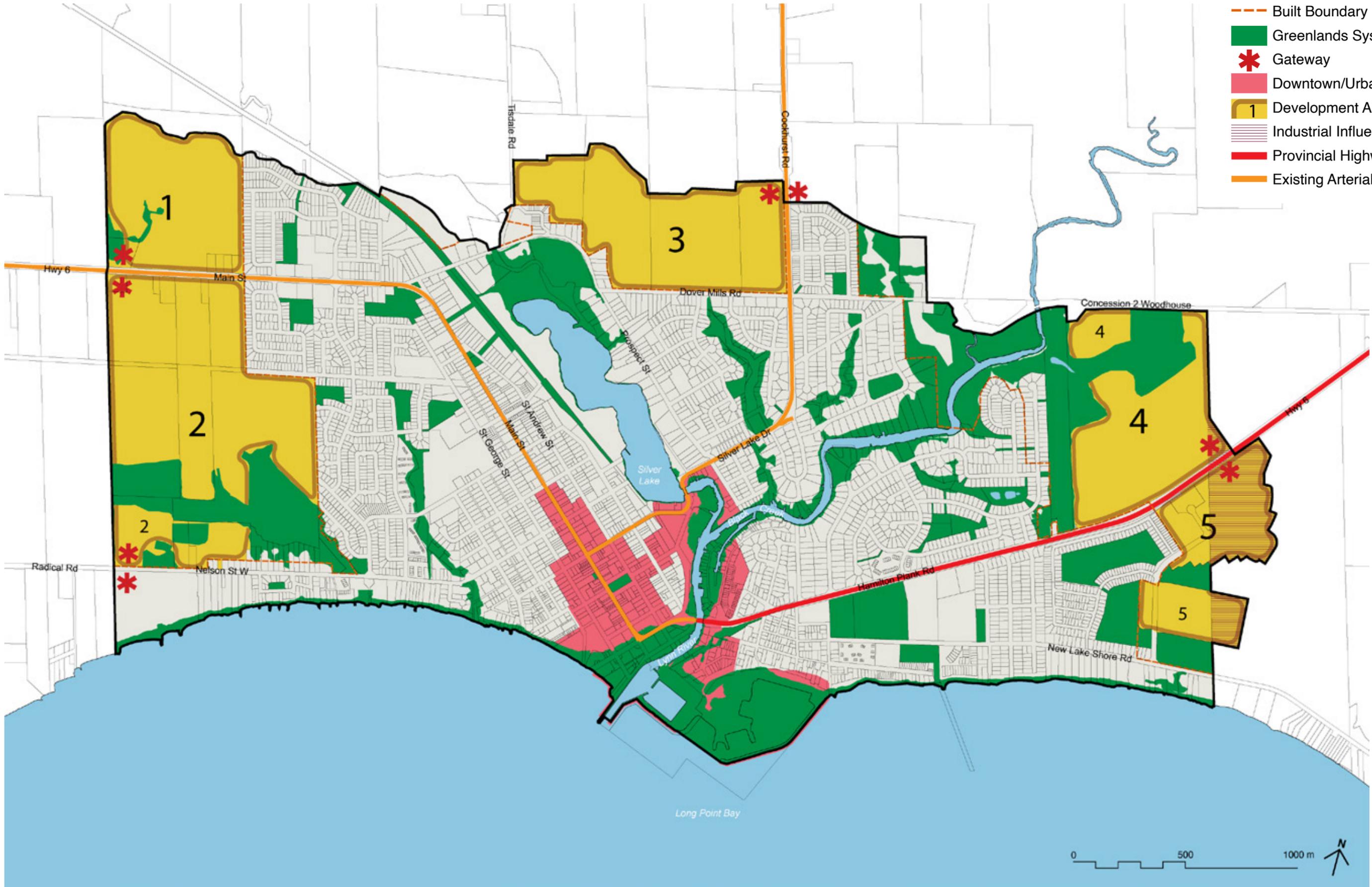
- i) An urban design peer review protocol;
- ii) A process for architectural control, including the appointment of a control architect(s); and/or,
- iii) The creation of a Design Review Panel.

# Schedule A: Community Structure

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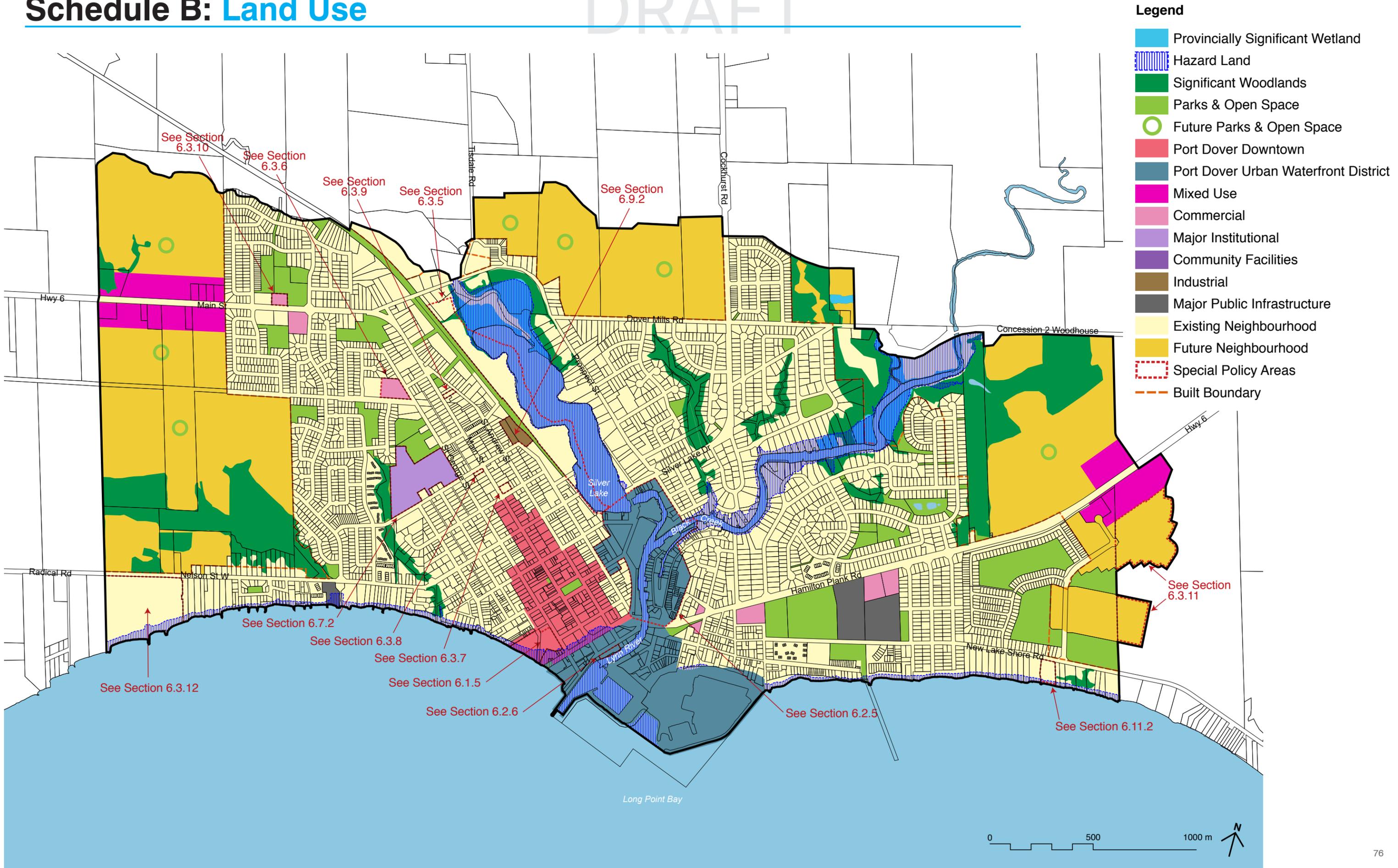
## Legend

- Settlement Boundary
- - - Built Boundary
- Greenlands System
- \* Gateway
- Downtown/Urban Waterfront
- 1 Development Areas
- ▨ Industrial Influence Area
- Provincial Highway
- Existing Arterial Road



# Schedule B: Land Use

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# Schedule C: Roads Plan

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## Legend

- Existing Provincial Highway
- Existing Arterial Road
- Existing Collector Road
- Potential Arterial Road
- Potential Collector Road
- Local Road



# Schedule D: Active Transportation Plan

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## Legend

- Existing Off Road Trail
- Proposed Off Road Trail
- Existing Paved Shoulder
- Proposed Paved Shoulder
- Proposed Bike Lane
- Proposed Signed Route With Sharrows
- Proposed Signed Route
- Lake Erie Waterfront Trail
- Existing Water's Edge Trail
- Proposed Water's Edge Trail



# Appendix I: **Port Dover Urban Design Guidelines**

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## 1 Introduction

The Port Dover Secondary Plan is the guiding document used to direct and manage growth in Port Dover. It articulates the vision and guiding principles for how the community should be developed and outlines the policies for how land in the community should be used. The Secondary Plan helps to ensure that future planning and development meets the specific needs of the community. Urban Design Guidelines provide a 'how-to' manual for implementation of the Secondary Plan's policies that reinforce the character and charm of Port Dover, respects its natural and cultural heritage and protects the small town feel cherished by those that live here.

Urban design involves the arrangement and design of the built environment and provides a framework that gives form, shape and character to buildings, public spaces, streets and amenities. It blends architecture, landscape architecture and town planning together to make urban centres, districts and neighbourhoods functional and attractive. The Urban Design Guidelines promote high quality urban design that is based upon the quality, scale and character of the surrounding existing and emerging contexts, reinforce 'human scaled' environments, and promote a sense of place.

Urban Design Guidelines (Guidelines) provide design guidance, criteria, and standards for how to shape the built environment, both the individual elements, as well as how these should be spatially arranged and relate to one another. The Guidelines are intended to provide direction for homeowners, designers, architects, landscape architects and developers to understand the County's objectives for design in Port Dover. The Guidelines also inform staff and Council in their review, and approval of development applications. The Guidelines will be used through the development process including Subdivision Planning, Site Planning, Landscape and Building Design.

The Urban Design Guidelines are an appendix to the Port Dover Secondary Plan. Some guidelines will be brought forward to become policy in the

Secondary Plan, others will be implemented through zoning, others through site plan control.

As such, they are not a "statutory" part of the Secondary Plan, and conformity to these Guidelines is not required. Urban Design Guidelines are always interpreted with inherent flexibility. However, they will be subject to the test "to be consistent with". These Guidelines are also intended to be given statutory force through the Implementing Zoning By-law, and/or through Site Plan Approval.

The Guidelines are organized to provide direction on development and redevelopment in the private and public realm. Some of the Guidelines apply to specific districts or areas within Port Dover, and others apply to particular uses or functions throughout the community of Port Dover.

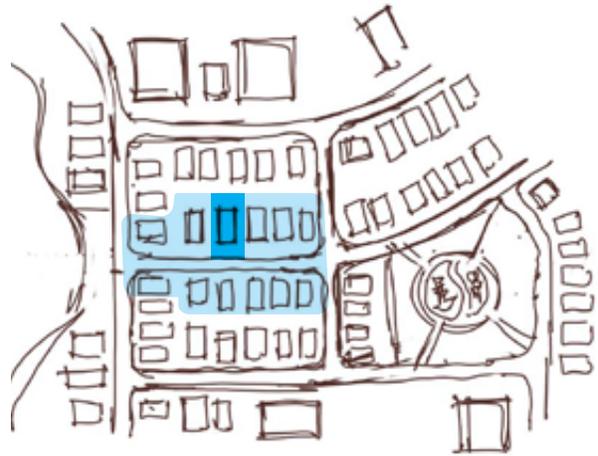
## 1.1 Compatibility

The Guidelines provide a framework for design that respects and reinforces the character of the Downtown Port Dover, the waterfront and the existing neighbourhoods. The Guidelines are based on a contextual approach to design that considers the visual impact to and relationships with adjacent and surrounding developments. This approach promotes compatible forms and designs, pedestrian scaled and oriented streetscapes, and allows for appropriate flexibility, innovation and diversity in design, qualities intrinsic to evolving communities.

In determining compatibility, an area of influence in the vicinity of the new development is used. New development should be compatible with the existing development within its area of influence. The scale of new development determines the appropriate scale of the area of influence. Norfolk County staff should be consulted in the determination of the appropriate size of the area of influence.

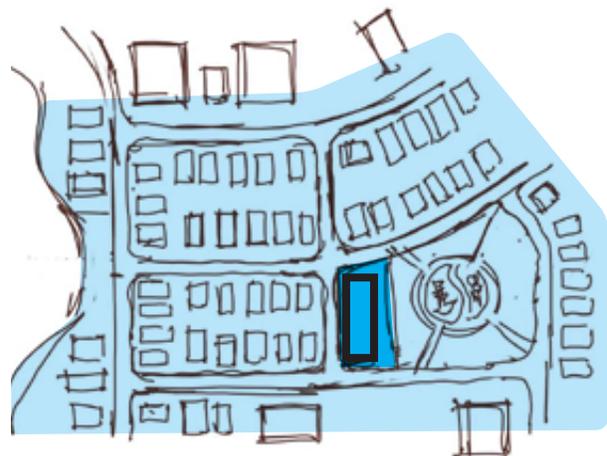


*The area of influence for modifications to an existing site or building will generally be the immediate neighbouring and facing properties.*



*The area of influence for demolition or redevelopment of a single house or property will generally be the block.*

Scale of new development	Area of influence for determining compatibility
modifications to an existing house or property	existing house and immediate neighbours (including across the street)
demolition or redevelopment of a single house or property	streetscape/block
large scale redevelopment; development with land assembly; intensification	neighbourhood



*The area of influence for large redevelopment sites will generally be the streetscape and neighbourhood.*

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Larger developments must consider impacts to the neighbourhood. Guidelines for determining the scale of the neighbourhood include:

- a local area where there is convenient access to one or more land uses or amenities common to daily living, such as housing, a school, park, shopping, personal/professional/medical services, daycare, and/or community uses;
- more than one block; usually several blocks linked together to form a network;
- an area bounded by a five-minute walk or 400 metre radius; and
- edges can be defined by major roads and/or natural features.

## 2 Downtown and Urban Waterfront

### 2.1 Introduction

Downtown and the Urban Waterfront are the heart and soul of Port Dover. Both areas establish the inherent small town character of Port Dover with its commercial fishing harbour - each forming critical components of its sense of place. Guidelines in this chapter establish direction for development and redevelopment in both areas.

#### Port Dover Downtown

Downtown has a mix of commercial, institutional and residential uses, and is anchored by Main Street. A grid pattern of streets create small blocks, making it easy to walk around and connect to the waterfront. Powell Park, an urban park, is a major focal point.

Buildings exhibit a variety of materials, with brick and siding most commonly used. There are a number of buildings with a heritage designation or potential concentrated Downtown.

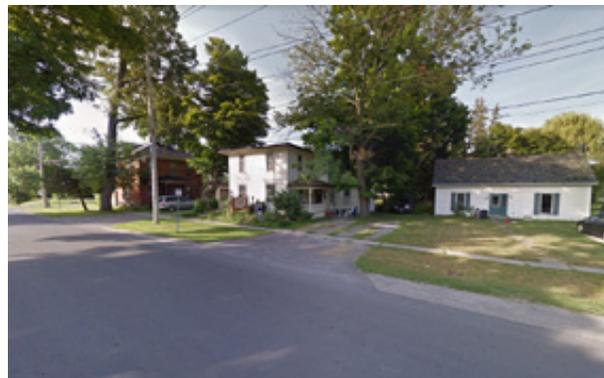
#### Main Street

In the core of the Downtown area, buildings are located close to the street line and to one another, resulting in a well-defined streetwall. Commercial buildings are typically 2-storeys in height, with flat roofs. Many buildings have an historic character that create a distinct sense of place. These are design characteristics that these guidelines seek to reinforce.

At each end of Main Street, buildings are more widely spaced, typically one storey in height, with varying set backs. On-street parking is replaced by vehicular lanes and parking is provided in large surface lots. These areas are more suburban in character and oriented to vehicular access. As these areas change or redevelop over time, these Guidelines will shape a more pedestrian-oriented and well-defined Main Street through Downtown.

#### Downtown Residential

The streets around Main Street are more residential in character, although they also have commercial uses mixed in. Buildings have set backs from each other and from their neighbours while still defining the street edge. There is a mix of historic, traditional and more modern architectural styles. Parking is often accommodated in side driveways or garages set back from the street edge. Large mature trees make a significant contribution to the attractiveness of this area.



# DRAFT



## Main Street Existing Character

The photograph and diagram on this page illustrate fundamental design qualities of Main Street and underpin the guidelines that follow.



1. Buildings frame the street edge
2. Two storey street wall
3. Fine grain with narrow storefronts
4. Transparency at ground level
5. Frequent entrances
6. Signage within a sign band

## Port Dover Urban Waterfront

The Urban Waterfront has a rich history as a commercial fishing port that continues today, particularly on the east side of Lynn River with both a land and water based function. The lands along the Lynn River to the north of the harbour are no longer used as part of the commercial fishing industry and are in the process of being redeveloped for residential uses. The west side of the harbour is a key destination for residents and visitors with a hub of restaurants and shops that cater to beach goers.



The Urban Waterfront includes two piers marking the entrance to the Lynn River. The Port Dover Lighthouse is at the end of the West Pier, and is a popular spot to visit. The West Pier includes seating and safety railings along its length. Just to the east is the commercial fishing basin and its associated port area. This working waterfront area is an important economic driver for Port Dover and provides an authentic, industrial character along the lake.



Further east is the Port Dover Harbour Marina with approximately 400 slips and a commercial basin used by the Coast Guard and Ministry of Natural Resource and Forestry.

Buildings in Port Dover's Urban Waterfront area are simple, functional buildings. There are many rectangular structures with peaked roofs and gable ends, clad in wood or metal siding. Public areas along the water's edge use durable, functional materials such as steel and concrete in paving, walls, bollards, railings and furniture.

## 2.2 General Building Guidelines for Downtown and Urban Waterfront

This section applies to all sites in the Downtown and Urban Waterfront. Additional guidelines in this document, for example, for parks, streets or active transportation, may also apply within the Downtown and Urban Waterfront.

### Height & Massing

- 1 Taller buildings may be encouraged at strategic locations such as corner sites and other prominent sites to enhance community structure, sense of place and provide landmarks.

### Placement & Orientation

- 2 Where more generous setbacks are appropriate, such as along streets with residential uses, the space should be used for landscaped areas, additional street tree planting, amenity areas, seating, display areas or sidewalk cafés and patios.
- 3 Along commercial street frontages, modest setbacks (typically 1 - 2 metres) could be provided where the setback provides public amenity or benefit. This setback zone should be designed as an extension of the sidewalk zone. It could accommodate outdoor cafe or patio space, retail display, seating, additional street trees, and/or other amenities. When not in use for patio or retail display, the setback zone should be accessible to the public as a seamless extension of the sidewalk.

### Street Wall / Facades

- 4 Building materials should reflect and complement the existing materials in the area and should be high quality, durable and easily maintained.
- 5 The materials selected should be consistent for a building's facade and any walls that are publicly visible.



Port Dover • Materials

6

6 Recommended building materials include brick, stone, wood and glass. One or two of these materials should be selected as base materials and may be complemented by a wider range of accent materials.

7 Design may incorporate contemporary materials, patterns and textures where materials and fastening systems are authentic to their purpose and neatly detailed.

8 Changes of material will be purposeful and coincide with substantial massing elements or datum lines of the building.

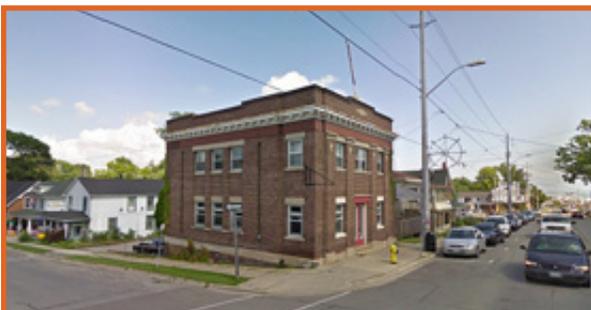


9

9 Where building sides are exposed to public view, the materials on the front facade should wrap around and extend a minimum of 2.0m on the side elevation to avoid the appearance of an 'applique facade,' where the front facade appears insubstantial or tacked-on.

10 The principal material of exposed side facades may differ from the front facade, but should be compatible with it. The side facade treatment should reference key architectural datums such as cornices, floor lines, or vertical rhythms.

11 Spaces between buildings (such as alleyways) should be well-lit and enhanced through fenestration, side entrances, decorative signage, decorative paving, and public art.



Port Dover • Treatment

13

12 Architectural features and decorative elements such as shutters, cornices, awnings, building projections, distinctive roof features, etc. which add visual interest are encouraged on all buildings.

13 Additional architectural treatment is encouraged for corner sites and landmark buildings to enhance the visual prominence of these buildings and their locations.

# DRAFT

- 14** The ground floor should be articulated in a manner that distinguishes it from upper storeys, for example, through canopies, awnings, lighting and signage.
- 15** Blank walls should not be permitted on any building fronting a street.
- 16** Renovations of or changes to existing historical building designs, or new designs in historical styles, should be designed or reviewed/approved by registered, qualified architects who have experience in designs of that stylistic expression to ensure building massing, proportion, and details exhibit integrity to historical usage.
- 17** There should be a consistent colour palette for a building's facade and any walls that are publicly visible. The colour palette should complement the building expression and/or business. Colours that enhance the individuality of a building or business, while complementing the building character of the streetscape, are encouraged.
- 18** Mechanical equipment should be screened from view.

## Shopfront Signage

**19** Signage should be located and designed to reflect the heritage character of the streetscape, while allowing for the creativity of individual businesses. Diversity in storefront signage should be encouraged to create shopfronts with "personality."

**20** Artistic expression and imagination are encouraged.

**21** Signage should be attractive, durable, easy to read and complementary to the overall facade design.

**22** Sign scale should be in proportion with the building and should reflect the pedestrian scale of the streetscape.

**23** Signs should be located outside the pedestrian clearway and, when located over pedestrian areas, should have a minimum clearance of 2.4m from grade.

**24** Signage should not obscure windows, doors or architectural features.

**25** Fascia signs within a sign band, window signs and signs hanging perpendicular to the building facade are preferred. Sidewalk retailing and sandwich board signs are also encouraged to create vibrancy and interest along the streetscape.



## Shopfront Awnings, Canopies and Lighting

**26** Awnings and canopies are encouraged for weather protection and shelter, for additional signage opportunities and for the aesthetic appeal of a facade. Retractable awnings are preferred as they are flexible for diurnal and seasonal changes.

**27** Awnings and canopies should align with modules of the building's vertical rhythm, such as the space between column or pilaster elements, or the divisions between windows and doors.

**28** The design, shape, colour and material selected for awnings and canopies should be complementary to the design of the building's facade and should reflect the character of the context.

**29** Signage on awnings should be located on the valence.

**30** Awnings should not obscure windows, entrances or architectural elements on a facade, or impede views down a street.

**31** Creative exterior and shop window lighting is encouraged to promote vibrant streetscapes at night, encourage pedestrian traffic and enhance the safety of the pedestrian experience.

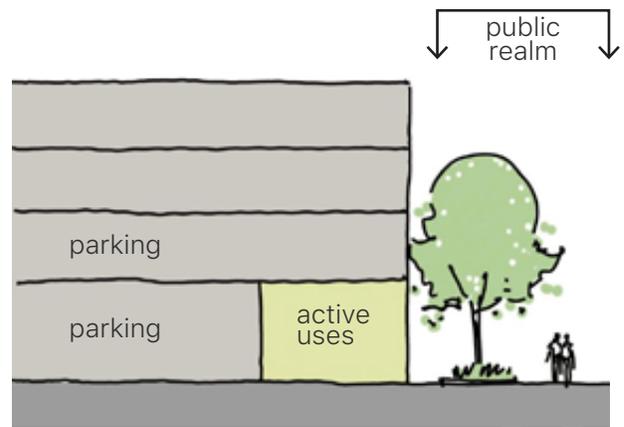
**32** Awnings, canopies and lighting should have a minimum clearance of 2.4m from grade to allow for unencumbered pedestrian access underneath.



## Parking

**33** For large developments, the preferred parking location is underground.

**34** Above ground structured parking located **at ground level** should be hidden from public view on all sides facing public streets and the water's edge. Habitable space within the building should be located between the structured parking and the public realm. The habitable space should face the public realm inclusive of windows and entrances. The habitable space could include, for example, retail uses, or portions of residential dwellings.



34 35

**35** Above ground structured parking located **above ground level** may be located on the exterior, public-facing facade of the building provided it is well-integrated with the building design. The principal, high quality massing and facade design, inclusive of columns, datum lines, materials, projections, solid/void relationships and other design elements, should be continuously applied across any **above ground level** parking visible from adjacent streets or the water's edge.

**36** Surface parking should be small in scale and located internal to sites where it is screened from view of the adjacent streets or the water's edge by the principal building massing.

**37** Additional landscaping should be provided to screen parking where parking elements (garage entrances, structured parking, parking lots) are visible from public streets, the water's edge, courtyards or walkways.



*Parking on upper levels. Facade treatment of upper levels is high quality and consistent throughout entire building expression.*

## 2.3 Urban Waterfront Character

The Port Dover Urban Waterfront has a distinct and established character in its recreational, tourism, industrial and port uses and buildings. New development should be compatible with and reinforce the existing nautical character.

- 1 Building and site elements should incorporate design strategies such as:
  - a. simple, rectangular building shapes;
  - b. peaked roofs, shed roofs or flat roofs; avoid hip roofs;
  - c. gable ends;
  - d. horizontal, vertical or shingle siding in wood, cement board, high quality vinyl or metal;
  - e. standing seam metal roofs;
  - f. simple trim elements such as , frieze and fascia boards, and window surrounds;
  - g. public art or signage that reinforces the nautical context;
  - h. simple, durable marine-grade site furnishings and details for lighting, awnings, railing, bollards, seating and the like.



## 2.4 Water's Edge Sites

Lake Erie and the Lynn River are fundamental to the history, existing character and ongoing activities of Port Dover. The water itself and the publicly accessible edges along the shorelines are key public spaces in Port Dover and define its image. Buildings along the water's edge help create the first impression of Port Dover's beaches, walkways and streets that are so beloved by residents and tourists. It is critical that the buildings along the water's edge reinforce a high quality image and frame the public realm. The following Guidelines apply to all properties within the Downtown and Urban Waterfront that are adjacent to Lake Erie or the Lynn River.

New buildings should not overwhelm these public spaces by abrupt increases in height over the established character of the community. They should look out onto the waterfront with active uses and use of transparency in their facades. It is recognized that new buildings may be taller as the waterfront intensifies, however, new buildings should be shaped by the following guidelines.

### Site Organization

- 1 A clear pedestrian zone should be provided as near the water's edge as possible that provides for linkages to existing or planned future pedestrian zones on adjacent properties. Over time, pedestrian zones will link together to provide continuous public access to the water's edge.
- 2 Large sites should provide outdoor, ground level, accessible connections between the water's edge pedestrian zone and an adjacent public street. This can take the form of a landscaped walkway, a publicly accessible street or lane, a publicly accessible privately owned open space, a park, or other means.
- 3 Where buildings have residential units at ground level, provide a 'front yard' landscape zone between the unit and the pedestrian zone to include demarcation of the private realm front yard space, for example, with a low wall, low permeable fence, and/or landscaping.
- 4 Locate parking and servicing functions to the centre of the site. Minimize interruptions for access, servicing or mechanical equipment along publicly accessible frontages.

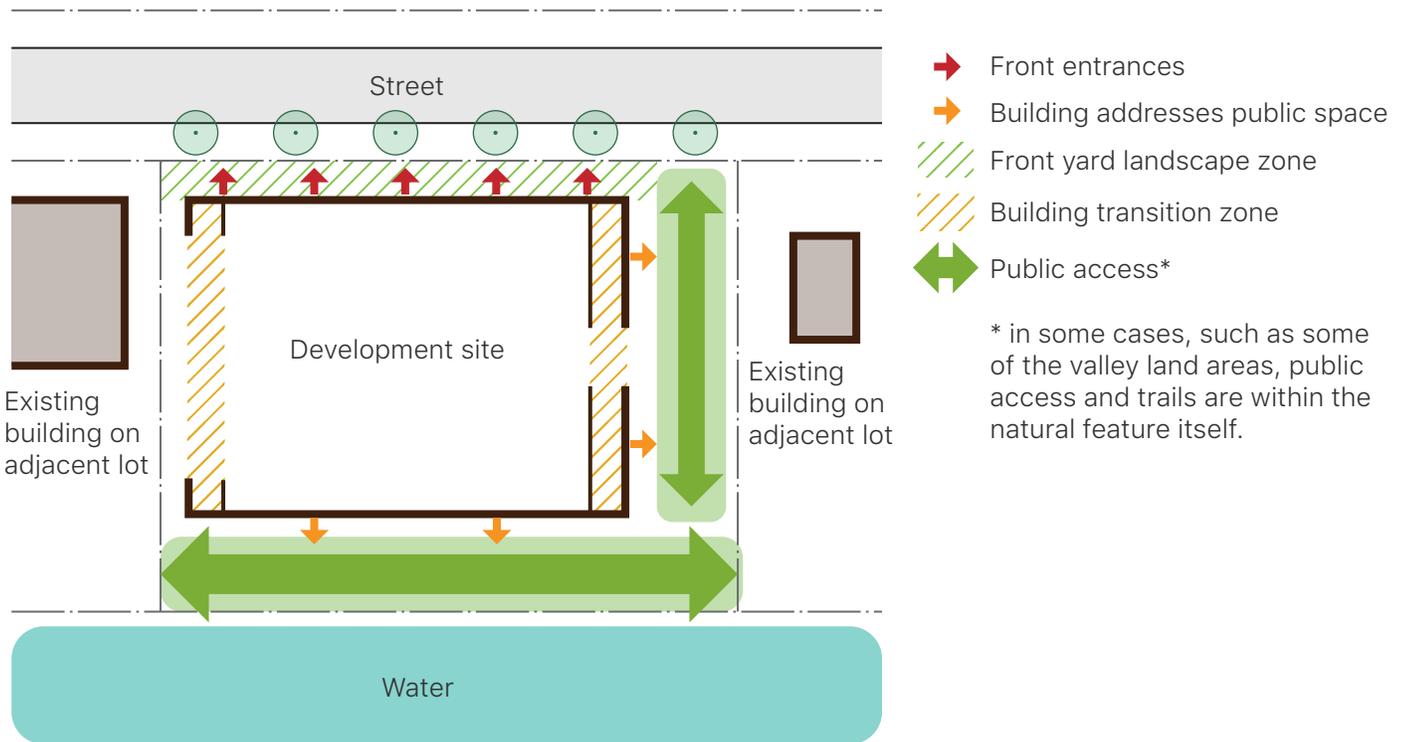


*Ground floor units with demarcated front yard landscape zone and direct access to adjacent walkway.*

## Building Design

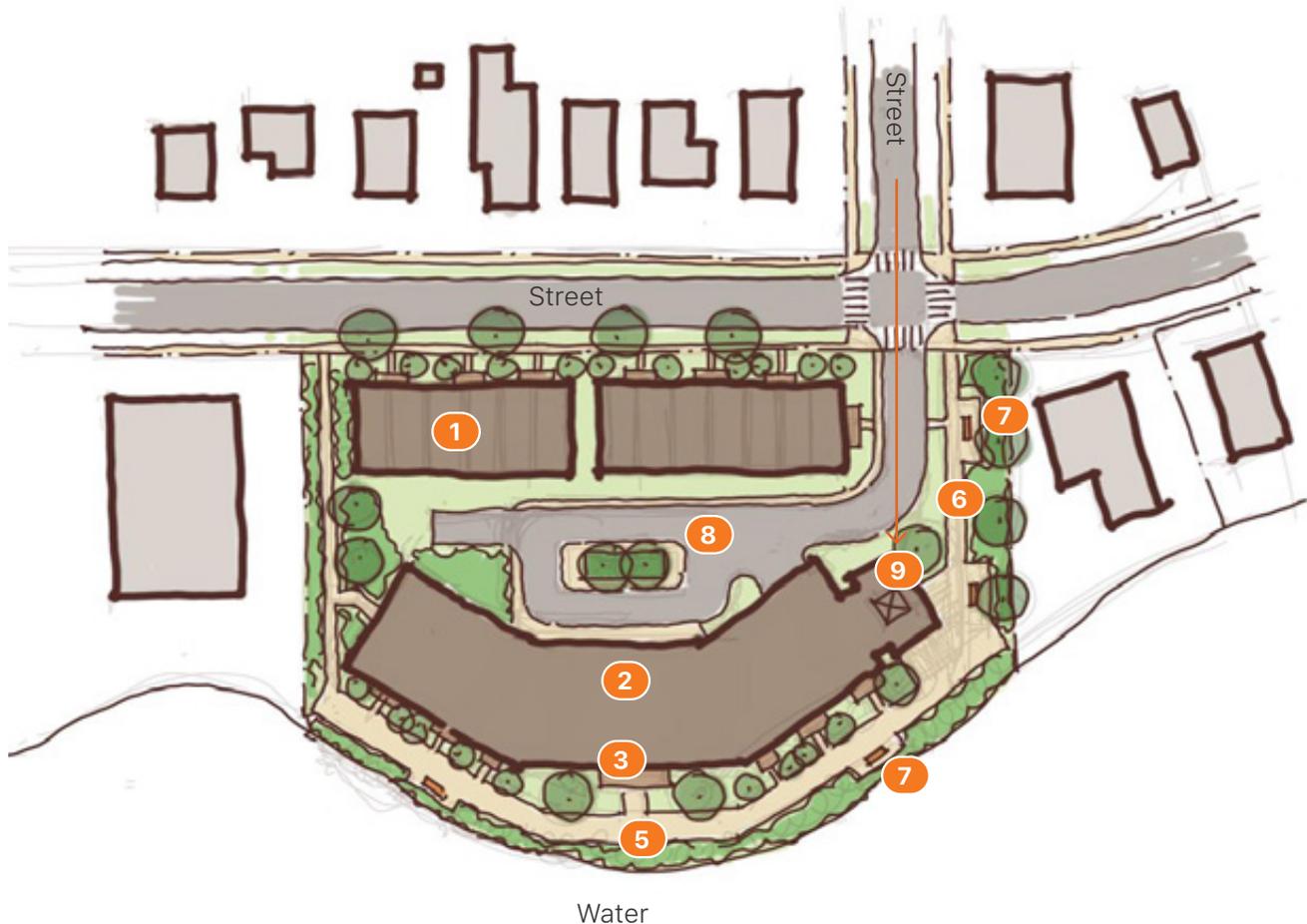
- 5** Ensure new building(s) have a transition to existing buildings on adjacent lots through setback, step back, street wall height, and overall building height within a transition zone adjacent to those lots.
  
- 6** Buildings facing the water's edge should be designed with a high quality 'frontal' appearance consistent with the other built form guidelines in this section that apply to buildings facing public streets and spaces. In particular, the ground level should be designed to maximize the following design elements:
  - a.** active uses at ground level facing the water's edge, such as commercial/retail, residential units, or common interior spaces such as lobbies and amenity areas. Servicing and utility areas should be minimized;
  - b.** direct building entrances facing the water's edge;
  - c.** a high degree of transparency: windows and doors;
  - d.** for residential units at ground level, entrances to individual units with walkway connections to the pedestrian zone walkway/trail.

# DRAFT



## Demonstration Concepts

Showing how the application of these Urban Design Guidelines can shape the conceptual design of the public realm and built form



1. Townhouses face public street and low-rise streetscape; direct entrances to individual units
2. Taller apartment building set back from street
3. Main lobby entrance along water's edge walkway
4. Ground level apartments have direct entrances to public walkway
5. Public walkway along the water's edge extends to site boundaries for future extension to adjacent sites
6. Public walkway links public street with water's edge public walkway
7. Amenity areas and seating along public walkways
8. Drop off, servicing and parking entrance internal to the site
9. Building massing responds to view terminus location from public street

## Demonstration Concepts

Sketches developed at the public workshop showing how the application of these Urban Design Guidelines can shape the conceptual design of the public realm and built form

### Waterfront site at the foot of St. George, Walker and Harbour Streets



1. 'Shared street' approach to water's edge rights of way
2. Public views and access to waterfront maintained
3. Buildings frame the street edge
4. Parking and servicing functions located internal to the block

Top: St. George Street right of way maintained

Below: St. George Street right of way partially closed, new water's edge street connects Walker Street and Harbour Street



## 3 Existing Neighbourhoods

### 3.1 Introduction

Port Dover has existing neighbourhoods that span over a century of urban growth. There is a wide range in the character of buildings, landscape and streetscape conditions.

Some existing neighbourhoods are older, with narrow roads, mature trees, eclectic buildings (many cottage-like), and a strong definition of front and/or side yard gardens. Other neighbourhoods are newer, with standard local roads and housing in bungalow forms. While different, each neighbourhood has a distinct character that must be understood. The guidelines in this chapter seek to ensure that character is maintained with new development or redevelopment.

The Existing Neighbourhoods Guidelines apply to significant change to sites, lots and buildings within existing neighbourhoods. Examples of the types of change that are addressed by the following Guidelines include:

- facade remodelling;
- major additions;
- replacement dwellings;
- additional dwelling units on a lot;
- change in land use or new use;
- lot assembly; and/or
- significant intensification.

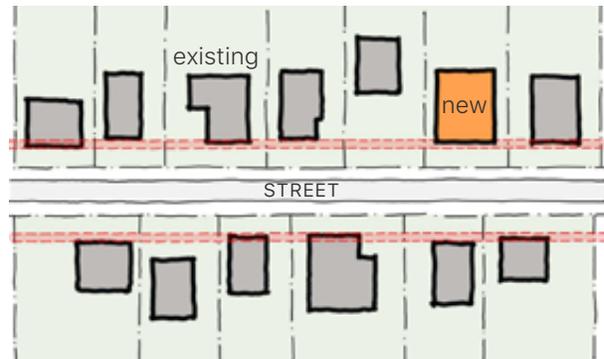
Guidelines in this chapter will most often be used for building additions, remodelling and reconstruction on a lot. Sometimes, the extent of change will be greater. Large sites with multi-unit buildings should consider the principles of the Downtown and Urban Waterfront Design Guidelines in addition to these Existing Neighbourhoods Guidelines.

## 3.2 Design Guidelines

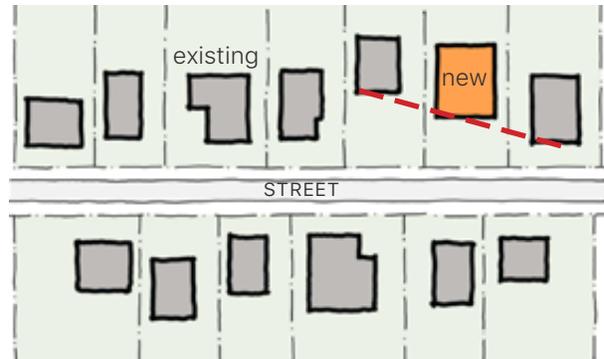
### Site Organization

#### Placement & Orientation

- 1 Building placement and siting on a property should be considered in relation to the street and its neighbours to reinforce the positive characteristics of the existing streetscape.
- 2 Maintain consistent setbacks along the street. New development should have a set back equal to the predominant setback (70%+) on the street (+/- 1.0m), or a distance that is the average of those on either side of the development site (+/- 1.0m).
- 3 Where there is no predominant setback along a street, new development should be set back a distance that is the average of those on either side of the development site (+/- 1.0m).
- 4 Generally, locate dwellings close to the street edge to frame the streetscapes, however, this will depend on the setbacks to houses on either side of the site.

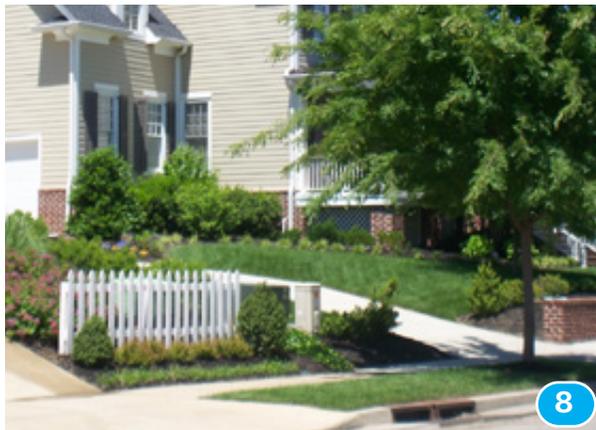
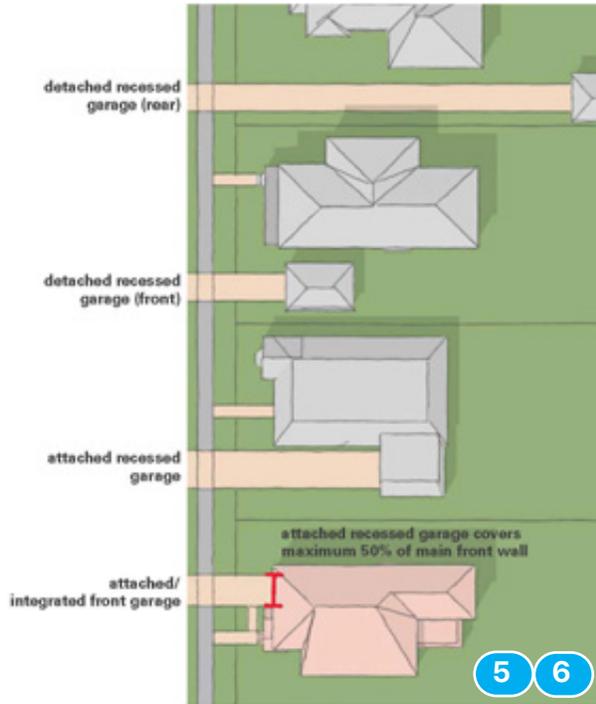


*New development is set back at the predominant set back along the streetscape.*



*New development is set back the average of set backs on adjacent properties.*

**1** **2** Placement and Orientation



## Access & Parking

- 5 Garages should be placed behind the front wall of the dwelling or at the side or rear of the lot, unless the predominant location (70%+) of the garage on other houses on the streetscape projects from the front of the house or is not at the side or rear.
- 6 Front-facing garages attached to the main dwelling should not occupy more than 50% of the building's width.
- 7 Driveways should be located and spaced to reinforce the rhythm along a street and to allow for street trees to be planted in the boulevard.

## Landscaping

- 8 Include landscaped areas in front of buildings that provide a transition from private to public areas. A minimum of 50% of the front yard zone should include soft landscaping areas (non-paved areas supporting grass, groundcovers, trees and/or shrubs).
- 9 Where the predominant (70%+) existing streetscape character has design elements such as low stone walls, low permeable fences, planting and/or other landscaping at the front of the lot, new development should provide similar elements.

## Building Design

### Massing & Elevation Articulation

**10** Ensure new building is generally consistent in height and massing with adjacent buildings along the streetscape.

**11** Provide appropriate transitions in height to/from existing adjacent buildings and ensure no new building is more than 1.5 storeys or 4.5m higher/lower than the adjacent dwellings, subject to the area's maximum height permission.

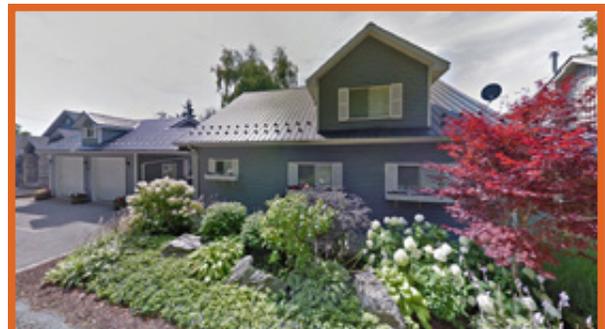
**12** Where possible, maintain the existing lot grading and the neighbourhood's characteristic first floor height.

**13** Design the building envelope, and individual architectural elements within the building, to reference the architectural treatment of buildings in the neighbourhood. The goal is not to replicate buildings of the neighbourhood, but to ensure new development relates to them by incorporating similarities in design language to promote compatibility. Massing and architectural elements to be considered include:

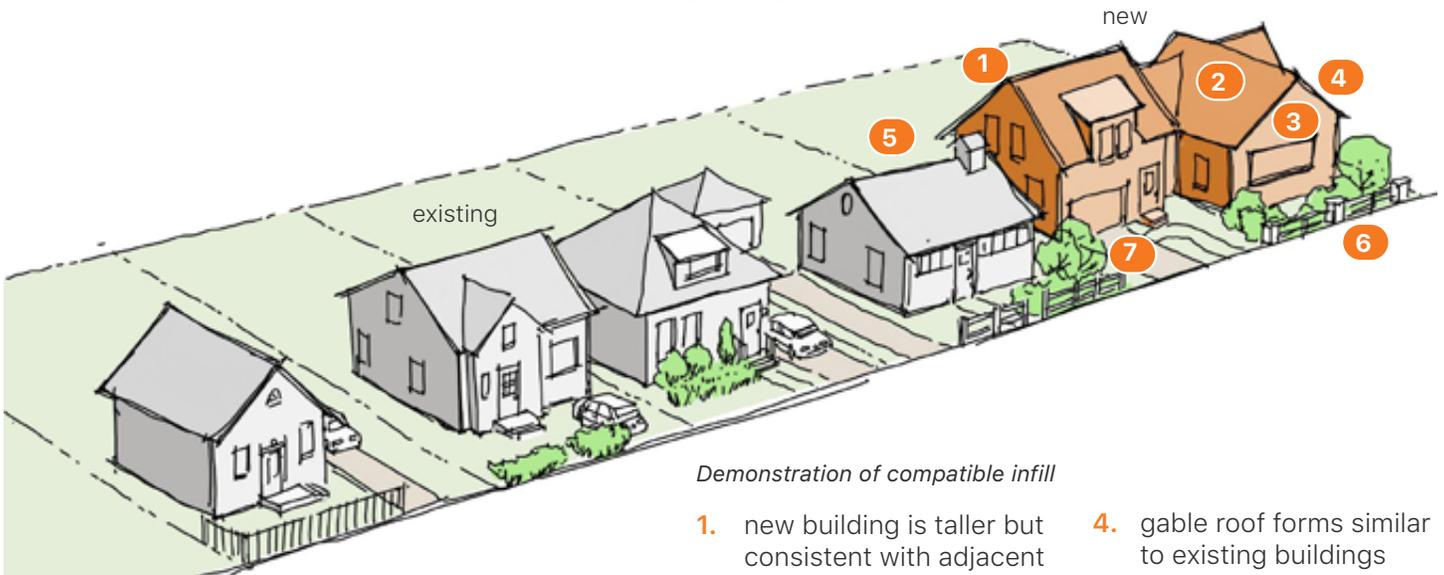
- a. similar building shape (square, rectangular, L-shaped, etc.);
- b. roof lines with similar massing, pitches and articulation (gable, hipped, shed, flat, use of dormers, etc.);
- c. similar principal building massing elements (bays, projections, first floor height, building height, entry features, etc.);
- d. similar architectural features (porches, stoops, chimneys, columns, frieze boards, etc.);
- e. important datum lines (cornice, base courses, string courses, window alignment, bays, etc.);
- f. similar proportions (bays, windows, garage, etc.).



*Example of new development that is compatible because of set back, massing, sloped roof form, material, and entrance prominence. Difference in architectural style is not as important.*



**Port Dover • Compatible new development**



*Demonstration of compatible infill*

1. new building is taller but consistent with adjacent buildings
2. massing of new, larger building is subdivided into smaller volumes that relate to the existing buildings on the street
3. the portion of the new building at the street edge has a similar proportion and width to the existing buildings
4. gable roof forms similar to existing buildings
5. second storey is partially within roof massing, similar to existing buildings
6. New landscaping defines a front yard zone similar to existing yard treatments
7. garage is set back

**14** Articulate facades to reflect the rhythm and proportion of solids/voids, walls/windows, of neighbouring dwellings.

**15** Ensure the elevation design reflects that of the adjacent homes. The horizontal expression of the windows and door should be similar to surrounding homes and their vertical rhythm should reflect those of adjacent homes.

**16** Design semi-detached unit facades as one elevation.

**17** Ensure facade details throughout all building elevations are consistent with their intended architectural expression.

**18** Avoid mixing historic architectural elements with other architectural styles elements.

**19** Historical designs should only be undertaken by registered, qualified architects who have experience in designs of that historic period.

**20** Contemporary designs may be considered provided they exhibit consistency with the massing and articulation guidelines in this section.

**21** Traditional designs may be considered where they are found in the neighbourhood.

## Materials

**22** Building materials should reflect and complement the existing materials in the area and should be high quality, durable and easily maintained.

**23** The materials selected should be consistent for a building's facade and any walls that are publicly visible.

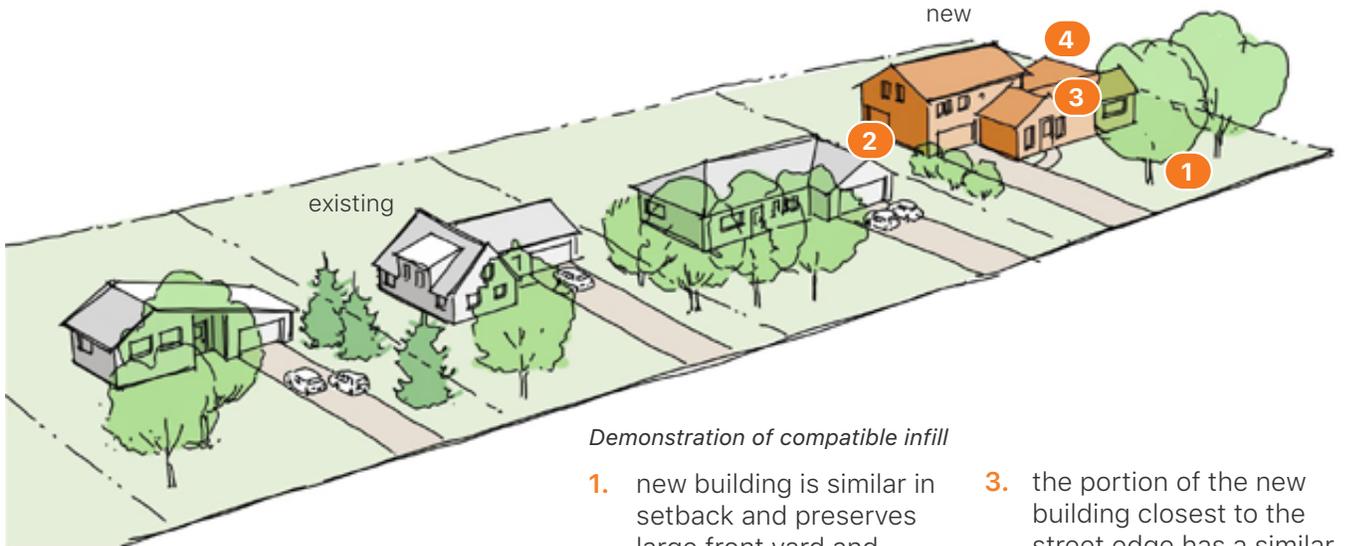
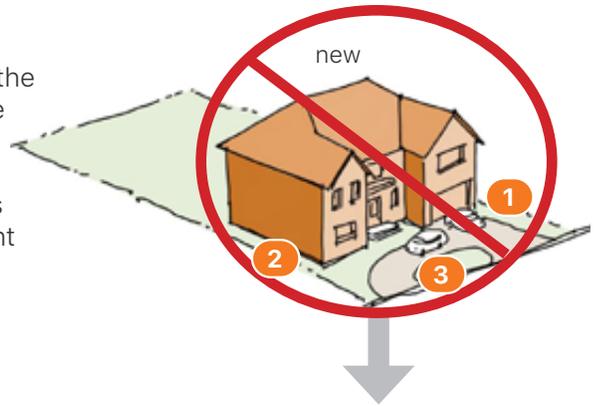
**24** Recommended building materials include brick, stone, wood, glass and concrete; one or two of these materials should be selected as base materials and may be complemented by a wider range of accent materials.

# DRAFT

## Incompatible infill

New building is dissimilar to the well-defined character of the existing context:

1. small front yard setback
2. small side yard setbacks
3. driveway dominated front yard

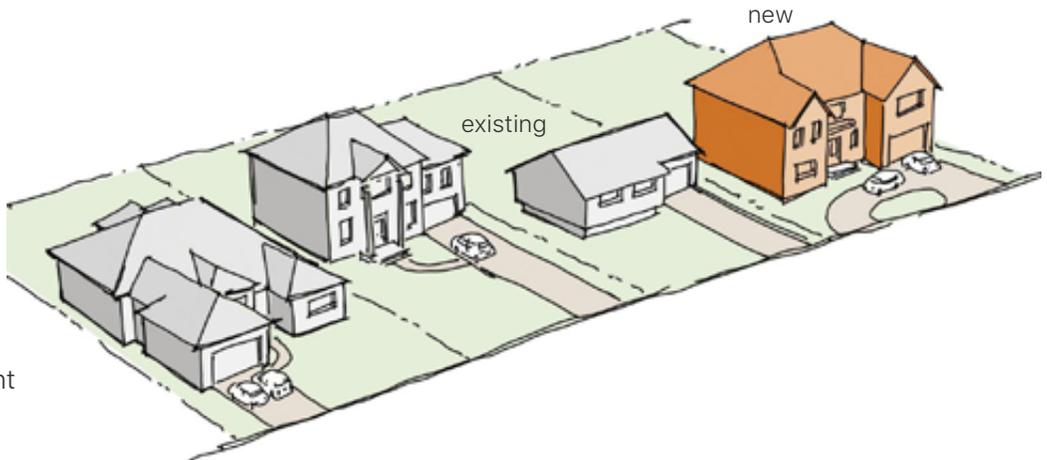


## Demonstration of compatible infill

1. new building is similar in setback and preserves large front yard and mature trees
2. new building is similar in side yard setback
3. the portion of the new building closest to the street edge has a similar low profile to the existing buildings; taller portions are set back
4. similarity in massing and roofline elements

## Compatible infill

The same building as shown at top may be compatible infill in a context that is less well defined and exhibits smaller setbacks, more varied or larger massing, less defined landscaping, few trees, and more recent architectural styles.



## 4 Future Neighbourhoods

### 4.1 Introduction

This chapter will be most relevant to low-rise housing forms such as single detached, semi-detached, townhouses, and stacked townhouses. New neighbourhoods may also include mixed uses and other non-residential uses, and these are covered by other sections of these guidelines.

Greenfield neighbourhoods in Port Dover will continue to be defined by the natural features that surround them and be connected to the overall community through their respective main spines, and green space and trails.

### 4.2 Design Guidelines for Greenfield Neighbourhoods

#### Site Organization

##### Lotting

- 1 Provide a transition in lot sizes, setbacks, massing, and grading that complements the existing and planned context.
- 2 Promote a variety of lots and building forms along streetscapes.

##### Placement & Orientation

- 3 Create consistent edges along public streets and open spaces.
- 4 Promote multi-storey buildings that contribute to a sense of enclosure along the street, particularly at locations.
- 5 Minimize the visual impact of long blocks; turn lots located on the end of blocks 90-degrees to face the perpendicular road, where appropriate. Consider a variety of lot facing conditions, in addition to flankage lots, along long stretches of road.

- 6 Orient lots and built form to front streets that are primary neighbourhood entry locations from the surrounding arterial and collector road pattern. Orient lots and built form to address primary internal neighbourhood streets such as collector roads.
- 7 Orient dwellings to face the public realm including streets, pedestrian connections and open space, to increase casual surveillance and foster a sense of increased safety.
- 8 Limit townhouse blocks to 8 units/modules.
- 9 For fronting townhouse buildings facing open spaces or common private lanes/ roads, provide a minimum 15.0m separation distance between buildings (front to front).
- 10 Avoid property and lot configurations that lead to front yards facing or being adjacent to rear yards or other non-frontal areas such as driveways, parking lots, or servicing areas.

## **Access & Parking (AP)**

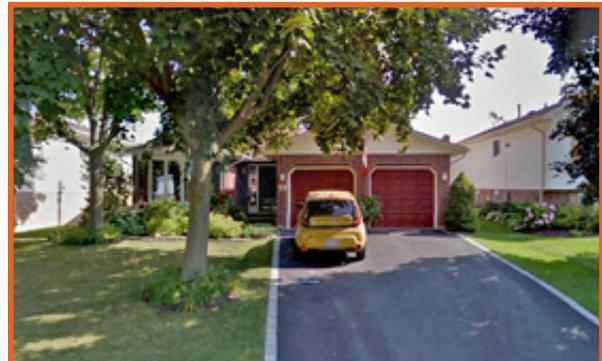
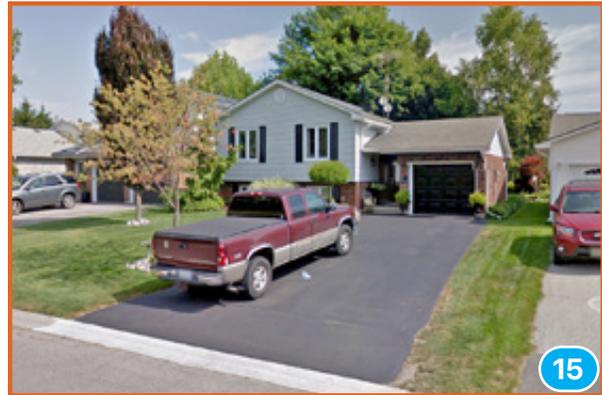
- 11 Locate driveways away from 'T' intersections and s, and on lots, encourage driveway / garage access from the side street.
- 12 Locate driveways away from parks and open spaces.
- 13 Pair driveways at a common property line, where possible, to allow for greater opportunities for landscaped/grassed areas along the streetscape and front yards, and allow sufficient space for on-street parking.
- 14 Provide a minimum 6m separation between driveways where driveways are not paired to allow for street tree planting and on-street parking.

**15** Minimize the visual impact / presence of front integrated garages on the streetscape:

- a. Recess the front wall of the garage from the main building wall;
- b. Limit front integrated garages to 2 cars and ensure that the overall width of the garage doors do not exceed 50% of overall width of the house;
- c. Ensure the driveway width at the street is not greater than the width of the garage, to a maximum of 6.0m;
- d. Provide glazed door panels on all garage doors;
- e. Second storey portions of the house above the garage should not be stepped back from the front wall of the garage more than 1.5m.

**16** For townhouses, provide a walkway from the front entrance of dwellings to the sidewalk.

**17** For townhouses, design walkways to be shared between adjacent townhouse units through the use of a common landing between units, leading to a singular walkway.



**Port Dover • Garage width**

15





## Garbage / Utilities

**18** Minimize the visual impact of garbage and utility areas on the street by planning for integrated garbage and utility solutions, for example:

- a.** deeper garages;
- b.** side yard garbage bin storage screened by a gate or enclosure;
- c.** consolidated garbage facilities for townhouse blocks screened with architectural and landscape elements;
- d.** locating utility metres, vents, HVAC equipment, etc. in the interior side or rear yard, integrating them with the design of the building, or screening them with walls and landscaping.



## Fencing

**19** Provide high quality wood privacy fencing on all lots where the rear yard or private amenity space is exposed to the street / public space:

- a.** Include an access gate;
- b.** Locate the fence entirely within private property.

**20** Provide wood privacy fencing for along the rear lot line of lots backing onto non-residential uses, and locate the fence within the non-residential use side.

## Building Design

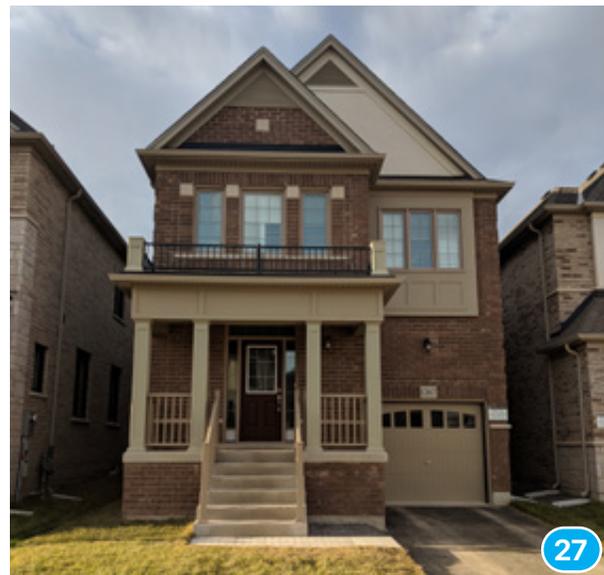
### Massing & Elevation Articulation

- 21** Incorporate a variety of architectural expressions including designs, models and elevations along the streetscape, including contemporary designs that may be located alongside 'traditional' forms, subject to design, massing and context.
- 22** Discourage designs that incorporate different/disparate architectural expressions and stylistic elements within a single dwelling or building.
- 23** Ensure facade details throughout all building elevations are consistent with their intended architectural style.
- 24** Discourage design elements that are superficial, decorative, or ornamental add-ons that are intended to evoke an architectural style that is not reflected in the principal building design and massing, for example, faux Victorian scrollwork on an otherwise contemporary design.
- 25** Take design cues from local architectural expressions.



### Entry Elements

- 26** Orient front entries to the street, or any adjacent publicly accessible open space that has pedestrian activity such as a walkway or park.
- 27** Use entry elements such as porches, arches, generous overhangs and massing elements such as a cantilevered upper storeys or recesses, to articulate front elevations at entrances.
- 28** Ensure steps are designed as an integral components of the elevation.
- 29** Ensure porches are sized to create usable space.
- 30** Provide municipal street numbers (address) that are visible and legible from the street.



Port Dover • Entry emphasis

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## Windows

- 31 Maintain consistent window treatment throughout individual buildings.
- 32 Locate windows to maximize daylight and reduce need for indoor lighting.

## Roofs

- 33 Encourage a variety of roof forms such as cottage or hipped roof, front gabled, side gabled, cross gabled, mansard and flat roofs; ensure roof forms are consistent with the architectural style of the dwelling.



Port Dover • Window consistency

- 34 For townhouse blocks, emphasize individual units through the articulation of roof lines (e.g. variations in roof slopes at end units, dormers, differing roof pitches, etc.) while maintaining a consistent roof style throughout the same block.

- 35 Avoid decorative dormers that do not have habitable space within them.

- 36 Locate stacks, gas flues and vents on the rear slope of the roof where possible.

- 37 Encourage skylights and solar panels, where present, to be designed as integrated components of the building or roof and away from public view.



Port Dover • Roof line variety

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## Materials

**38** Ensure materials reflect and complement the architectural style of the building.

**39** For traditional styles, provide a variety of high quality and complementary wall cladding materials including brick masonry, stone, stucco, and cementitious siding.

**40** For contemporary styles include materials such as brick masonry with smooth finishes, high quality stone cut to larger calibre units, corrugated steel panelling, marble, metal, concrete and metal roofing.

**41** On exposed elevations, ensure material changes occur at a change in massing or has a substantial material return.

**42** Ensure colours are consistent and coordinated throughout a building design and match the architectural style.





Example of priority lot locations.

**C** - lots in prominent locations

**OS** - parks/open space lots

**T** - T lots at the end of a view corridor

**E** - elbow lots

## Priority Lots

Priority Lots are located in prominent locations and/or are highly visible from the public realm, including:

- Lots in prominent locations;
- Parks / Open Space Lots;
- Window Street Lots;
- 'T' and 'Elbow' Lots; and
- End Units (in the case of townhouse blocks).

**43** Provide highly articulated elevations that include changes of plane, substantial window openings and upgraded architectural detailing and materials.

**44** Include gables, dormers and/or bay windows, and decorative panels/louvres, where appropriate.

**45** Provide wrap around windows, porches and other architectural treatments at conditions. Locate active living spaces at the /exterior side elevation.

**46** Design treatment of the exterior side elevation should be equal to that of the front elevation.

**47** Use stone or other upgraded materials as the main building material for gateway dwellings.

**48** Provide landscaping and upgraded lot fencing.

**49** Locate driveways / garages away from the terminus view; pair the side yards of visible lots.

**50** Consider flankage entrances on lots.

## 5 Commercial & Institutional Uses

### 5.1 General Guidelines for All Commercial and Institutional Sites

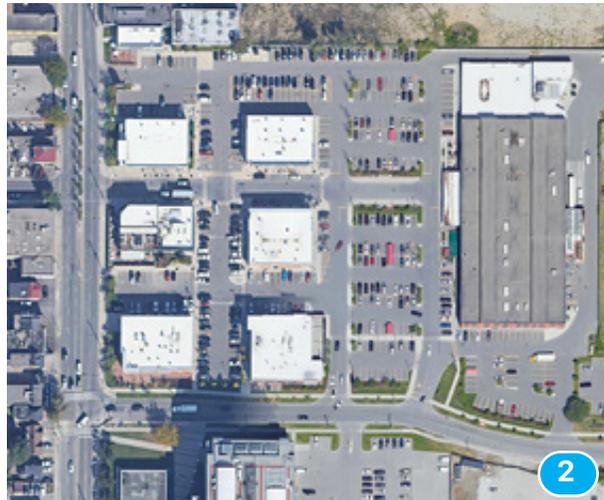
The following design guidelines apply to buildings that have non-residential uses in them. They aim to create vibrant and street focused built form that is compatible with its surroundings and coordinated, pedestrian-scaled public spaces that contribute to place-making.

#### Site Organization

- 1 Provide for appropriate transitions to adjacent neighbourhoods and different land uses (i.e. setbacks, landscaping, location of servicing and parking areas).
- 2 Create a pedestrian-scaled, permeable and connected internal layout (block and street pattern) and arrange buildings to create comfortable and protected pedestrian spaces that have a sense of enclosure. A sense of enclosure is created when both sides of a street (or the edges of a public space) are well-defined by building fronts.
- 3 Integrate existing topography and natural features into the development, and minimize alteration to both, wherever feasible.
- 4 Provide a connected street and open space network, including a pedestrian/cyclist system that encourages active transportation.
- 5 Organize the site to enhance wayfinding including buildings as gateways and landmarks, public spaces as focal points, streetscapes that frame significant views.

#### Placement & Orientation

- 6 Locate buildings along primary streets, at or near the street line.
- 7 Avoid large areas of surface parking between the main building wall and the street; limit front yard setbacks to 6.0m.





- 8 At corners, locate the longer building wall parallel to the primary street frontage.
- 9 Orient buildings to face the public realm, in particular any adjacent / adjoining streetscape, pedestrian connection and open space.
- 10 Arrange buildings to frame views / vistas, parks and open spaces.
- 11 Arrange buildings to allow for patios and spill out areas which animate the site/street.
- 12 Locate active uses at the base of buildings and on all elevations fronting onto public spaces (i.e. streets and open spaces).

### Access & Parking

- 13 Provide a safe, clear and accessible site circulation system for pedestrians, cyclists and vehicles, including visible access points and connections to the surrounding street network, public sidewalks, and parking areas.
- 14 Minimize interruptions to the sidewalk and potential conflict between vehicles, cyclists and pedestrians; provide barrier free, landscaped pedestrian connections from, to and through parking areas.
- 15 Provide prominent and easily accessible entry points to the site.
- 16 Provide direct access to at-grade uses from sidewalks and parking areas.
- 17 Locate parking areas away from the street frontage, preferably where they are screened from view by the building massing, or use landscaping and other structures to screen them.
- 18 Ensure CPTED principles are applied to parking areas including clear views within the parking areas, multiple points of pedestrian and/or vehicular entry, well-defined pedestrian routes, and the ability to be overlooked by adjacent buildings.

**19** Design surface parking to minimize environmental impact by reducing parking lot/garage size, considering shared parking facilities with adjacent buildings and providing preferential parking for fuel efficient vehicles.



**20** Avoid large areas of surface parking:

- a. Disperse surface parking throughout the site;
- b. Utilize low impact design principles, permeable paving materials, and reduce heat island effect through light materials or canopy coverage;
- c. Incorporate landscaping within parking areas (aim for 20 to 30 percent of the parking area).

**21** Parking structures should be integrated with and/or behind principal buildings.

**22** Parking structures along street/public frontages should be lined with active uses at grade and include enhanced articulated elevations.



**23** Provide accessible and secure bike racks and parking at retail, commercial, and employment area developments, as well as at key nodal locations to promote purposeful cycling.

### Servicing & Loading

**24** Locate garbage/recycling, loading and service areas to the rear or side yards, away and fully screened from public view, abutting residential areas, major roads and open spaces.



**25** Integrate garbage/recycling functions within buildings wherever possible.

**26** Incorporate garbage storage bins that can be accessed for garbage pick up into the principal building design; ensure food waste is stored in climate controlled rooms.



**27** Provide on-site recycling facilities for handling, storing, and separation of recyclables for large developments, such as multi-unit residential buildings, employment and office buildings, and institutional or public buildings.

**28** Loading/garage doors should not face the public street / space.

### Landscaping

**29** Provide an enhanced public realm interface along the street, including landscaped areas, open space, gathering areas in association with front door(s) and/or walkways.

**30** Design landscaping within the private areas and public interface to be coordinated and to enhance the character of the development and the neighbourhood.

**31** Design landscaping to reinforce the structure of the site with a focus on creating a safe, comfortable and animated pedestrian environment – including streets, edges, gateways, transitions, public spaces, and building entrances.

**32** Design landscaping to enhance and contribute to the broader environment including ecological function, stormwater management functions, urban forest, and bio-diversity.

**33** A comprehensive strategy for planting, built features, fencing, walls, paving, lighting signage and site furnishings shall be provided.

**34** Distinguish walkways from driveways through a material change and/or planted/sodded edge.

**35** Use high-quality, durable materials for paving, walls, planters, site furniture, shade structures, etc.

**36** Design fences and walls to be coordinated with building designs.

**37** Use berms in landscape strips to minimize views/noise from adjacent uses, parking, loading and service areas.

## Building Design

### Massing & Elevation Articulation

**38** Ensure massing and design is compatible with and transitions to the surrounding neighbourhood character.

**39** Encourage multi-storey building designs wherever feasible and appropriate.

**40** Incorporate prominent building massing and special architectural elements at intersections, s and gateways.

**41** Encourage a range of design expressions to promote architectural variety.

**42** Provide a high-degree of articulation on building elevations that face onto streets and public spaces, through design elements such as changes in plane, fenestration, projections, relief, horizontal and vertical elements.

**43** Establish a rhythm of minor breaks or wall articulation along the facade, distinguishing one unit (retail) or building component from the next. When selecting the rhythm, scale and proportion, take cues from adjacent buildings.

**44** For buildings located at s, design building elevations to equally address the two main street frontages; prominent massing, height, architectural elements and detailing should be used to emphasize these locations.

**45** For sites adjacent to highways, provide the same degree of building articulation on all exposed elevations, in addition to the primary elevation.





- 46** Avoid blank, uninterrupted walls and false frontages along streets and open spaces.
- 47** Coordinate the design of ancillary buildings and structures with that of the principal building(s); height, massing, architectural details, lighting, signage, materials, and colours.
- 48** Provide main building entrances in prominent and highly visible locations, and oriented to primary streets.
- 49** Ensure building entrances are accessible, safely and clearly connected to the sidewalk and parking areas.
- 50** Concentrate the highest degree of articulation at entrances and along main building elevations.
- 51** Ensure elevations along streets include a significant amount of glazing.
- 52** Provide weather protection elements at entrances and along highly pedestrian edges.
- 53** Incorporate architectural elements to enhance the pedestrian environment – canopies, overhangs, awnings, projecting display windows, arcades, colonnades, etc.; these elements should be designed as integral parts of the building in terms of form, style, materials, colours, etc.
- 54** Screen roof top mechanical equipment from view through the use of architectural screens, parapet walls and/or integration into the design of the building.

## **Materials & Lighting**

- 55** Coordinate building materials among buildings on a site and ensure they reflect, complement and enhance the building's architectural style and use.

**56** Use high-quality, durable exterior building materials; avoid reflective and mirrored spandrel glass.

**57** Provide a high level of visual transparency (glass) at eye level for lobbies, main frontages and prominent entrances.

**58** Provide an overall lighting strategy that coordinates site, building and landscape lighting to ensure pedestrian safety and comfort.

**59** Lighting design should minimize light spill over into residential neighbourhoods.

**60** Consider lighting powered by alternate energy sources such as solar power.

## Signage

**61** Provide an overall signage strategy that coordinates the site and buildings within a multi-tenant site.

**62** Integrate signage to the building design and ensure it complements the building's elevation, animate the ground level and enhance the streetscape.

**63** Signage should be consistent in design with respect to materials, size, location on a building, lettering and lighting, while also allowing some flexibility for tenant branding.

**64** Avoid neon signs, rooftop signs and visual clutter.

**65** Limit the number of monument /pylon signs on a site and coordinate their design with that of the buildings.

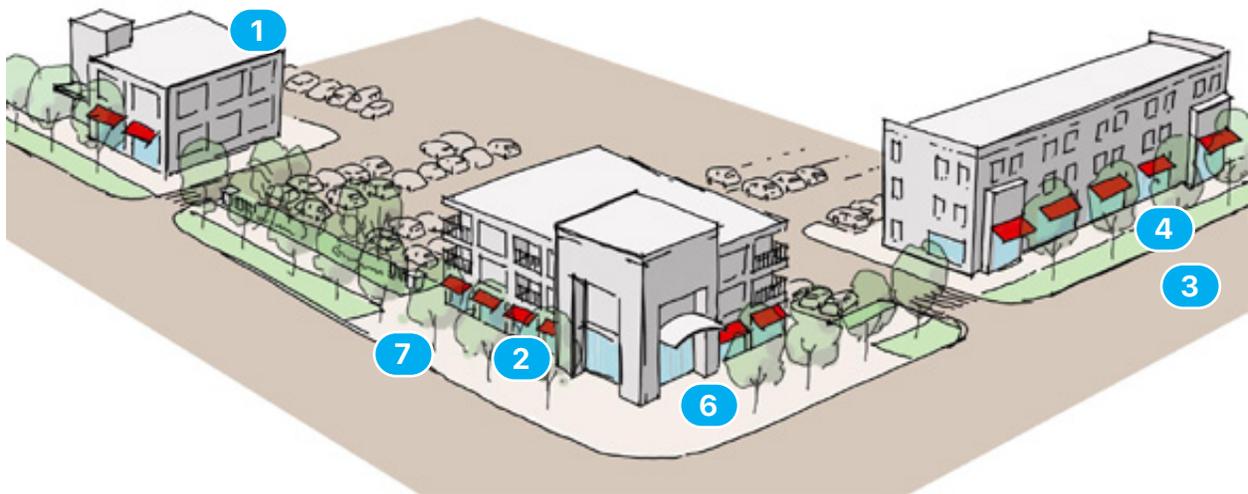
**66** Monument/pylon signs should be coordinated in design with the buildings.



## 5.2 Mixed Use

This section applies to new mixed use development outside of the downtown. This can include the redevelopment and intensification of commercial sites, and the provision of new neighbourhood focal areas in greenfield development.

- 1 Provide active uses at ground level such as retail, commercial, restaurant, professional and medical services, or office along a minimum of 75% of the built frontage of buildings facing the site's primary street. Generally, the only other ground level use along primary streets should be entrances to the other, above-grade uses in the building.
- 2 Establish direct building frontage along a minimum of 50% of a site's adjacent right(s) of way.
- 3 The primary building facade and main front entrances shall orient to and be accessed from a public street sidewalk.
- 4 Where there is more than one adjacent street, orient primary building facades to the street with higher pedestrian potential.
- 5 For corner buildings, locate main entrances at or near the corner.
- 6 Sidewalks should be wider and provide opportunity for patios and retail display.
- 7 Site organization and building design should follow the Non-residential design guidelines. Parking should generally be located at the rear of buildings.



## 5.3 Retail

This section applies to sites where one (or more) commercial/retail users exceeds 3,000 square metres of building area for that individual user/premises.

- 1 The largest buildings should be located at the rear of sites.
- 2 Locate smaller buildings along street edges and at s, and ensure they address streets with a high quality building expression including windows.
- 3 Seek to establish direct building frontage along a minimum of 35% of a site's adjacent right(s) of way. Where this cannot be achieved, provide an enhanced pedestrian-oriented landscape treatment along the street edges including:
  - a. continuous sidewalks;
  - b. direct connections between sidewalks and internal walkways that lead to building entrances;
  - c. double row of street trees;
  - d. minimum 3.0 metre wide landscape zone along the street with low plantings;
  - e. hard landscape elements that define gateways and enhance pedestrian comfort such as seating, low walls, pergolas, columns and gateway features.
- 4 Site organization and building design should follow the General Guidelines for All Sites in this section.



## 5.4 Drive-through Facilities

- 1 Within larger developments, locate drive-through facilities at mid-block locations with queueing and drive-through lanes at the side or rear yards.
- 2 For sites that contain two or more drive-through facilities, ensure clear separation of their respective driveways and queue lanes.
- 3 Provide separate entrances/exits for drive-through facilities and the site.
- 4 Locate queue lanes (and intercom stations) away from residential areas and outdoor amenity areas.
- 5 Where possible, consider double drive-through lanes that merge into a single queue lane for pick-up.
- 6 Avoid locating queueing and drive-through lanes between the street and the building; for exceptions where this condition occurs provide a minimum 4.5m separation between the street and the drive-through / queue lanes that is landscaped, including plantings, fences and walls to screen these areas from public view.
- 7 Provide queue lanes to accommodate the following minimum number of vehicles:
  - a. 10 vehicle spaces for restaurants;
  - b. 8 vehicle spaces for financial institutions;
  - c. 3 vehicle spaces for other uses, such as pharmacies.
- 8 Provide a 2.0m minimum separation between queue lanes and parking areas, with the use of raised medians, planting, fences and walls.



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- 9 Avoid pedestrian routes that cross driveways and queue lanes; if they must cross these areas, they should be located to minimize potential conflict, and should be designed to prioritize pedestrians, through the use of clear pavement markings, special pavement, signage and other cues to ensure safety.
- 10 Separate payment and pick-up windows where possible.
- 11 Block spill over of vehicle headlights onto adjacent residential properties, public streets and public spaces.
- 12 Provide weather protection for payment / pick-up windows.

## 5.5 Institutional/Community Use Sites and Buildings

These buildings / sites have a focal role within the community and should demonstrate the highest level of design considerations and use of quality material.



- 1 Buildings should be sited prominently to anchor / gateway locations, and/or terminate vistas.
- 2 Principal buildings should be sited close to the primary street with building presence along at least 60% of the street frontage.
- 3 Massing and articulation of buildings should be considered in the context of creating 'landmarks' within the community.
- 4 Locate vehicle drop off and parking areas away from the street frontage, preferably at the sides of the principal building.
- 5 Where drop off and parking areas are located along the street, they should be designed as part of an enhanced public realm (continuous paving, rolled/flush curbs, street furniture, seating, and planting).

## 6 Gateways

### 6.1 Introduction

The Gateways are located at the boundary along the most important approach roads to Port Dover. They are points of transition from the agricultural surroundings to the urban area, and are important in forming the first impression of Port Dover. Development within gateway areas should establish a high quality image for the community, provide a sense of arrival, and reinforce the community's character. For commercial or institutional uses to be located in a Gateway, refer to guidelines for non-residential uses included in Chapter 6.

### 6.2 Guidelines for Gateways

- 1 One or more buildings located adjacent to the principal arrival road should be designed as landmarks with unique architectural treatments. The landmark building(s) should frame the arrival to Port Dover by being located close to the road right of way.
- 2 Building massing and design in a Gateway visible from the principal arrival road should reinforce the importance of the area through taller buildings, higher roofs, and/or prominent visual features such as a taller massing element withing the building design, gables, dormers, significant front entrances, use of transparency, and upgraded material use.
- 3 Buildings should address the principal arrival road with high quality 'frontal' appearance inclusive of windows, entrances, and sidewalk connections (if a sidewalk is present along the principal arrival road).
- 4 Rear yards with long privacy fencing is not appropriate at gateway locations.
- 5 Generous landscaping along the principal arrival road should highlight local natural ecologies through choice of plant material and design layout.
- 6 Planting should provide views to landmark building(s), screen parking areas, and create



a 'green corridor' along the principal arrival road.

- 7 A vertical element in the landscape design is encouraged, to act as a focal point and landmark, with sufficient scale to be seen from a distance.
- 8 High quality signs for municipal identity, retail/commercial uses and wayfinding can also contribute to a sense of gateway. They should be designed to be compatible with the built form and landscape character, and of a scale to be seen and read in the landscape.
- 9 Consider traffic calming elements within the right of way such as unit paving, raised crosswalks, table top intersections, narrowed vehicular lanes, and on-street parking.

## 7 Parks & Stormwater Management

### 7.1 Parks

#### Powell Park

Powell Park is the focal public space Downtown. It is a unique park in Port Dover with an important cultural heritage story. It has a variety of amenities, including seating, picnic tables, a gazebo, walking paths, public art, monuments, children's play, waste receptacles and lighting. The current character and function of the park should be maintained.

- 1 Maintain open park frontage on all four sides with direct sidewalk and crosswalk connections to all surrounding sidewalks.
- 2 Enhance the children's play function. This could include dedicated play equipment, or playful elements that serve dual purposes.
- 3 Consider special treatment, temporary or permanent, for Market Street East and Park Street, that visually extend the park environment and allow the parking and/or street areas to be used for park events.
- 4 Expand opportunities to reference Port Dover's history.
- 5 Consider adding more opportunities for accessible activities, picnicking and seating over time.



#### Silver Lake Park

Silver Lake Park is owned by the Port Dover Lions Club. Purchased in 1992, the park is the site of a soccer field, home to the Silver Lake Market and the location for many activities, celebrations and festivals. Rowing, dragon boat racing, canoeing and kayaking occur in the south end of the park, organized by various groups. There has been interest in repairing Misner Dam and revitalizing Silver Lake since 2010 with goals to:

- Revitalize the Provincially Significant Wetlands in the north end;

- Create a smaller Silver Lake in the south end suitable for water based recreation;
- Create a series of silt traps in the Silver Lake Basin with truck access to protect the river below Misner Dam, and the harbour; and
- Provide public access to both areas for recreation and environmental purposes.

## Vista Parks

Vista Parks are intended to be located at the interface between Port Dover's community fabric and the natural heritage system or water's edge. They are often located at the end of a road as it approaches the lake, one of the river valleys, a woodlot, or other natural feature. Vista Parks provide a window and access to the natural feature and/or water's edge. Vista Parks may be trail access locations, a small park, visual amenities managed as ecological amenities, or a combination. They are important in defining the unique sense of place for Port Dover and complementing the parks and active transportation network.



- 6 Locate Vista Parks to enhance connectivity and clearly define access and views to the natural heritage system, river valleys and Lake Erie.



- 7 Design Vista Parks as natural parks, organizing its elements to harmonize with the surrounding landscape and incorporate the use of natural materials.

- 8 Incorporate low maintenance, native plantings and ensure appropriate landscaped transition to buffer enhancement areas and natural heritage features.



- 9 Where Vista Parks are of sufficient size, consider additional park elements such as children's play, seating, shade, gateway features, wayfinding signage, waste and recycling receptacles, lighting and gathering spots, as appropriate.

*A lake front Vista Park with trail connection, seating, an overlook area, and wayfinding signage.*

- 10 Vista Parks along Lake Erie should include a sign identifying clear public access and an accessible hard surface and seating area for enjoyment of the lake views, at a minimum.

- 11 Buildings adjacent to Vista Parks should treat the elevation facing the park as a frontal elevation, and should consider an entrance facing the park.

## Neighbourhood Parks in Greenfield Neighbourhoods

New neighbourhood parks are multi-purpose community open space amenities located at or near the centre of neighbourhoods. They act as community focal points, and can be associated with a school, natural feature, or commercial or higher density residential uses.

- 12 Locate parks centrally within neighbourhoods, ideally within a 400 to 800m radius (5 to 10 minute walk) to most residences on the neighbourhood.
- 13 Connect parks to the larger open space system through public streets, trails, and natural features.
- 14 Locate and design parks to support, complement, and buffer the natural heritage system.
- 15 Provide significant publicly accessible frontage adjacent to parks, in the form of a public road, a school, or the natural heritage system.
- 16 Ensure neighbourhood parks have significant frontage on adjacent streets to promote views and reinforce their focal nature. Encourage street frontages on 3 sides (preferable configuration), and provide a minimum of 2 sides fronting onto streets.
- 17 Locate neighbourhood parks adjacent to school sites, where appropriate, to allow for shared amenities, such as parking lots and recreational play fields.
- 18 Ensure the neighbourhood park public street frontage is between 50 to 80m when bordering a school or residential area.





- 19 Consider community gardens in neighbourhood parks to further encourage social interaction and to provide access to locally grown produce.
- 20 Ensure the park system includes a variety of elements ranging in size, scale and function, both passive and active recreation.
- 21 Provide a range of physical activity spaces for children and adults to promote physical activity in different age groups.
- 22 Maintain existing, healthy trees and other vegetation on site.
- 23 Incorporate new trees and landscaping within parks to contribute to the urban tree canopy and buffer natural areas.
- 24 Ensure bicycle and pedestrian routes to parks are accessible, safe, and visible.
- 25 Incorporate Crime Prevention through Environmental Design (CPTED) principles into the design of parks to ensure clear views into and out of surrounding areas, including:
  - a. Adequate lighting;
  - b. Front buildings overlooking public spaces, especially playgrounds which should be highly visible to public streets and/or houses to enhance safety;
  - c. Proper signs and design for ease of access and egress; and,
  - d. Mix of activity to encourage constant use of the space.
- 26 Provide lighting to be Dark Sky/Nighttime Friendly compliant. Where feasible, incorporate LED or solar powered lighting.
- 27 Direct lighting for sports fields away from the natural heritage features and design lighting to minimize disturbance to adjacent properties.
- 28 Consider opportunities for renewable energy use such as solar powered lighting for

natural trails, park pathways and other public spaces to reduce electric energy supply in the public realm.

- 29 Provide wayfinding signage that has a high level of clarity, visibility, and visual interest; is made of high quality materials; and aids pedestrians, cyclists and drivers in navigating the area, especially at night.

## Parkettes

Parkettes are small parks that complement neighbourhood parks. They provide small scale outdoor amenities such as seating, children's play and small grassed flex spaces.

- 30 Locate parkettes within a 3 to 5 minute walk of most residents (200m radius) and include passive recreational features for the immediate neighbourhood.
- 31 Locate parkettes to achieve significant public exposure and access. Urban design options include surrounding the park with streets, or dwellings fronting directly onto the parkette to create visually attractive 'edges' to these spaces and ensure there are clear sight lines from surrounding buildings to the public space.
- 32 Ensure the parkette design complements and enhances the surrounding public realm by integrating the landscape treatment (built form features, site furniture and landscape elements) within adjacent streetscapes and public space areas.
- 33 Consider designs that complements / harmonize with adjacent greenlands / natural heritage areas; use natural, sustainable materials.



Port Dover • Edward Lewis Parkette

## 7.2 Stormwater Management Facilities



Stormwater management facilities (SWM) are part of the Port Dover's infrastructure, and perform the required function of collecting and treating runoff and controlling flood potential in neighbourhoods. These facilities also form part of the Natural Heritage and Park network, providing opportunities for passive recreation and nature interpretation.

- 1 Provide walking trails, seating nodes and low-maintenance naturalized plantings on table land areas of the SWM Block.
- 2 Combine trails with maintenance paths where possible.
- 3 Connect walking trails to the broader trails network.
- 4 Design stormwater management facilities using ecological principles that emphasize naturalization of aquatic and terrestrial planting and grading systems to enhance the ecological function, reduce maintenance cost, and beautify the facility.



# 8 Active Transportation

## 8.1 Introduction

Trails for pedestrians and cyclists enhance the connectivity of the community, provide low-impact transportation choices, and promote active, healthy lifestyles. Port Dover seeks to provide a linked active transportation network throughout the community, complemented by, and interlinked with, the road network.

There has been much interest in extending the Lynn Valley Trail along the south edge of Silver Lake, along the river edge to the harbour pier. The Conceptual Active Transportation Network locates a water's edge trail.

This chapter includes general guidelines for Active Transportation based on the work of the County's Integrated Sustainable Master Plan.

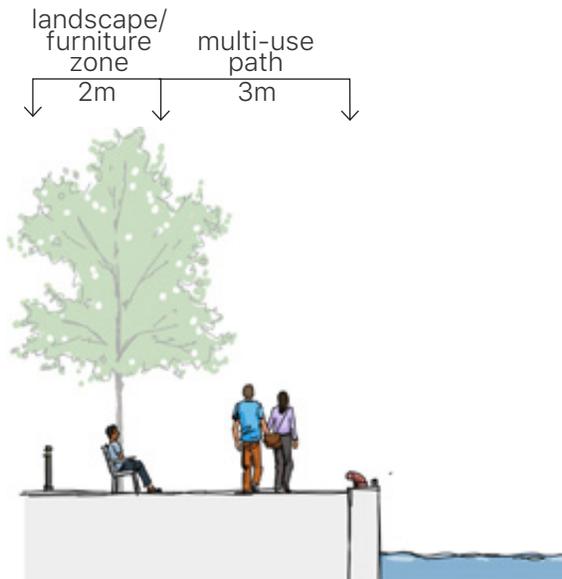
## 8.2 Guidelines for Active Transportation

- 1 Identify public land that can be used for access and/or recreation and enhancing its accessibility through signage and capital improvements. An example could include providing seating and a walking path along the Lake Erie shoreline where a municipal right of way extends to the water.
- 2 Provide new public space or trails where development occurs. An example could include creating a walkway along the Lynn River.
- 3 Manage vegetation along the water's edge to open views.
- 4 Provide benches along the pedestrian trail.
- 5 Support cycling by providing more infrastructure such as bike lock-up areas, bike repair areas, and covered bike parking.

## Trails

- 6 Encourage active transportation and physical activity through the provision of a linked system of walking and cycling trails that provide residents with access and mobility options to local destinations.
- 7 Expand the network of trails throughout Port Dover where feasible.
- 8 Design trails to accommodate a range of users and abilities and be barrier-free, where appropriate.
- 9 Provide trail entrances at the intersections of trails with the street R.O.W and coordinate their design with that of the adjacent streetscapes and open spaces.
- 10 Provide benches and waste and recycling receptacles at trail heads and at regular intervals along the route.
- 11 Consider special treatments at trail head entrances including high quality features such as landscaping, decorative paving pattern, interpretive or directional signage, or wider pathway widths.
- 12 Ensure trails are minimum 2.5m wide, and in areas of high pedestrian traffic, wider where possible.
- 13 Ensure pedestrian trails located in environmentally sensitive areas consist of low impact materials such as natural earth, woodchips, or mown strips.
- 14 Provide lighting for pedestrian safety along busy urban trails, but minimize the disturbance on natural heritage habitats.
- 15 Provide wayfinding signage and/or trail markers throughout the trail network and clearly sign trails regarding permitted uses and speed.





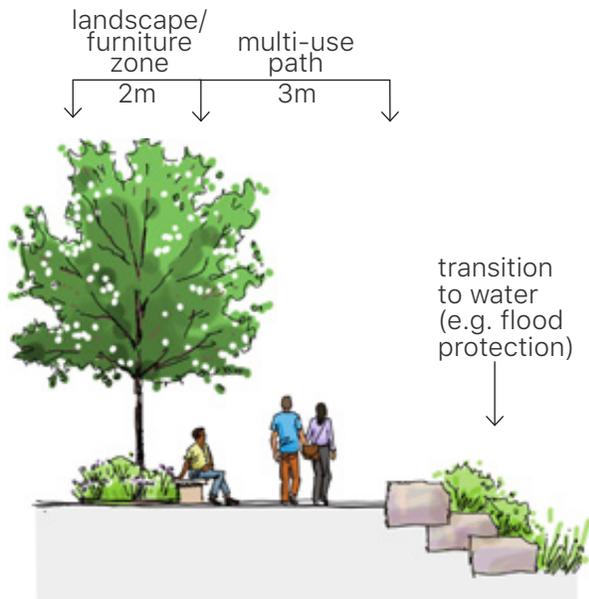
5m wide public realm at water's edge – seawall condition

- 16 Use native, non-invasive species that can contribute to the urban tree canopy along trails abutting natural features and coordinate planting design to shade trails.

### Water's Edge Trail

- 17 The water's edge trail should have a minimum width of 3m wherever possible.

- 18 Provide a minimum 2m landscape/furniture zone in association with the trail. The landscape/furniture zone can accommodate planting, trees, seating, lighting, bike racks, signage, and trail connections. Additional land required for grading or hazard lands should not be part of the landscape zone.



5m wide public realm at water's edge – landscape condition



# 9 Streets

## 9.1 Introduction

Streets are the largest component of public space in communities and as such play a critical role in establishing a beautiful and high quality image in Port Dover. The street network includes the Provincial highway, Primary streets comprised of existing and potential arterial and collector streets, local streets (including Green/Vista Streets) and special streets (Main Street, other commercial streets and Walker Street).

## 9.2 General Guidelines for Streets

### Street Network

- 1 The functional hierarchy of the street network should be enhanced by streetscape design; a variety of different streetscape character types should be provided within new neighbourhoods.
- 2 Incorporate traffic calming measures such as on-street parking, reduced lane widths, public laneways, raised intersections, and/or traffic circles to reduce vehicular traffic speeds and to ensure safe walking and cycling environments.

### Walkability

- 1 Provide continuous sidewalks, or equivalent provisions for walking, on both sides of the road.
- 2 Provide large canopy deciduous trees on both sides of the street.
- 3 Explore opportunities to create more space between the building face and the roadway to accommodate pedestrians areas, trees, landscaping and spill-out space for businesses.
- 4 Add traffic calming elements such as enhanced crosswalks or mid-block pedestrian crossings.



- 5 Building entrances, sidewalks and crosswalks should be barrier-free and accessible.
- 6 All pedestrian clearways should be designed to a minimum width of 2.0m, with additional space in commercial areas for street furniture, trees, and spill-out space for businesses.
- 7 Use distinctive feature paving, alternative pavement markings or materials to minimize the conflict between vehicles and pedestrians and to enhance pedestrian crossings visibility and quality. At minimum, provide crossings identified with distinctive painted lines.



## Street Furniture

- 8 Streetscape elements, such as street furniture, refuse and recycling containers, newspaper boxes, should be coordinated, clustered and not impede paths of travel.
- 9 Provide a coordinated and consistent family of street furnishings within distinct areas of Port Dover.
- 10 Coordinate above- and below-ground utilities to avoid visual clutter in the streetscape and to minimize conflict with street trees. The provision of street trees should take precedence.

## Street Landscaping

- 11 Existing street trees and planting beds should be retained and maintained where possible/appropriate.
- 12 Street trees should be provided to develop, over time, a strong mature canopy and create a healthy and attractive streetscape environment.
- 13 Hanging baskets, seasonal planters and other landscape features which add warmth and visual interest to the streetscape should be installed and maintained in key areas.

- 14** Trees and landscaping should not obscure views and sight lines.
- 15** Provide planting materials to add visual interest all year.
- 16** Introduce green infrastructure, such as bioswales, within the public right-of-way to enhance ground water infiltration and improve water quality as part of a comprehensive water management plan.

## Lighting

- 17** Decorative and pedestrian-scaled lighting should be used to enhance the streetscape experience, to animate streetscapes at night, and for safety and pedestrian comfort.
- 18** Existing lighting elements should be maintained, and as development occurs, extended to new block frontages.
- 19** Spotlighting and decorative lighting should be used to highlight landscape and architectural features, landmark buildings and signage.
- 20** Lighting should be designed using energy efficient sources and to avoid light pollution, spillover and glare.

## Signage and Wayfinding

- 21** Coordinated directional signage should be provided to improve accessibility and wayfinding for residents and visitors.
- 22** Signage and wayfinding should be designed to be similar to the overall theme of the streetscape and architectural character of buildings, and should be unique to Port Dover.
- 23** Signage and wayfinding should be designed for clarity and visibility (not blocked by vegetation) and where possible, information should be consolidated on one panel or post.

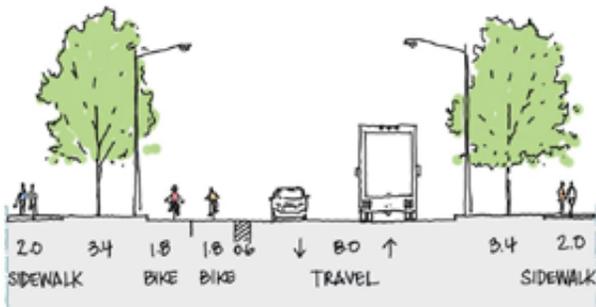
- 24 Signage placement should anticipate snow pile up and be located to be legible where possible throughout the year.

## 9.3 Street Typologies

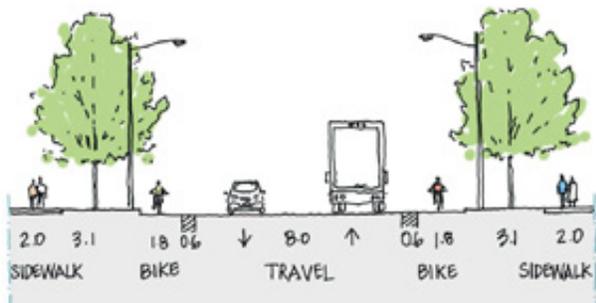
### Primary Streets (Arterials and Collectors)

Primary Streets connect the neighbourhoods of Port Dover and its surrounding context. They should be planned to accommodate all modes of travel. New Primary Streets in new neighbourhoods should be planned as integral components of the bike network; many new Primary Streets will be suitable as cycling routes. The selection of uni- or bi-directional facilities will depend on the context of the street.

- 1 Provide sidewalks on both sides of the street.
- 2 Provide a softscape boulevard with large canopy street trees on both sides of the street.
- 3 Provide cycling lanes as appropriate.



Primary Street with bi-directional bike lanes and buffer on one side of the street. Dimensions are to be confirmed through appropriate engineering analysis.



Primary Street with uni-directional bike lanes and buffer on both sides of the street. Dimensions are to be confirmed through appropriate engineering analysis.

## Special Streets

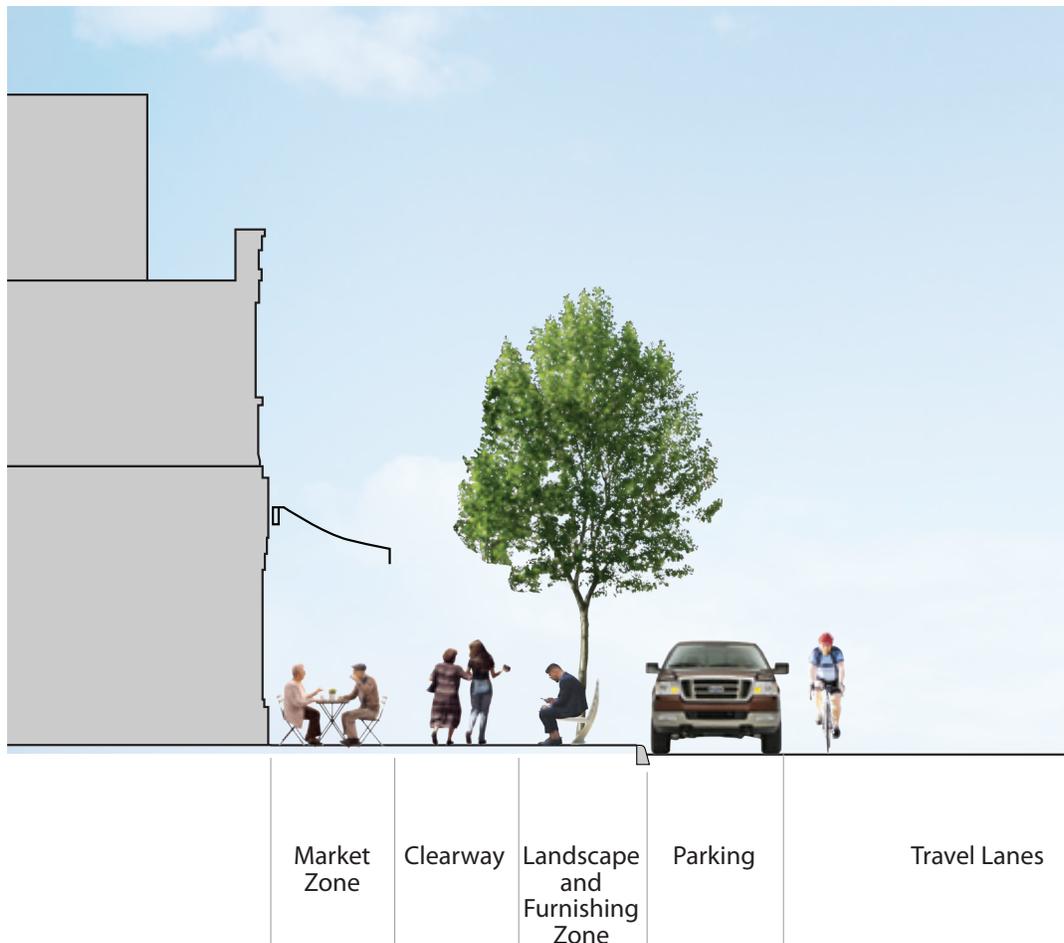
### Commerical Streets

Streets in Port Dover that support commercial-retail activity, such as Main Street, side streets in Downtown, and potential future mixed use nodes, should be designed to support pedestrian activity as a priority.

- 1 Provide a 2.0m minimum pedestrian clearway on all sidewalks.
- 2 Include a 'Market Zone' along commercial-retail frontages, either within the right of way or within the setback. The Market Zone is a paved extension of the sidewalk for commercial spill out such as patios, signs and merchandise display.
- 3 Designate a zone adjacent to the vehicular travel way for landscaping and furnishing, inclusive of lighting, seating, trees and signage.

### Business Spill-Out Spaces

- 4 Outdoor areas with spill-out space from businesses, such as sidewalk cafés and patios, are encouraged as they help to animate the street.
- 5 Spill-out spaces should be located along the street sidewalk edge or within the spaces between buildings. These spaces should relate to the street and be open to public view.
- 6 Sidewalk cafés and patios should be designed and located so as not to impede pedestrian movement.
- 7 Decorative fencing and patio furniture should be used to add interest in the streetscape and complement the design of the building.



## Main Street

Main Street is the traditional heart of downtown and the focus of commercial activity and street life. It must accommodate and balance a lot of infrastructure and activities within its right of way, from walking to driving to parking, and signs, lighting, furniture, planting and other street elements. Increasing the flexibility of Main Street to be used in different ways can enhance its role and function as the downtown's focal point.

While Main Street is classified as an Arterial Road, it must be designed to support its role as the heart of downtown, and not as a standard arterial road.

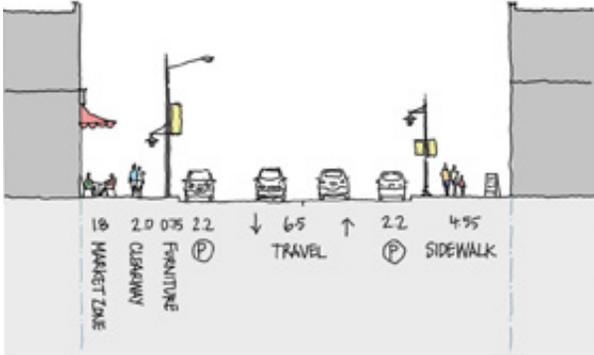
If Main Street is selected as an active transportation route with cycling (sharrows), it is recommended that on-street parking be removed on one side, in order to maintain sufficient sidewalk space as the premier pedestrian-oriented space in Port Dover.

- 1 Provide sidewalks on both sides of the street, paved from curb edge to building face, that accommodate:
  - a. a minimum 2.0m pedestrian clearway;
  - b. a market zone adjacent to the building face that can host sidewalk cafes or other retail spill-out;
  - c. a curbside furniture zone for lighting, signs, seating and other amenities.
- 2 Where there is additional space within the right of way, or the adjacent building is set back, use the space as an extension of the sidewalk.
- 3 Consider zones of on-street parallel parking that can be re-purposed for commercial or public uses, either seasonally or for events.

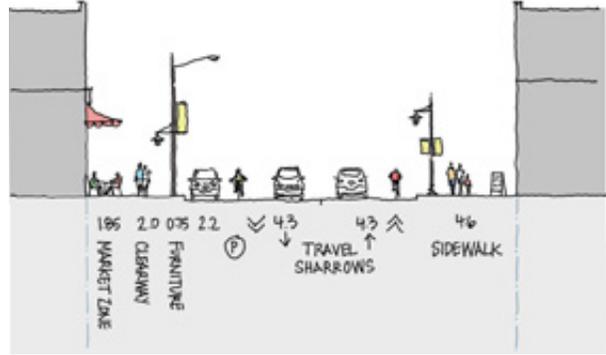


*Examples of a permanent flexible parking lane that can be re-purposed seasonally using movable bollards. The flex space is used for parking in some seasons, usually winter, and for patios and public seating in others.*

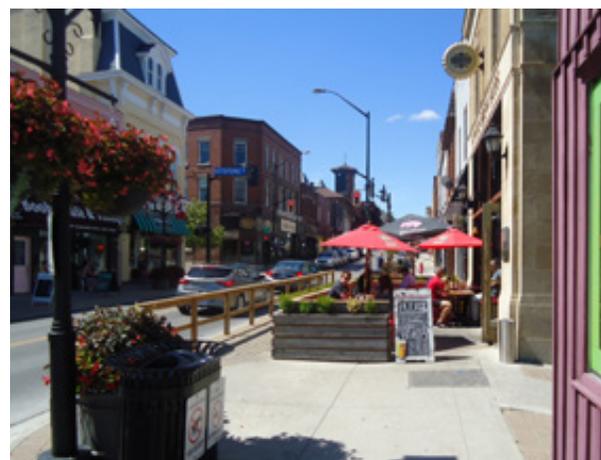
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Main Street (and other commercial streets) without bike sharrows. Dimensions are to be confirmed through appropriate engineering analysis.



Main Street (and other commercial streets) with bike sharrows. Dimensions are to be confirmed through appropriate engineering analysis.



Examples of temporary sidewalk diversion to accommodate a patio along the building edge. The temporary sidewalk is located within the on-street parking lane, and raised to curb height for better accessibility.



Examples of temporary patios and parklettes located within the on-street parking lane.

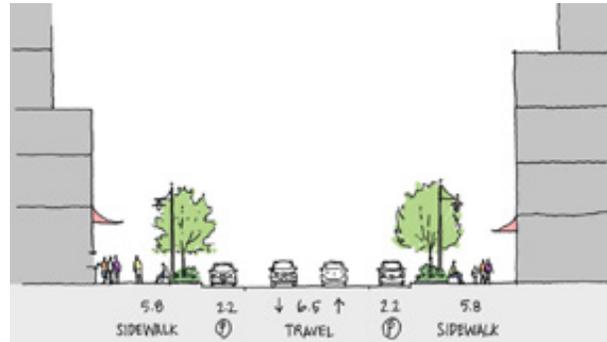
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## Walker Street

Walker Street is an important character-defining street in Port Dover, providing commercial activity and access to the beach. New development has the potential to intensify the street as a pedestrian destination and transform the right of way. The right-angled parking that has existing on Walker Street is not conducive to a pedestrian environment.

While part of Walker Street is classified as a Collector Road, it must be designed to support its role as the character-defining street of the waterfront area, and not as a standard collector road.

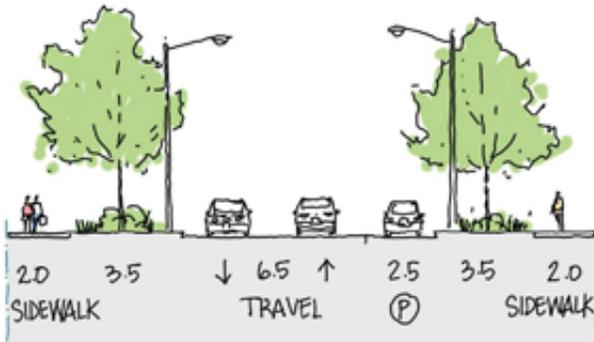
- 1 Provide wide sidewalks along both sides of Walker Street through reductions in the vehicular space.
- 2 Sidewalks should be paved from curb edge to building face.
- 3 Include generous landscaping and seating along the length of Walker Street.



*Walker Street can have generous sidewalks by providing parallel parking on both sides of the street. Dimensions are to be confirmed through appropriate engineering analysis.*



*Walker Street today*



*Dimensions are to be confirmed through appropriate engineering analysis.*

## Local Streets

Local Streets will comprise the majority of the streets in new neighbourhoods. They should be planned with a greater emphasis on the pedestrian environment.

- 1 Provide sidewalks on both sides of the street.
- 2 Provide a softscape boulevard with large canopy street trees on both sides of the street. Consider Low Impact Development measures with the boulevards such as stormwater management and enhanced planting.

## **Green/Vista Street**

- 1** Provide 'Green / Vista Streets' - These streets should be oriented to visually connect new neighbourhoods to the surrounding natural context and rural landscapes. They should be designed as pedestrian oriented streets that connect parks and open spaces to one another and to the natural heritage system.
- 2** Green / Vista Streets should consider incorporating wider boulevards to allow for wider sidewalks, a double row of street trees, bio-retention swales, and naturalized planting instead of sod.
- 3** Green / Vista Streets should include upgraded front elevations for all dwellings on either side of the street.
- 4** Green / Vista Streets should incorporate visually impactful street trees distinct in their size, form and fall leaf colour.

## 10 Green Infrastructure and Building

While sustainability is an overarching objective throughout the Guidelines, this section provides guidance on green infrastructure and building practices and helps achieve the broad sustainability principles of the Secondary Plan.

As part of the strategy to achieve a high level of sustainability in regards to the reduction of energy, water and waste, the Green Infrastructure and Building Guidelines apply to both the private and public realm.



### 10.1 Energy Conservation

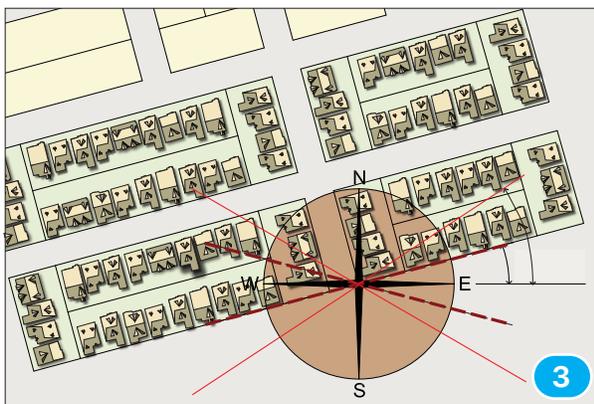
Provide for the reduction of energy use and consider the inclusion of alternative energy sources.

1 Where feasible, provide alternative community energy systems such as district energy, geo-exchange, sewer heat recovery, and/or inter-seasonal thermal energy.

2 Consider reducing demand for energy from the grid and encourage renewable energy production. Renewable energy sources that could be employed may include the use of solar thermal and photo voltaic equipment, and/or wind power. Proposed alternative energy source(s) could be used in combination with energy from the grid.



3 Encourage passive solar orientation to permit enhanced energy efficiencies by creating optimum conditions for the use of passive and active solar strategies. The integration of passive building systems is enhanced with buildings oriented to maximize the potential for sunlight and natural ventilation.



4 Where feasible, implement street and block alignment within 25 degrees of geographic east-west to maximize passive solar orientation of buildings front and rear windows.

5 Consider constructing all low and medium density residential buildings to be Solar Ready, built with all the necessary piping and equipment that would be needed to install a rooftop solar power system.

- 6 Consider the purchase of energy from renewable resources available from local utility/energy providers.
- 7 Reduce heat absorption through the use of cool roofs that are designed to reflect more sunlight and absorb less heat than a standard roof. Cool roofs can be made of a highly reflective type of paint, a sheet covering, or highly reflective tiles or shingles. Consider cool roofing material with a minimum initial solar reflectance of 0.65 and minimum thermal emittance of 0.90.
- 8 For a low sloped roof, typical of commercial and institutional buildings, the cool roof Solar Reflectance Index (SRI) value should be 0.64 and for steep sloped roofs, typical of residential, the SRI value should be 15.
- 9 Mitigate heat island impacts through the use of paving material with high solar reflectance, strategic use of deciduous trees or preserve existing trees as part of a free cooling strategy to help with evapotranspiration and shading of sidewalks and hard surface areas in summer and solar access in winter.
- 10 Charging stations that would supply electricity for electric vehicles are encouraged in Draft Plans/Site Plans. Charging stations could be provided in parking areas of mixed-uses, institutional uses, or within parking garage structures.
- 11 Grade related residential unit driveways are encouraged to be paved with light-coloured material to reduce the heat island effect.



## 10.2 Water Use and Management

The benefits of high performance, compact, mixed use projects include reduction in household water consumption and water utility costs, as well as the protection of the natural water supply. Compact development reduces impervious surfaces and makes it easier to protect natural areas which are the most important steps a community can take to maintain water quality.



- 1** Encourage the implementation of Low Impact Design Standards that emphasize the use of bio-swales, innovative stormwater practices, constructed wetlands, at-source infiltration, greywater re-use system, and alternative filtration systems such as treatment trains.
- 2** Implement a comprehensive rainwater and water recharge strategy in conjunction with required stormwater management facilities.
- 3** Implement strategies for stormwater retention and run-off such as:
  - a.** Retain stormwater on-site through rainwater harvesting, on-site infiltration, and evapotranspiration;
  - b.** Consider the inclusion of third pipe greywater systems and rain water harvesting for watering lawns and gardening, to reduce demand on potable water use;
  - c.** Direct flow to landscaped areas and minimize the use of hard surfaces in order to reduce the volume of run-off into the storm drainage system;
  - d.** Store snow piles away from drainage courses, storm drain inlets, and planted areas; and,
  - e.** Use infiltration trenches, dry swales and naturalized bioswales adjacent to parking areas to improve on-site infiltration.

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- 4 Introduce green infrastructure, such as bioswales, within the public right-of-way to enhance ground water infiltration and improve water quality as part of a comprehensive water management plan.
- 5 Encourage the use of porous or permeable pavement instead of standard asphalt and concrete for surfacing sidewalks, driveways, parking areas, and many types of road surfaces as a stormwater run-off management strategy.
- 6 Implement a rainwater harvesting program to provide the passive irrigation of public and/or private greenspace, including absorbent landscaping, cisterns, rain barrels, underground storage tanks, infiltration trenches, etc.
- 7 Implement xeriscaping using native, drought-tolerant plants, a cost-effective landscape method to conserve water and other resources on a residential and community-wide level.
- 8 Where feasible, implement curb cuts along sidewalks and driveways to allow water to flow onto planted zones or infiltration basins.
- 9 Consider the installation of subsurface basins below parking lots to enable stormwater to be stored and absorbed slowly into surrounding soils.



## 10.3 Material Resources and Solid Waste

Assist in the reduction and diversion of waste from landfills and increase measures for recycling and reuse.

- 1 Consider the use of recycled/reclaimed materials for new infrastructure including roadways, parking lots, sidewalks, unit pavings, curbs, water retention tanks and vaults, stormwater management facilities, sanitary sewers, and/or water pipes.
- 2 Incorporate strategies that emphasize targets for a higher diversion rate in recycling for the plan area.
- 3 Reduce waste volumes through the provision of recycling/reuse stations, drop-off points for potentially hazardous waste, and centralized composting stations.
- 4 Consider incorporating existing heritage buildings in situ through retention, restoration, and adaptive reuse to avoid further construction waste.
- 5 In large buildings, such as multi-unit residential buildings and institutional or public buildings, provide on-site recycling facilities for handling, storing, and separation of recyclables.
- 6 Recycle and/or salvage at least 50% of nonhazardous construction and demolition debris and locate a designated area on site during construction for recyclable materials.

## 10.4 Air Quality

In order to minimize the air quality and climate change impacts associated with new growth, the following measures are encouraged.

- 1** Reduce the impact of air pollution by encouraging the development of 'complete' communities that are characterized by greater densities placed at neighbourhood centres, mixed use nodes, or near transit facilities; mixed land uses; mix and diversity of housing types; connected and walkable road patterns, and are designed to encourage active transportation.
- 2** Encourage and promote alternative modes of transportation such as public transit, walking, and cycling. Provide transit within a 200 to 400 metre (3 to 5 minute) walking distance of residential development.
- 3** Ensure the separation of sensitive land uses from air pollutant sources through land use planning and zoning. Refer to the Ministry of the Environment guidelines.
- 4** To promote transit ridership, programs such as developer-sponsored transit passes at reduced-costs for each residential unit or employee are encouraged.
- 5** Provide the minimum number of parking spaces to minimize the impact of car parking:
  - a.** Mixed use developments should include shared use of parking among uses that have different peaking characteristics;
  - b.** Design parking areas so they are not the primary visual component of a neighbourhood;
  - c.** Reduce the parking ratio required in areas that are served by transit; and,
  - d.** Dedicate priority parking spaces for carpool, ride sharing, and ultra low emission vehicles - 5% of total parking space.



## 10.5 Lighting



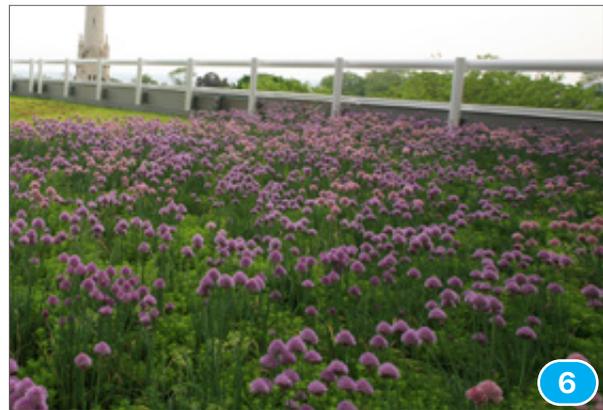
- 1 Promote Dark Sky/Nighttime Friendly compliant practices to minimize light pollution and the intrusion of unwanted lighting on natural areas.
- 2 Consider high efficiency street lighting to reduce energy use.
- 3 Consider opportunities for renewable energy use to reduce electric energy supply in the public realm, such as solar powered lighting for natural trails and park pathways.



## 10.6 Green Buildings/Green Sites

Promote innovative programs to encourage the design and construction of energy efficient green buildings and sites.

- 1 Consider third-party certification and rating programs, such as LEED® for New Development (ND).
- 2 Consider innovative residential development designs which contribute to affordability and energy and natural resource conservation.
- 3 Consider building(s) that are LEED® Certified or recognized or accredited by a third-party certification program i.e. Energy Star, LEED H, LEED NC, LEED for Schools, BREAM, etc.
- 4 Green roofs are encouraged for high-density residential, office buildings, as well as, public, institutional buildings to minimize surface runoff, reduce urban heat island effect, provide noise insulation, and improve local air quality.
- 5 Encourage synergies between buildings and site management practices that conserve water, reduce waste, and are energy efficient.
- 6 Provide green roofs for 80% of all high density development. In high-density residential buildings, design roofs as amenity areas.
- 7 Develop a heat island reduction strategy for community and public buildings to install green roofs with 50% coverage, remainder covered with light coloured material. Light coloured roofs have a high solar reflectance, which reduces energy costs and reduces urban heat island effect.



## 8 Promote Energy Efficiency:

- a.** Where feasible, provide alternative community energy systems such as geothermal, sewer heat recovery, or inter-seasonal thermal energy; and,
- b.** Development plans and building design shall provide opportunities for south facing windows and building orientation to maximize potential for passive and active solar energy.

## 9 Promote Water Efficiency:

- a.** All buildings comply with Ontario's Building Code required water fixtures efficiency;
- b.** Building uses Low Impact Development strategies to deal with on-site run-off and heat island effects;
- c.** Building's landscaping is water efficient and drought resistant by using native planting materials; and,
- d.** Pre-design for grey-water pipe infrastructure.

## 10 Promote Green Materials:

- a.** Incorporate waste reduction work plans and construction best practices that reduce construction waste;
- b.** Incorporate green building material standards to reduce impact on the environment and ensure materials are purchased/obtained from a responsible ethical sources; and,
- c.** Materials sourced from certified local businesses.

## 10.7 Stewardship and Education

- 1 Create a well-documented master plan including illustrations that promote sustainable aspects of the development.
- 2 Include environmental builder specifications in all subcontracts.
- 3 Produce detailed sales and promotion materials that feature conservation aspects of the development.
- 4 Develop subdivision covenants that establish ground rules for the maintenance of shared open lands and individual lots.
- 5 Create a Homebuyer's Environmental Instruction Guide that explains the unique environmental aspects of the subdivision/site and special maintenance considerations.
- 6 Include an owner/tenant education package at the time of purchase or rental regarding household activities to improve energy and water efficiency, access to transit, location of recycling station, etc. Coordinate with existing municipal and regional information.

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## **Appendix II: Cultural Heritage Resources**

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# Appendix II: Cultural Heritage Resources

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Legend  
■ Designated Property



# Appendix III: Urban Forestry Guidelines

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## The Urban Forest

Trees are an important part of our towns and cities, and we are only beginning to recognize the tremendous value of a healthy urban forest. Trees perform infrastructural roles including stormwater management through root uptake and respiration, urban heat island attenuation through shading, provision of habitat for avian and insect populations, and increase resiliency in the face of climate change. Trees beautify the environment, provide psychological benefits and promote physical health in people. The economic value of a tree increases exponentially with age, with a cumulative value of \$160,000+ per tree. Over 50 years one tree will produce \$31,250 of oxygen, \$37,500 of recycled water, and \$31,500 worth of erosion control (US Forest Service, ISA, 2013). A fundamental goal of these design guidelines is to promote an expanded urban forest that exhibits health and resiliency.

Careful attention to the growing requirements of trees is intended to ensure that tree life expectancy cycles are considered in increments of 50 to 100 years. That is, even the fastest growing and shortest lived trees will thrive for 50 years, and the slower growing and longer lived trees will have an opportunity to last a century.

There are many factors that contribute to successful trees, but foremost is their soil conditions: sufficient soil volume, quality soils that are not compacted, and access to air and water. The best place to grow trees are in protected natural heritage areas. In lower density neighbourhoods where space is plentiful, soil condition requirements are easier to meet. But in Port Dover's older, more urban areas, where space is more intensively used for roads, buildings and sidewalks, deliberate care must be taken to meet the soil needs of trees.

## Diversity

Biodiversity is founded on ecological principles with a host of benefits, but of particular importance is the establishment of resiliency. The International Society of Arboriculture's (ISA) best practices promote species selection based on the following guidelines:

- no more than 30% from the same family;
- no more than 20% from the same genus; and
- no more than 10% from the same species.

The purpose of this allocation is to prevent the same kind of monoculture that devastated the trees of our towns and cities including the American Chestnut, American Elm, and Ash trees. Many species today are under threat from pests and diseases, and diversity will help to ensure the urban forest is more resilient and remains green when pests and diseases inevitably strike.

It is important to look forward towards issues which may impact trees which are currently keystone species in the landscape, such as oak and maple. These species may be subject to pests and pathogens in the near future such as Oak Wilt, Gypsy Moth, and Asian Longhorn Beetle. These species should still be planted, but must not be over planted. Monoculture plantings of a single species should not be used so future pathogens do not cause widespread mortality.

Promoting Carolinian species is a unique way that Port Dover can increase urban forest diversity and resiliency against climate change and northward moving pathogens.

While the ISA percentages are a goal of the urban forest system, it will be difficult to achieve in the Downtown and Urban Waterfront, where there are fewer trees to choose from of sufficient hardiness and adaptation to urban conditions.

## Tree Preservation

Existing trees are the greatest asset of the urban forest, and new developments in Port Dover should attempt to preserve healthy mature trees where they are not located within proposed building envelopes. Opportunities to preserve trees should be looked at early in the approvals process to increase the likelihood of success. When existing trees must be removed, replacement tree plantings should strive to provide equivalent benefits to the community. The Council of Tree & Landscape Appraisers provides a guide to plant appraisal which can be a useful tool for determining the value of existing trees when preservation or replacement plantings are not possible.

Existing tree preservation should prioritize large mature shade trees, trees which may have heritage value, native trees, and trees which can be managed as a group. In particular, the preservation of trees located at the perimeter of lots can often provide buffering between properties.

Tree preservation planning should consider the extent of the tree above and below ground, and the understanding that development can impact the roots of trees on adjacent properties. The critical root zone (CRZ) can be understood as a measured circle around a living tree that represents the minimum rooting area that is essential for its structural integrity and capability to remain alive and upright. The CRZ should be determined in consultation with an International Society of Arborists (ISA) certified arborist or other qualified person. However, a tree protection zone (TPZ) can be modelled as a function of the trunk diameter at breast height (DBH) and used as a guide for protection trees during development. The ISA recommends a standard TPZ of 30cm of offset per 2.54cm of diameter.

## Monitoring

It is recommended that Carolinian species are incorporated into the plantings of parks and natural areas, and monitored to collect data on their performance. Producing an urban forest inventory is one way to understand the

strengths and vulnerabilities of the local tree canopy, but will require an investment of staff and other resources.

## Planting Considerations

### Soil

The biomass of urban trees that we see above grade, which includes the trunk, branches and leaf volume, is approximately equal to the biomass below ground, which includes the root network. Therefore, soil volume for canopy trees should be 30 cubic metres per tree, or 20 cubic metres per tree if soil volumes are shared among more than one tree. Soil volume should be within 1.4 metres of the surface. Volumes less than 14 cubic metres will not support a long-term functional tree (James Urban, Making Space for Roots: Tree Planting Options in Urban Areas, 2009. <http://www.isa-arbor.com/myaccount/myeducation/resources/ceu-april10.pdf>).

Tree roots are opportunistic, in that they will fill the available space. This means that there is a lot of flexibility in the shape of the soil area, such as, long linear boulevard spaces between the sidewalk and a street, or even in soil cells underneath sidewalks.

It is recognized that 30m<sup>3</sup> of soil volume will not be achievable in all locations. 20m<sup>3</sup> of soil volume will still support a large tree, though at a lesser growth potential. At reduced soil volumes, different types of trees need to be considered. There are very few instances where trees are not possible.

### Planting Conditions

Open lawn areas will comprise the majority of planting conditions along streets and parks in Port Dover. Generally, there will be no issue in achieving the required soil volumes that promote healthy tree growth. However, in measuring available soil volume, only contiguous areas of lawn or soil should be counted. Sidewalks, walkways and other infrastructure, because of the compaction of the sub-grade granular underneath them, usually create barriers for tree roots.

In the Downtown and Urban Waterfront there will typically be less space available for trees. In very constrained areas, structural soil cells can achieve required soil volumes by allowing uncompacted soils to extend under the paved surfaces, areas that would otherwise be unavailable to tree roots. These engineered techniques transfer the weight of the sidewalk, street furniture and vehicles to the ground underneath a tree's required uncompacted soil volume. Structural soil cells have significant benefits both for the long term health of the urban forest, and for increasing growth rate and reducing 1-5 year stunting common to urban locations.

Open planting beds are appropriate for areas with less pedestrian traffic. This provides space for multiple trees to be co-located within a consolidated soil volume. Open planting beds with flush curb edges allow storm water to irrigate the soil area.

Irrigation systems are not generally recommended, however, it is critical to the long term health of trees to provide weekly watering during the first two years following transplant when feeder root establishment occurs. This should be accompanied by appropriate fertilization.

Soil pH levels are important for nutrient uptake and appropriate levels vary depending on species (Soil Management for Urban Trees - Best Management Practices, 2014). Soil Organic Matter content should be 3-10% (Soil Management for Urban Trees - Best Management Practices, 2014). Soil can be amended at time of planting with mycorrhizal inoculant.

Roots require air and therefore soil must be well drained. Soil should drain within 24 hours of rainfall events. Appropriate mitigation is required if soil does not drain. It is too late to do this after construction is complete.

## Planting Details

Temporary tree guards can be used after tree transplant to protect trees from snow removal operations, bikes, and vandalism. Tree guards should be removed after approximately 5 years or when the tree reaches 30cm diameter at breast height. Tree guards should not touch the tree or constrain growth in any fashion. Tree grates and paving details must allow adequate space for future trunk flare. Trees should be planted with the trunk flare above finished grade (top of rootball should be planted 2 to 4 cm above grade to allow for settlement), and ensure the trunk is plumb. Planting Typology

When the canopies of mature trees interconnect overhead, the value of the urban forest in performing infrastructural benefits is significantly enhanced. Tree spacing is recommended at 10 to 15 metres on centre, or closer, depending on species growth rate expectations.

## Species Selection

Tree species selection should be performance based, with the right tree matched to the right location. Trees provide the most benefits to the community when they can grow for many years and attain their natural mature size. The selection of trees for individual sites must give careful consideration to the existing conditions and potential barriers to their success, including soil type, moisture, available growing space above and below ground, and proximity to sidewalks, roads and utilities. Some planting sites, like urban boulevards surrounded by paving, are extremely harsh, and only suitable for a short list of resilient tree species, while planting sites in parks or garden beds may support many more tree species. As not all trees are suitable for all locations, tree species should be selected which possess the characteristics that most closely meet the environmental conditions of each site, with the gradient of diversity increasing as the quality of the planting site increases.

Port Dover's climate is among the most temperate in Ontario, and its location within the Canadian Carolinian Zone creates the opportunity to support an urban forest with more diversity than most other locations in Ontario. The Canadian Carolinian Zone is a unique and fragile ecosystem, with the warmest average annual temperatures, the longest frost-free seasons, and the mildest winters in Ontario. Its boundary is based on the northern limit of key species which are typically only found in more southern regions, and the term 'Carolinian' refers to its similarity to the forests found in the Carolinas in the southern United States.

This unique climate means that Port Dover could have success using tree species which have little documented use in other municipalities, but are suited to the local climate.

### Trees with Different Growth Rates

Growth rate stratification is based on the fact that different species grow at different rates and have different life expectancies. Generally, fast growing species are shorter lived than slow growing species. By mixing both types throughout the urban area, it ensures that a green canopy is achieved as quickly as possible, and that it remains green over time, because when the fast growing species reach the end of their lives, the slower growing species will have reached their mature size.

### Natural Areas

In naturalized areas, particularly in proximity to shorelines and forested areas, native species should be planted. Invasive species which have negative impacts do the most harm when they can escape cultivation into the habitat of native species which they are able to outcompete. In particular, the species of trees for planting near riparian areas should be carefully considered.

Native species often do better in undisturbed soil, and tend to be less tolerant of the imported and compacted soil that is often found in streetscapes and new subdivisions. Many native

tree species which do not have suitable qualities for parks or streets, but are valuable habitat or food sources for pollinators and wildlife can be planted in or adjacent to naturalized areas. For example, some native species produce an abundance of fruit that can stain pavements at certain times of the year. These species should still be planted, as they are important to wildlife lifecycles, but they should be sited within an awareness of their characteristics.

Pin cherry is a good example of this type of native tree; the fruit is valuable to birds, and the bright red berries and spring flowers are attractive; however, it is a short-lived and weak wooded tree with is prone to black knot fungus. It is best planted in locations where it can freely spread by suckers and naturalize into colonies.

Refer to local conservation authorities for more extensive species planting list.

### Waterfront Areas

Plantings in waterfront areas should take into account local water tables and floodplains. Many tree species which are urban and drought tolerant are intolerant of standing water. Some species which are tolerant of flooding, like silver maple, river birch, poplars and willows, have other characteristics which can be problematic, like weak wood or poor disease resistance. These species should only be used if they can be located in areas where their vulnerability to storm damage will not cause safety hazards.

Some desirable native trees like red maple, hackberry, swamp white oak and black gum can do well in wet conditions, but adaptation to flooding will depend on the source of seed stock, so this requirement should be communicated with the supplier, and locally adapted specimens grown in flood prone habitats should be sourced.

Some non-native trees, like Dutch-Elm Disease resistant hybrid elms, and hybrid soft maples are appropriate for waterfront areas, but care should be taken to avoid using any species with invasive qualities near riparian areas.

## Roadside Plantings

Rural and peri-urban roadside plantings are typically subject to stress such as road salt spray, wind, compacted soil and drought. Native species from the urban boulevards list are appropriate for roadside plantings, however, if plantings can be set back further from the road the planting palette can be expanded to include native species with a lower salt and pollution tolerance from the general list, such as sugar maples.

## Species Lists

The following tables provide an overview of suggested planting for Port Dover's natural areas, parks and streets. It should be considered as a starting point, to be refined through observation and local knowledge of tree health and performance in Port Dover's context.

The trees are organized in three groups:

### **Urban Boulevards, Constrained Conditions**

- these trees are suitable for planting on sites with a high degree of impermeable surfaces and/or low opportunities for planting areas. These areas can include downtown's commercial streets, like Main Street, where the right of way is almost continuously paved. Constrained soil volumes, compaction from vehicles and pedestrian traffic, road salt and other stressors create difficult growing conditions. Species must be tolerant of these stressors. By necessity, there are a higher number of non-native trees in this group in order to provide the diversity and performance needed to achieve an urban canopy.

**Sub-urban Boulevards, Parks** - these areas typically have significantly improved growing conditions and as a result a much greater variety of trees that can thrive. Trees designated for Urban Boulevards should be assumed to be suitable for Softscape Boulevards and Parks. Trees designated for Softscape Boulevards should be assumed to be suitable for Park as well, but not Urban Boulevards. Trees designated Parks should be assumed to be sensitive, and should be considered on a species by species basis for

open lawn, and planting beds. Only native trees are suitable for naturalized areas, and the local conservation area can provide a more extensive list of acceptable species.

**Trees to be Avoided** - these trees are known to be problems, because they are invasive (they take over natural areas and compromise their ecological function), they are weak wooded and pose safety hazards in our urban areas, or they harbour invasive pests; sometimes all three.

## Urban Boulevards, Constrained Conditions

Tree Species	Native Range	Comments and Notes	Size
<b><i>Acer ginnala</i></b> Amur Maple	Non-Native	Compact form/red & yellow fall colour/ lots of seeds/tends to sucker/specify single stem form	Small
<b><i>Acer griseum</i></b> Paperbark Maple	Non-Native	Compact form, exfoliating bark. Prefers moist soil.	Medium
<b><i>Acer x freemanii</i></b> Hybrid Soft Maple	Native to Ontario	Tolerant of urban conditions. Caution: Many cultivars of <i>Acer rubrum</i> and <i>A. saccharinum</i> exist under the name <i>Freemanii</i> , each with different characteristics	Large
<b><i>Acer myabei</i></b> Myabei Maple	Non-Native	Compact form	Medium
<b><i>Acer tataricum</i></b> Tatarian Maple	Non-Native	Compact form/red & yellow fall colour. Specify single stem form.	Small
<b><i>Amelanchier x grandiflora</i></b> Serviceberry (specific cultivars) 'Robin Hill' 'Autumn Brilliance'	Native to Ontario (cultivar)	Tolerant of urban stress, flowering. Specify single stem form.	Medium
<b><i>Amelanchier laevis</i></b> Serviceberry (specific cultivars) 'Spring Flurry' 'Cumulus'	Native to Ontario (cultivar)	Tolerant of urban stress, flowering. Specify single stem form.	Medium
<b><i>Carpinus caroliniana</i></b> Blue beech or Musclewood	Native to Ontario	Difficult to transplant. Keep away from road salt & spray, likes wet soil thin bark and sculptured trunk	Medium
<b><i>Celtis occidentalis</i></b> Common Hackberry	Native to Ontario	Tolerant of urban conditions, and poor drainage. Requires pruning for general form.	Large
<b><i>Cockspur hawthorn</i></b> <i>Crataegus crus-galli</i>	Native to Ontario	Compact, white flowers in spring. Tolerant of urban conditions	Small
<b><i>Cornus alternifolia</i></b> Alternate-leaf Dogwood	Native to Ontario	Use local winter hardy material only. Specify single stem	Medium
<b><i>Gymnocladus dioicus</i></b> Kentucky Coffeetree	Native to Ontario	Tolerant of urban conditions. Male trees drop large seed pods.	Large

## Urban Boulevards, Constrained Conditions

Tree Species	Native Range	Comments and Notes	Size
<b><i>Ginkgo biloba</i></b> Ginkgo Tree	Non-Native	Tolerant of urban conditions. Plant male cultivars to avoid fruit litter. This species is very slow growing, and takes a long time to provide canopy cover. However, it can be useful in constrained conditions.	Large
<b><i>Gleditsia triacanthos var. inermis</i></b> Thornless Honey Locust 'Shademaster' 'Skyline'	Native to North America (cultivar)	Tolerant of urban conditions. Provides a filtered shade/susceptible to defoliation by leafhopper/susceptible to canker and other pests and diseases. Prone to over-use.	Large
<b><i>Liquidambar styraciflua</i></b> Sweetgum	Native to Eastern North America	Carolinian species, uncommon as street tree in Ontario.	Large
<b><i>Nyssa sylvatica</i></b> Black Gum	Native to Ontario	Carolinian species, tolerant of urban conditions, but uncommon as a street tree in Ontario. Tolerates poor drainage.	Large
<b><i>Quercus bicolor</i></b> Swamp White Oak	Native to Ontario	Grows in wetter conditions than most oak species. Tolerates urban conditions, but best located away from potential exposure to salt spray (eg. Wide boulevards)	Large
<b><i>Quercus muehlenbergii</i></b> Chinquapin Oak	Native to Ontario	Attractive tree, especially in old age. Limited planting, due to possibility of Oak Wilt entering Ontario	Medium
<b><i>Quercus robur</i></b> English Oak	Non-Native	Needs well drained soil/difficult to transplant/large size at maturity	Large
<b><i>Sophora japonica</i></b> Japanese Pagoda Tree	Non-Native	Showy flowers, produces seed pods, tolerant of urban conditions, but uncommon as a street tree in Ontario.	Medium
<b><i>Syringa reticulata</i></b> Japanese Tree Lilac (Ivory Silk)	Non-Native	Good white summer flower excellent small specimen Prone to over-use	Small
<b><i>Tilia cordata</i></b> Littleleaf Linden 'Glenleven' 'Greenspire'	Non-Native	Aphid & borer problems; suckers from base; messy species	Medium

## Urban Boulevards, Constrained Conditions

Tree Species	Native Range	Comments and Notes	Size
<b><i>Tilia Americana</i></b> Basswood	Native to Ontario	Prefers deep moist fertile soil will grow on drier heavier soil needs large space	Large
<b><i>Ulmus japonica x Ulmus wilsoniana</i></b> Accolade Elm Princeton Morton Morton Glossy Frontier	Non-Native, Hybrid species	Tolerant of urban conditions and moist soil. These cultivars are currently shown to have good resistance to Dutch Elm Disease, but this is something in flux and should be constantly reassessed.	Large
<b><i>Ulmus americana</i></b> (DED resistant cultivars) Elm 'Homestead' 'Pioneer' 'Jefferson'	Native to North America (cultivar)	Tolerant of urban conditions and moist soil. These cultivars are currently shown to have good resistance to Dutch Elm Disease, but this is in flux and should be constantly reassessed.	Large

## Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<b><i>Abies balsamea</i></b> Balsam Fir	Native to Ontario	Park	Dense, conical form. Prefers moist, cool locations	Large
<b><i>Acer griseum</i></b> Paperbark Maple	Non-Native	Park	Compact form, exfoliating bark. Prefers moist soil.	Medium
<b><i>Acer nigrum</i></b> Black Maple	Native to Ontario	Softscape Boulevard	Lots of seed for winter interest/rare/needs moist soil	Large
<b><i>Acer pseudoplatanus</i></b> Sycamore Maple	Non-Native	Softscape Boulevard	Very pollution and salt tolerant. Cankers can cause high maintenance	Large
<b><i>Acer rubrum</i></b> Red Maple 'October Glory' 'Red Sunset'	Native to Ontario	Softscape Boulevard	Green summer foliage & yellow to red fall colour. Tolerates wet soil/ poor drainage.	Large
<b><i>Acer saccharinum</i></b> Silver Maple	Native to Ontario	Softscape Boulevard	Fast growing softwood maple; Maintenance issues as tree nears maturity due to weak wood. Tolerates wet soil/ poor drainage.	Large
<b><i>Acer saccharum</i></b> Sugar Maple	Native to Ontario	Softscape Boulevard	Upright form/fall colour varies/prefers good drainage/shallow roots/salt sensitive	Large
<b><i>Acer x freemanii</i></b> Hybrid Soft Maple	Native to Ontario	Hardscape Boulevard	Caution: Many cultivars of <i>Acer rubrum</i> and <i>A. saccharinum</i> exist under the name <i>Freemanii</i> , each with different characteristics	Large
<b><i>Aesculus glabra</i></b> Ohio Buckeye	Native to Ontario	Park	Showy flowers. More resistant to anthracnose than other chestnut species.	Medium
<b><i>Aesculus hippocastanum</i></b> Horsechestnut 'Baumannii'	Non-Native	Park	Good spring flower with no fruit/limit use due to disease susceptibility	Large
<b><i>Aesculus flava</i></b> Yellow Buckeye	Native to North America	Park	Showy flowers. More resistant to anthracnose than other chestnut species.	Medium
<b><i>Amelanchier arborea</i></b> Downy Serviceberry	Native to Ontario	Naturalizing	Showy flower & fruit/ tolerant of wet & dry soil	Medium

## Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<b><i>Amelanchier laevis</i></b> Smooth Serviceberry	Native to Ontario	Softscape Boulevard	Multi-stem specimens by prior approval only	Small
<b><i>Amelanchier canadensis</i></b> Shadblow Serviceberry	Native to Ontario	Softscape Boulevard	Difficult to maintain single stem. Four-season interest. Tolerates moist soil	Medium
<b><i>Asimina triloba</i></b> Pawpaw	Native to Ontario	Park	Large fruit has food value to humans	Large
<b><i>Betula alleghaniensis</i></b> Yellow Birch	Native to Ontario	Park	Interesting bark features and good fall colour	Large
<b><i>Betula papyrifera</i></b> White Birch	Native to Ontario	Park	Interesting bark features and good fall colour. Susceptible to Bronze Birch Borer, use in limited circumstances.	Large
<b><i>Betula populifolia</i></b> Grey Birch	Native to Ontario	Park	Ornamental bark. Susceptible to Bronze Birch Borer, use in limited circumstances.	Large
<b><i>Betula nigra</i></b> River Birch	Native to North America	Park	Ornamental bark. Susceptible to Bronze Birch Borer, use in limited circumstances. Tolerant of poor drainage.	Large
<b><i>Carpinus caroliniana</i></b> Blue beech or Musclemwood	Native to Ontario	Softscape Boulevard	Difficult to transplant. Keep away from road salt & spray, likes wet soil thin bark and sculptured trunk	Medium
<b><i>Carpinus betulus</i></b> European Hornbeam	Non-Native	Park	Difficult to transplant. Keep away from road salt & spray	Large
<b><i>Catalpa speciosa</i></b> Northern Catalpa	Native to North America	Park	Pods can be messy	Large
<b><i>Carya cordiformis</i></b> Bitternut Hickory	Native to Ontario	Park	Difficult to transplant due to large tap root. Messy fruit	Large
<b><i>Carya glabra</i></b> Pignut Hickory	Native to Ontario	Park	Difficult to transplant due to large tap root. Messy fruit	Large
<b><i>Carya laciniosa</i></b> Shellbark Hickory	Native to Ontario	Park	Difficult to transplant due to large tap root. Messy fruit. Sensitive	Large

## Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<b><i>Carya ovata</i></b> Shagbark Hickory	Native to Ontario	Park	Difficult to transplant due to large tap root. Messy fruit	Large
<b><i>Carya tomentosa</i></b> Mockernut Hickory	Native to North America	Park	Difficult to transplant due to large tap root. Messy fruit. Can tolerate dry sites but prefers deep fertile soil.	Large
<b><i>Celtis occidentalis</i></b> Common Hackberry	Native to Ontario	Hardscape Boulevard	Requires pruning for general form. Very hardy. Tolerant of poor drainage.	Large
<b><i>Cercidiphyllum japonicum</i></b> Katsura Tree	Non-Native	Softscape Boulevard	Specify single or multi-stem. Difficult to transplant. Thin bark. Needs supplemental water.	Large
<b><i>Cercis canadensis</i></b> Redbud	Native to Ontario	Softscape Boulevard	Seeds readily. Suitable for lawns but not formal boulevard due to low branching.	Medium
<b><i>Chamaecyparis nootkatensis</i></b> <b>'Pendula'</b> Weeping Nookta cypress	Native to North America	Park	Drooping branches add landscape appeal	Medium
<b><i>Cladrastis kentukea (lutea)</i></b> Yellowwood	Native to North America	Park	Few problems/use local seed sources or stock only/prune early. Sensitive	Large
<b><i>Cornus alternifolia</i></b> Alternate-leaf Dogwood	Native to Ontario	Softscape Boulevard	Use local winter hardy material only. Specify single stem	Medium
<b><i>Cornus florida</i></b> Eastern Flowering Dogwood	Native to North America	Park, Naturalizing	Endangered. Susceptible to dogwood anthracnose. Plant in limited quantities only. Symbol of Norfolk County	Small
<b><i>Cornus kousa</i></b> Kousa dogwood	Non-Native	Park	Resistant to dogwood anthracnose; berries have human food value	Small
<b><i>Cornus racemosa</i></b> Grey dogwood	Native to Ontario	Hardscape Boulevard	Specify single stem. Tolerant of poor drainage.	Small
<b><i>Corylus americana</i></b> American Hazel	Native to Ontario	Park, Naturalizing	Shrub or bush forming. Valuable food source for wildlife	Small

## Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<b><i>Corylus colurna</i></b> Turkish Hazel	Non-Native	Park	Good form/ difficult to transplant/ winter interest/ needs supplemental water	Large
<b><i>Crataegus (varieties)</i></b> Hawthorns	(Dependent on species)	Softscape Boulevard	Thornless & disease resistant varieties only. <i>Crataegus monogyna</i> is invasive, not to be used	Medium
<b><i>Fagus sylvatica</i></b> European Beech	Non-Native	Park	Needs moist soil/different leaf colours with varieties/sensitive to activity within root zone/leaves persist through winter/thin bark	Large
<b><i>Ginkgo biloba</i></b> Maidenhair tree (Male cultivar only)	Non-Native	Hardscape Boulevard	Good yellow fall colour, thin bark. Tolerant of city conditions & pollution. Slow growing but very large at maturity, virtually pest and disease free	Large
<b><i>Gleditsia triacanthos var. inermis</i></b> Thornless Honey Locust 'Shademaster' 'Skyline'	Native to North America	Hardscape Boulevard	Provides a filtered shade/susceptible to defoliation by leafhopper/susceptible to canker and other pests and diseases	Large
<b><i>Juglans cinerea</i></b> Butternut Walnut	Native to Ontario	Park, Naturalizing	Messy fruit, needs large area. Fungal disease severely affects this species.	Large
<b><i>Juglans nigra</i></b> Black Walnut	Native to North America	Park	Messy fruit/needs large area.	Large
<b><i>Juglans regia</i></b> Persian Walnut	Non-native	Park	Messy fruit/needs large area.	Large
<b><i>Liquidambar styraciflua</i></b> Sweetgum	Native to Eastern North America	Softscape Boulevard	Carolinian species, uncommon as street tree in Ontario.	Large
<b><i>Liriodendron tulipifera</i></b> Tulip tree	Native to Ontario	Softscape Boulevard	Good flowers and yellow fall colour/local sources/moist well drained soil/very large tree most appropriate for lawn areas/ somewhat weak wooded	Large

## Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<b><i>Maackia amurensis</i></b> Amur Maackia	Non-Native	Softscape Boulevard	Small, round headed tree/slow growing/ summer, flowering/bronze coloured bark. Not for use near naturalized areas.	Small
<b><i>Magnolia acuminata</i></b> Cucumber tree	Native to Ontario	Park	Status: Endangered	Medium
<b><i>Malus</i></b> Flowering & Domestic Crab Apple	(Dependent on species)	Park	Maintenance problems. Disease & insect problems. Tolerates most soils. Choose persistent fruit-holding, or poorly-fruited types.	Small to Medium
<b><i>Malus coronia</i></b> Wild Crabapple	Native to Ontario	Park		Large
<b><i>Nyssa sylvatica</i></b> Black Gum	Native to Ontario	Hardscape Boulevard	Carolinian species, tolerant of urban conditions, but uncommon as a street tree in Ontario. Tolerant of poor drainage.	Large
<b><i>Ostrya virginiana</i></b> Hop Hornbeam or Ironwood	Native to Ontario	Softscape Boulevard	Mainly an understory species	Medium
<b><i>Phellodendron amurense</i></b> Amur corktree	Non-Native	Softscape Boulevard	Good winter texture in bark, lots of black berries. Not appropriate near naturalized areas.	Medium
<b><i>Physocarpus</i></b> Common Ninebark	Native Ontario	Park, Naturalizing	Shrub or bush forming growing almost as wide as it does tall. Fast growing.	Small
<b><i>Picea abies</i></b> Norway spruce	Non-native	Park	Fast growing. Requires a lot of space. Shade tolerant.	Large
<b><i>Picea glauca</i></b> White spruce	Native to Ontario	Park	Slower growth rate than Norway spruce. Tolerates wide range of soil types	Large
<b><i>Pinus strobus</i></b> White Pine	Native to Ontario	Park	Locate with care in boulevards, due to possible sight line and access issues when mature (bushy) Avoid Ribes (alternate host for white pine blister rust)	Large

## Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<b><i>Platanus occidentalis</i></b> Sycamore	Native to Ontario	Softscape Boulevard	Frost cracks on trunk/attractive peeling bark/fruit can cause problems/very large at maturity – reserve for large lots and lawn areas	Large
<b><i>Platanus x acerifolia</i></b> London Planetree	Hybrid of <i>Platanus occidentalis</i> (N. America) and <i>Platanus orientalis</i> (Europe), so has no native range	Softscape Boulevard	Prone to frost cracks on trunk. Attractive peeling bark. Fruit can cause problems. Very large at maturity – reserve for large lots and lawn areas	Large
<b><i>Populus ssp.</i></b> Balsam Poplar, Eastern Cottonwood, Large-tooth Aspen, Trembling Aspen	Balsam Poplar, Eastern Cottonwood, Large-tooth Aspen: Native to Ontario	Park, Naturalizing Not permitted in Boulevard	Wood is light, soft and weak, breaks easily in storms. Drops flowers, fruit, twigs and branches; tolerant of poor drainage	Large
<b><i>Populus ssp.</i></b> Dwarf varieties (eg. <i>Populus balsamifera</i> 'Paskapoo')		Park	Limited numbers may be considered in Boulevards on a trial basis	Medium
<b><i>Prunus spp</i></b> Flowering Cherry <b><i>Prunus sargentii</i></b> <b><i>Prunus kwanzan</i></b> <b><i>Prunus x yedoensis</i></b>	(Dependent on species; most popular flowering cherries are non-continental)	Softscape Boulevard	Weeping cankers; prone to fungal infections. For use in limited circumstances	Small
<b><i>Prunus Americana</i></b> American plum	Native to Ontario	Park	Somewhat thorny. Not Suitable for boulevards	Small
<b><i>Prunus nigra</i></b> Canada plum	Native to Ontario	Park	Somewhat thorny. Not suitable for boulevards	Medium
<b><i>Prunus pensylvanica</i></b> Pin Cherry	Native to Ontario	Park; naturalizing	Excellent flowers with no fruit. Single stem to be specified. Weeping cankers. Not specimen tree.	Small

## Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<b><i>Prunus serotina</i></b> Black Cherry	Native to Ontario	Park; naturalizing	Interesting bark, messy fruit. Better in lawns than in formal boulevard.	Large
<b><i>Prunus virginiana</i></b> Choke Cherry	Native to Ontario	Park; naturalizing	Green spring foliage & red in summer/bark. Tends to split. Black knot. Not specimen tree.	Small
<b><i>Ptelea trifoliata</i></b> Hop Tree	Native to Ontario	Softscape Boulevard	Easily grown in average, dry to medium, well-drained soils in part shade to full shade. Tolerates full sun. One of two native larval host plants for the rare Giant Swallowtail butterfly. Adaptable to wide range of growing conditions.	Medium
<b><i>Quercus alba</i></b> White Oak	Native to Ontario	Softscape Boulevard	Needs moist soil/fruit maintenance/needs large space at maturity	Large
<b><i>Quercus ellipsoidalis</i></b> Northern Pin Oak	Native to Ontario	Softscape Boulevard	Vulnerable to iron deficiency	
<b><i>Quercus macrocarpa</i></b> Bur Oak	Native to Ontario	Hardscape Boulevard	Large size at maturity – reserve for large lots and lawn areas fruit drop. Difficult to transplant; requires good soils	Large
<b><i>Quercus robur</i></b> 'Fastigata' Fastigate English Oak	Non-Native	Park	Needs well drained soil holds leaves through the winter difficult to transplant very upright in form – reserve for sites with specific need for this form	Large
<b><i>Quercus robur</i></b> English Oak	Non-Native	Park	Needs well drained soil/difficult to transplant/large size at maturity	Large
<b><i>Quercus muehlenbergii</i></b> Chinquapin Oak	Native to Ontario	Softscape Boulevard	Attractive tree, especially in old age. Limited planting, due to possibility of Oak Wilt entering Ontario	Medium
<b><i>Quercus rubra</i></b> Red Oak	Native to Ontario	Hardscape Boulevard	Limited planting, due to possibility of Oak Wilt entering Ontario. Needs sandy loam soil difficult to transplant more salt tolerant and faster growing than other oaks	Large
<b><i>Quercus shumardii</i></b> Shumard Oak	Native to Ontario	Park	Listed as 'special concern' in Ontario. Prefers moist, well-drained soils.	Large

## Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<b><i>Quercus velutina</i></b> Black Oak	Native to Ontario	Park	Limited planting, due to possibility of Oak Wilt entering Ontario. Needs well drained soil/difficult to transplant/large size at maturity	Large
<b><i>Rhus ssp.</i></b> Staghorn Sumac, Smooth Sumac, Fragrant	Native to Ontario	Park; naturalizing	Spreads quick, freely suckers from roots creating wide spreading colonies. Tolerates dry sterile soils	Small
<b><i>Sassafrass albidum</i></b> Sassafrass	Native to Ontario	Park, Naturalizing	Prefers sandy soils	Medium
<b><i>Sophora japonica</i></b> Japanese Pagoda Tree	Non-Native	Hardscape Boulevard	Excellent white flower green stem when young. Limit use due to messy characteristics	Large
<b><i>Staphylea trifolia</i></b> Bladdernut	Native to Ontario	Park, Naturalizing	Shrub or bush forming, usually found in lowlands or along streams.	Small
<b><i>Syringa reticulata</i></b> Japanese Tree Lilac (Ivory Silk)	Non-Native	Hardscape Boulevard	Good white summer flower. Excellent small specimen. Prone to over-use	Small
<b><i>Tilia Americana</i></b> Basswood	Native to Ontario	Hardscape Boulevard	Prefers deep moist fertile soil. Will grow on drier heavier soil. Needs large space	Large
<b><i>Tilia cordata</i></b> Littleleaf Linden 'Glenleven' 'Greenspire' 'Greenglobe'	Non-Native	Hardscape Boulevard	Aphid & borer problems; suckers from base	Medium
<b><i>Tilia tomentosa</i></b> Silver Linden	Non-Native	Softscape Boulevard	Heat and drought tolerant.	Medium
<b><i>Tilia x euchlora</i></b> Crimean Linden	Non-Native	Softscape Boulevard	Fruit messy suckers from base. For use in limited circumstances	Medium
<b><i>Tsuga canadensis</i></b> Eastern Hemlock	Native to Ontario	Park	Prefers moist soils and sheltered areas. Tolerates shade	Large

## Sub-urban Boulevards, Parks

Tree Species	Native Range	Use	Comments and Notes	Size
<b><i>Ulmus Americana</i></b> Elm 'Homestead' 'Pioneer' 'Jefferson'	Specific cultivars hybridized for disease resistance	Hardscape Boulevard	Tolerant of urban conditions and moist soil. These cultivars are currently shown to have good resistance to Dutch Elm Disease, but this is in flux and should be constantly reassessed.	Large
<b><i>Zelkova serrata</i></b> Japanese Zelkova 'Green Vase' 'Village Green'	Non-Native	Softscape Boulevard	Rapid growth Narrow branch angles promote fork split. Frost susceptibility when young	Large

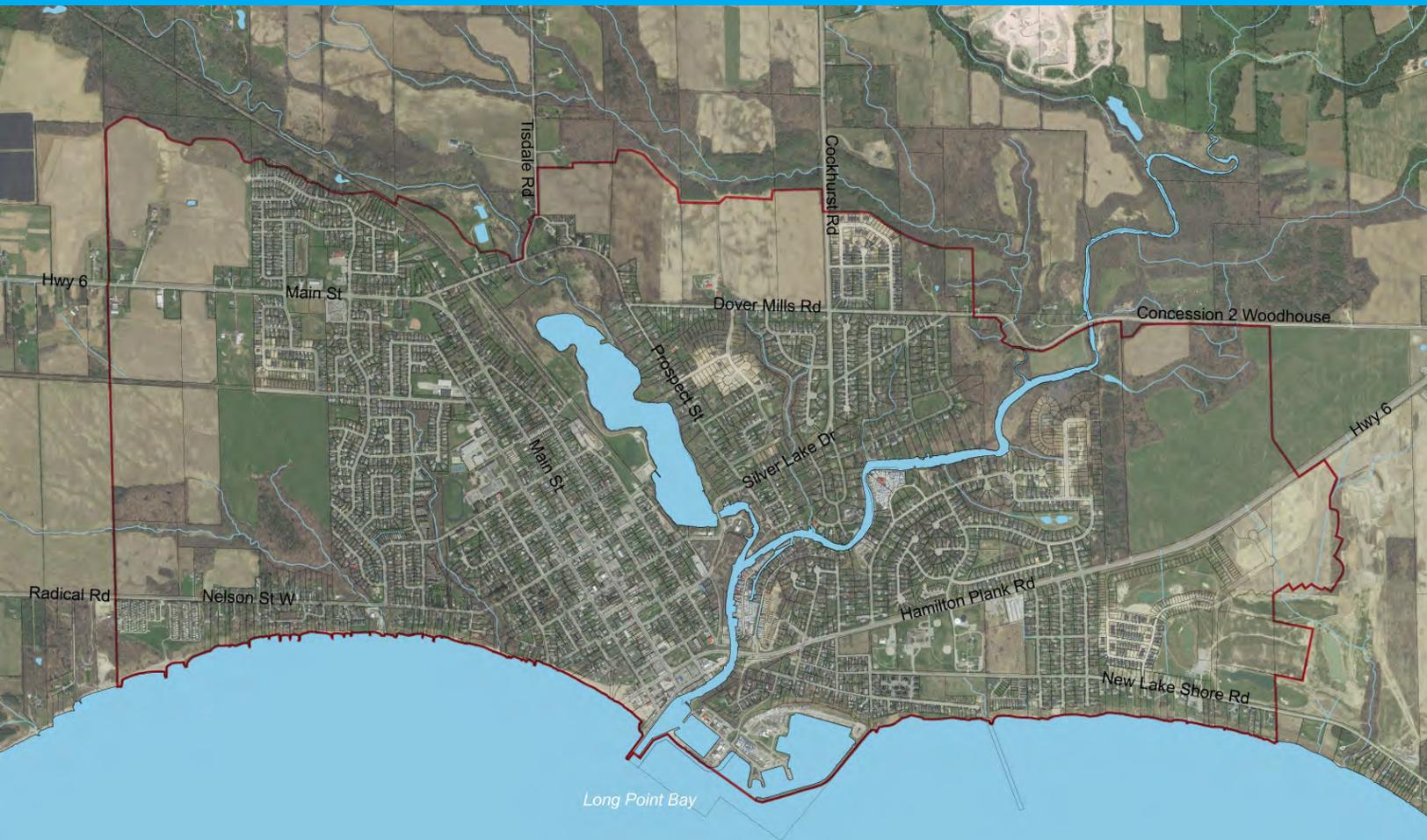
## Trees to be Avoided

Tree Species	Native Range	Comments and Notes	Size
<b><i>Acer platanoides</i></b> OR <b><i>Acer x platanoides</i></b> Norway Maple (many cultivars) or Norway maple cross	Non-Native	Surface roots conflict with and turf/girdling roots/aphid and wilt problems. Invasive.	Medium
<b><i>Acer negundo</i></b> Manitoba maple	Native to North America	Weak-wooded	Large
<b><i>Ailanthus altissima</i></b> Tree of Heaven	Non-Native	Invasive, host for spotted lantern fly	Large
<b><i>Alnus glutinosa</i></b> European Alder	Non-Native	Tolerant of wet & dry soil. Invasive tendencies checked by dry sites.	Medium
<b><i>Caragana arborescens</i></b> Siberian Pea-shrub	Non-Native	Toxic	Small
<b><i>Eleagnus angustifolia</i></b> Russian Olive	Non-Native	Invasive	Medium
<b><i>Fraxinus spp.</i></b> Ash (all species)	Native to Ontario	Susceptible to Emerald Ash Borer.	Large
<b><i>Morus spp.</i></b> Mulberry	Non-Native	Invasive	Large
<b><i>Paulownia spp.</i></b> Princess Tree	Non-Native	Invasive (within the United States)	
<b><i>Pyrus calleryana</i></b> Callery Pear 'Chanticleer' 'Bradford'	Non-Native	Fireblight problems, weak wooded	Small
<b><i>Robina pseudoacacia</i></b> Black locust	Non-Native	Invasive	Large
<b><i>Sorbinia spp.</i></b> Mountain Ash	Non-Native	Fireblight problems	Medium

## Trees to be Avoided

Tree Species	Native Range	Comments and Notes	Size
<i>Ulmus pumila</i> Siberian Elm	Non-Native	Invasive	Large
<i>Chionanthus virginicus</i> Fringe tree	Non-Native	Alternate host for Emerald Ash Borer	Medium

# Port Dover Secondary Plan



## Background Report

DRAFT July 31, 2020



PLAN B Natural Heritage

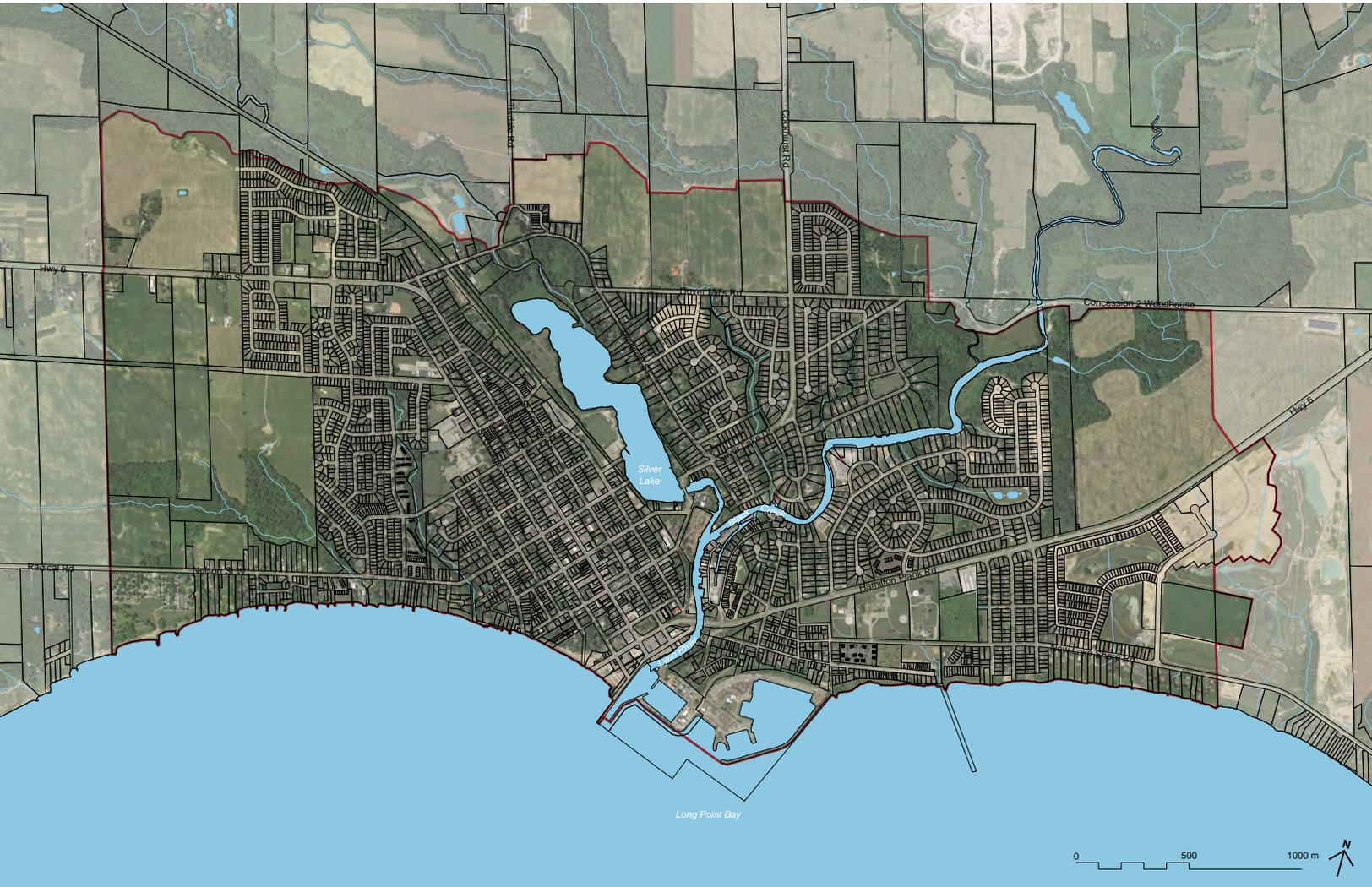
BA Group

SCS Consulting Group



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*Study area for the Port Dover Secondary Plan*

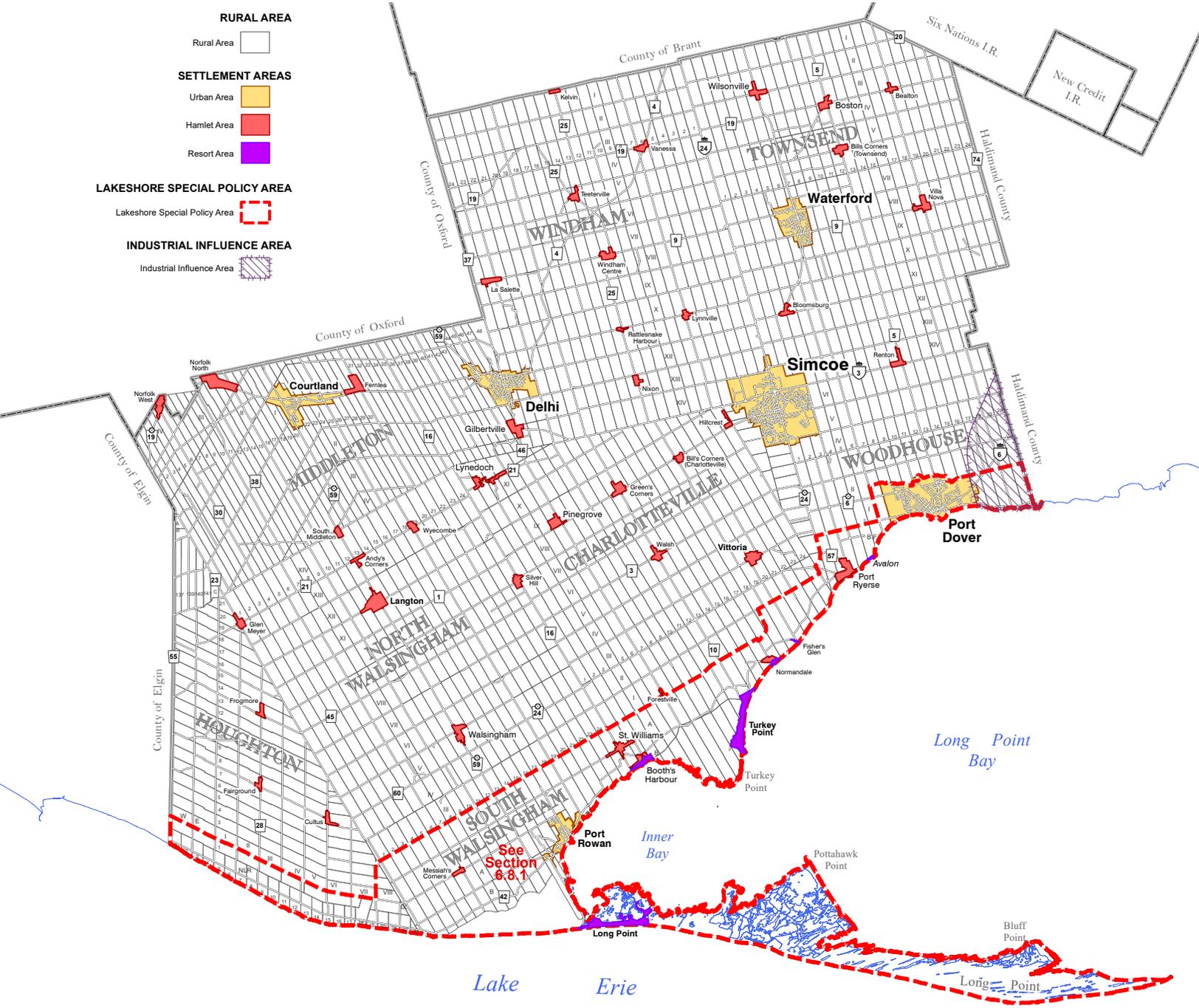
# 1 Introduction

Norfolk County is preparing a Secondary Plan for Port Dover to provide a clear and detailed planning framework for the waterfront, downtown and future residential development within the urban boundary. The Secondary Plan will be used to guide municipal stewardship of the lake and river and natural heritage areas, create a vision for downtown and evaluate development applications.

Council's vision for the Port Dover Secondary Plan is to transform Port Dover into a world-class tourist destination with a thriving economy, vibrant community spirit and protected natural heritage system. The Secondary Plan will provide the mechanisms to enhance the beauty of its waterfront, the place-making tools to spark the unique character of the small town, and the policies to protect the long-term financial and ecological health of the area.

As part of Phase 1 of Secondary Plan process, the team has reviewed background information to understand existing conditions and to identify the key considerations for the Secondary Plan. This report summarizes the review of:

- Planning by The Planning Partnership;
- Downtown Port Dover by The Planning Partnership;
- River and lakefront by The Planning Partnership;
- Natural heritage by PLAN B Natural Heritage;
- Servicing by SCS Consulting Group; and,
- Transportation by BA Group.



Norfolk County Official Plan Schedule A - Community Structure

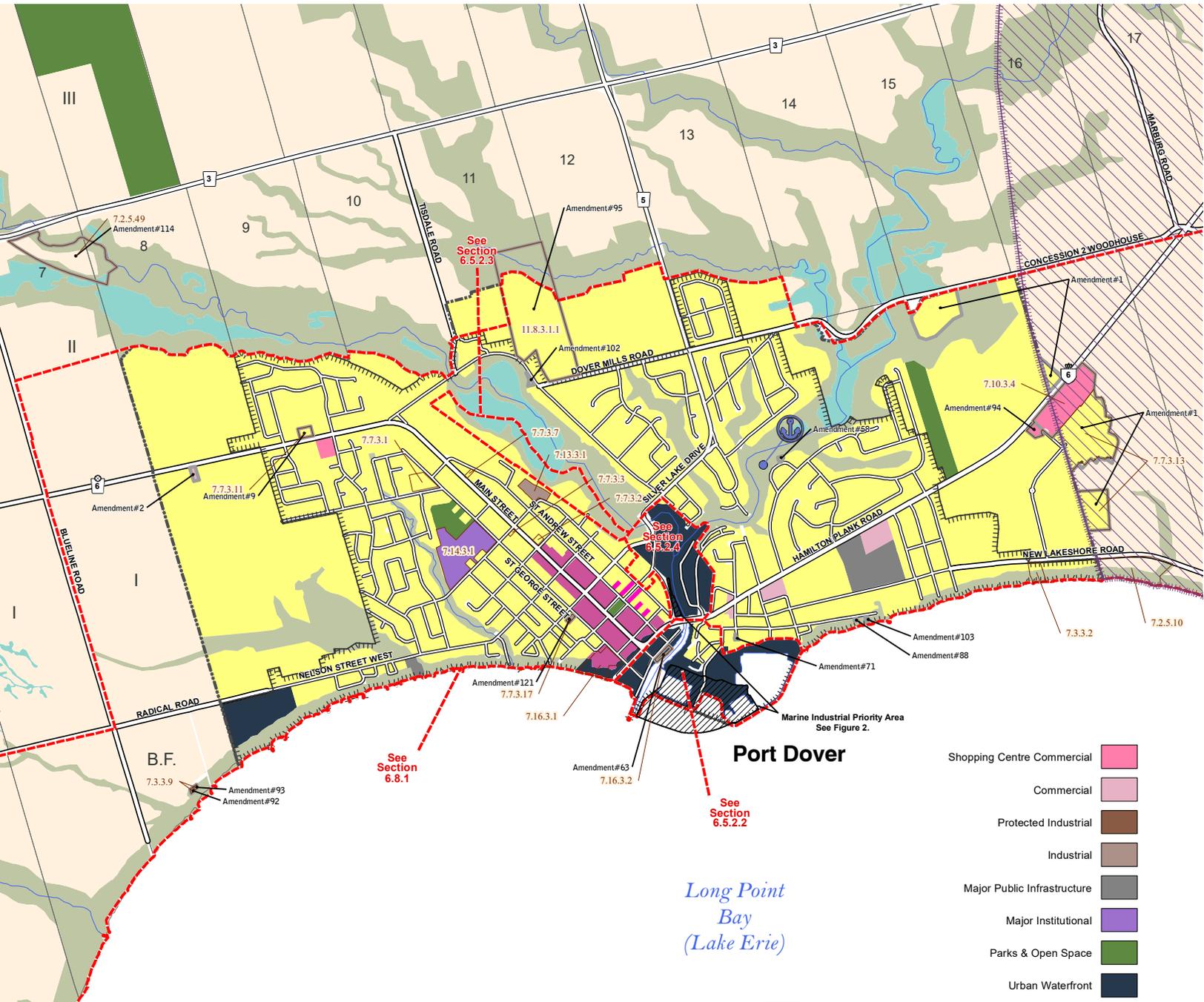
# 2 Planning

## 2.1 Planning Policy

### Schedule A - Community Structure

Port Dover is identified on Schedule A of the Official Plan as an Urban Area. It is also located within the Lakeshore Special Policy Area, and the far eastern edge of the community is identified as within the Industrial Influence Area.

- The **Urban Areas** have been identified as places that will accommodate the greatest amount of growth over the planning horizon. The Urban Areas will accommodate a wide range of uses such as residential, commercial, various employment uses, entertainment, cultural, and health services.
- The **Lakeshore Special Policy Area Secondary Plan** encompasses the entire extent of Norfolk County's waterfront, lakeshore related Settlement Areas and related Agricultural Area. The Lakeshore Special Policy Area Secondary Plan provides more detailed policies related to growth management, agriculture, natural and cultural heritage, tourism and recreation, infrastructure and community design for the County's lakeshore, recognizing the unique attributes and development pressures along the lakeshore.
- The **Industrial Influence Area** is a 3 kilometre buffer around the exterior of the Lake Erie Industrial Park that protects heavy industrial uses from new incompatible land uses that would reduce their ability to operate effectively. Uses that are permitted with the Agricultural Designation, including limited residential development is permitted within the Industrial Influence Area.



Norfolk County Official Plan Schedule B-16 - Land Use

- |                                  |  |  |  |
|----------------------------------|--|--|--|
| Agricultural                     |  | Shopping Centre Commercial                         |  |
| Hazard Lands                     |  | Commercial   |  |
| Provincially Significant Wetland |  | Protected Industrial                               |  |
| Hamlet                           |  | Industrial   |  |
| Resort Residential               |  | Major Public Infrastructure                        |  |
| Urban Residential                |  | Major Institutional                                |  |
| Mixed Residential/Commercial     |  | Parks & Open Space                                 |  |
| Downtown                         |  | Urban Waterfront                                   |  |
|                                  |  | Built Boundary                                     |  |
|                                  |  | Special Policy Area                                |  |
|                                  |  | Site Specific Policy Area                          |  |
|                                  |  | Industrial Influence                               |  |
|                                  |  | Marine Use in Hazard Lands (Section 7.3.1(g)(iii)) |  |
|                                  |  | Urban Area Boundary                                |  |
|                                  |  | Hamlet Area Boundary                               |  |
|                                  |  | Resort Area Boundary                               |  |

## Schedule B-16 - Land Use

This is a complex schedule that identifies the land use pattern within the urban area boundary of the Port Dover Community. The Schedule also identifies the Built Boundary and the Special Policy Area Boundary. Within the Special Policy Area Boundary, there are a number of site specific policy references - Sections 6.5.2.2, 6.5.2.3, 6.5.2.4, and 6.8.1.

### 6.5.2.2 Port Dover Waterfront Special Policy Area

- The County identifies the Port Dover Waterfront Area as an important tourism and economic resource, and it is the intent of the County's Official Plan that the Area remain an area that supports tourism, the commercial fishery, and provides recreational, commercial and cultural opportunities for residents.

### 6.5.2.3 Silver Lake Special Policy Area

- The Silver Lake Policy Area is an area that has historically included industrial and open space uses and is identified as an area that can transition to open space and recreational uses through the creation of a linkage between the Downtown Area with the open space areas and the Lynn Valley Trail.

### 6.5.3.4 Lynn River/Black Creek Special Policy Area

- This area is the site of marine industries, as Lynn River is the main artery for marine traffic to this area. The Lynn River/Black Creek Special Policy Area has the potential to support a variety of interests related to tourism, recreation and innovative residential development as well as supporting the existing marine industries, while recognizing that a significant portion of the lands within this Area are considered Hazard Lands.
- It is the intent of the County's Official Plan to create a continuous link for commercial and tourism related uses between the Downtown Area and the waterfront through the Lynn River/Black Creek Area and back to the Downtown Area through a connection along Market Street, Chapman Street and Powell Park.

## 6.8.1 Lakeshore Special Policy Area

- The Lakeshore Special Policy Area Secondary Plan provides more detailed policies related to growth management, agriculture, natural and cultural heritage, tourism and recreation, infrastructure and community design.

The majority of the Town is Urban Residential. The following other land use designations are identified:

- **Downtown** – This designation is the most permissive, allowing retail, office, service, entertainment, and other commercial uses, as well as government, institutional, residential and community activities. The Downtown designation promotes pedestrian scaled, compact development, with active and attractive uses at grade.
- **Shopping Centre Commercial** – This designation applies to the existing shopping centres located outside of the Downtown areas and permits a wide range of commercial uses that are primarily related to retail, as well as a limited amount of office uses related to the local area. This designation also permits medium and high-density housing. Shopping centre commercial uses are generally located along Provincial Highways and arterial roads.
- **Commercial** – This designation applies to commercial areas along the major transportation routes, and permits retail and commercial uses which are intended to serve the travelling public and are space-extensive, destination uses. Residential uses are permitted within this designation.
- **Urban Waterfront** – This designation applies to urban areas in direct proximity to Lake Erie's water's edge and permits commercial and service facilities. Residential development is permitted as long as it is located within a mixed-use building. This designation is largely composed of lands that are within flood prone area and are considered to be hazard lands.

- **Major Institutional** – This designation applies to major public institutions within the County, and also permits places of worship, cultural facilities, special needs housing including housing for seniors, and other clubs and organizations.
- **Parks + Open Space** – This designation applies to parks and open space within the county and permits a wide range of recreational facilities and uses.
- **Major Public Infrastructure** – This designation applies to public services and is to be protected from intrusions by sensitive land uses.
- **Industrial** – This designation applies to older industrial sites that are under-utilized and are situated near residential areas, making it difficult to attract new industrial investment. The conversion of industrial lands within this designation to uses more compatible with the neighbourhood context is encouraged.
- **Marine Use in Hazard Lands (Section 7.3.1 (g) (iii))** – This designation permits marinas that may include a restaurant, recreational park and or sales/service function in association with the marine operation and accessory uses.
- **Provincially Significant Wetland** – This designation permits agricultural uses, conservation management, recreational and educational activities, hunting/trapping, boardwalks, fish hut, and duck blinds.
- **Hazard Lands** – This designation applies to lands that have an inherent environmental hazard. Permitted uses within this designation include agriculture, forestry, conservation, flood and erosion controls, outdoor recreational uses, resource extraction and marine uses.

There are also a host of site-specific Official Plan Amendments located throughout the community.

#### Schedule D-6 - Source Water Protection

This map identifies that the Port Dover Community lies outside of the Highly Vulnerable Aquifers that cover most of the rest of the County.

#### Schedule D-7 - Source Water Protection

This map identifies that the Port Dover Community lies outside of the Significant Groundwater Recharge Areas that cover most of the rest of the County – with the minor exception of a small area in the far north-west of the Community.

- The Significant Groundwater Recharge Areas have been identified within Norfolk's Official Plan for information purposes only. The Source Water Protection Plans that govern Norfolk County do not currently contain specific policies relating to Significant Groundwater Recharge Areas and no Official Plan policies are in place in relation to Significant Groundwater Recharge Areas.

#### Schedule J-5 - Natural Resources

This map identifies that much of central and western Port Dover is within an Abandoned Petroleum Pools Area, with some pockets identified as Suspended Petroleum Pools. Much of the lands abutting the creek systems are identified as a Bedrock Resource. There is also a Closed Waste Disposal Site identified on the Map.

- The Official Plan states that exhausted petroleum resource areas and wells be rehabilitated for appropriate uses that are compatible with the surrounding areas.
- Bedrock Resource Areas shall be protected for future use, and any development or changes in land use which would prevent future use will not be permitted unless it can be demonstrated that:
  - Resource use would not be feasible
  - Proposed development or land use serve a greater long-term interest
  - Issues of public health, public safety and environmental impact addressed

- Wayside pits and quarries are not permitted adjacent to or within Provincially Significant Features, Natural Heritage Features, Urban Areas, Hamlet Areas or Resort Areas.

Redevelopment of closed Waste Disposal Sites is permitted by way of Official Plan and Zoning By-law amendments. In addition, development proposals within 500 metres of a closed Waste Disposal Site shall be accompanied by a hydrogeologic/engineering study, which will address any mitigation measures required.

### Lakeshore Special Policy Area Secondary Plan

The Lakeshore Special Policy Area (LSPA) Secondary Plan (2009) includes policies and supporting Community Design Guidelines that manage growth, address and protect the special lakeshore attributes and the unique local land use arrangements in order to ensure the community, environmental and economic sustainability of the County's waterfront.

The LSPA includes the entire extent of the County's waterfront, lakeshore related Settlement Areas, and related Agricultural Area and is identified on Schedule F - Community Structure.

**Schedule F - Community Structure** - This map is related to the Lakeshore Special Policy Area Secondary Plan. It identifies conceptually that the core of Port Dover is an Urban Tourism Node, and Nelson Street West is identified as Ontario's South Coast Scenic Route.

- Urban Tourism Nodes are intended to be the focus of tourism and economic activities, and a range of tourism related uses such as commercial, accommodations, restaurants, entertainment and recreational uses are permitted and encouraged. Within an Urban Tourism Node, tourism development should be primarily directed to Downtown Area and to lands within the Urban Waterfront Designation.
- Ontario's South Coast Scenic Route extends across the County's lakeshore and is envisioned as both a scenic drive and recreational trail, which will accommodate a variety of active transportation users and link major tourist, recreation, agriculture and natural environment destinations along the lakeshore. Recreational, commercial and tourism uses are permitted and encouraged along the Scenic Route.

### **Schedule H-1 - Port Dover Neighbourhood Planning Areas**

- This map is related to the Lakeshore Special Policy Area Secondary Plan. This map identifies two Neighbourhood Planning Areas - Port Dover West, and North Dover Mills.

- Neighbourhoods Planning Areas are areas that have been identified where a Neighbourhood Plan will need to be prepared prior to development occurring.

### **Schedule H-1A - Port Dover Neighbourhood Planning Areas**

- This map is related to the Lakeshore Special Policy Area Secondary Plan. It identifies a more detailed residential density distribution and primary road pattern for most of the North Dover Mills Neighbourhood Planning Area.

- The North Dover Mills Neighbourhood Planning Area is subject to a Neighbourhood Plan. Neighbourhood Plans provide a more detailed land use plan and specific policies to guide and manage future development. The North Dover Mills Neighbourhood Plan permits high density residential, which allows for apartment buildings and other residential buildings of similar density, medium density residential, which allows for converted dwellings with more than 2 units, triplexes, fourplexes, townhouses, walk-up apartments and other residential buildings that have a net density of 15 units per hectare and single detached dwellings.

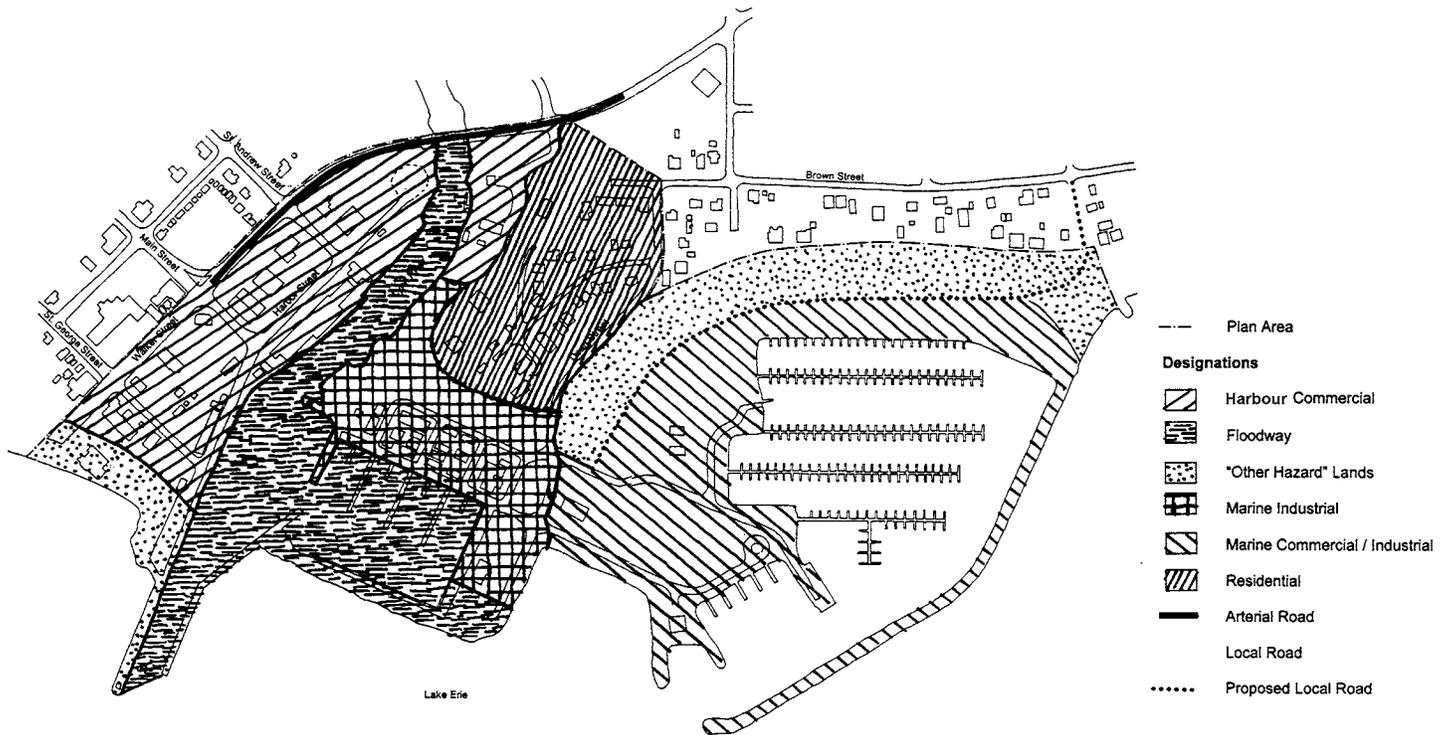
### **Port Dover Waterfront Secondary Plan**

The Port Dover Waterfront Master Plan and Secondary Plan was completed in 2001 for the former City of Nanticoke with the intent to guide future development in a manner that is compatible with the existing character and charm of Port Dover.

The Secondary Plan moved the emphasis for development appraisal from controlling the land use to the careful review of built form and compatibility for which the Secondary Plan policies and companion Urban Design Guidelines provide the framework.

**Land Use Map** - This map (at right) from the Port Dover Waterfront Secondary Plan identifies six land use designations in the Port Dover Waterfront:

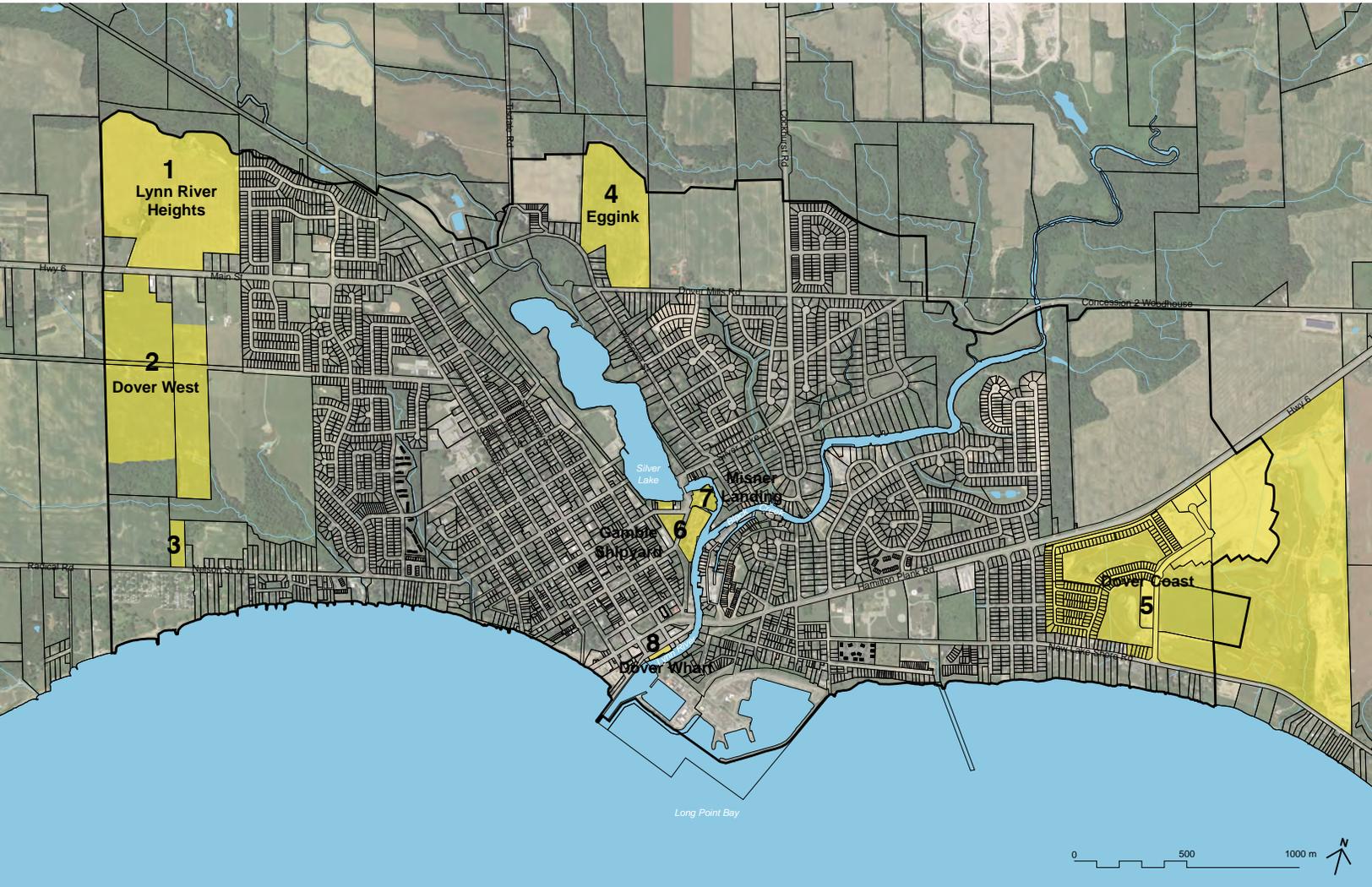
- **Harbour Commercial** – This new designation permitted a wide range of mixed retail, commercial and residential uses, but any residential and office uses may not be located at grade.
- **Floodway** – Applied the policies of Sections P.1 and P.2 of the Nanticoke Official Plan.
- **Other Hazard Lands** – Applied the policies of Section P.4 of the Nanticoke Official Plan.
- **Marine Industrial** – Applied the Marine Industrial designation and policies of Section J.5 from the Nanticoke Official Plan with intent to protect and maintain the Commercial Fishing Port as an ongoing viable commercial/ industrial activity that has historic relevance and adds to the charm and appeal of the community.
- **Marine Commercial/Industrial** – This new designation permitted restaurants and snack bars, marinas and related retail, commercial, office, recreation, club house and banquet uses and facilities, parks, open space uses and public walkways, and industrial uses including boat-works and other marine related commercial uses, with the intent to promote and encourage the ongoing operations and the potential expansion of the facilities and services of the Recreational Marina.



Land Use Map from Port Dover Waterfront Secondary Plan

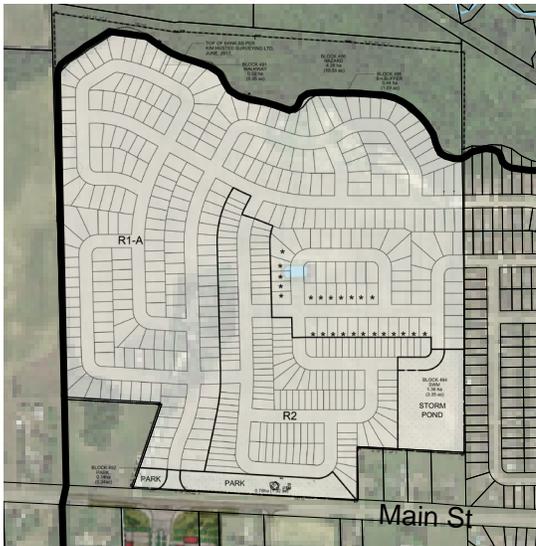
- **Residential** – Applied the policies of Section G of the Nanticoke Official Plan with the intent to protect and enhance the existing Residential Neighbourhood.

The policies of Section G of the Nanticoke Official Plan were applied to the road classifications identified on the Land Use Map. A new roadway was identified that may be required to service the existing Recreational Marina and Commercial Port.

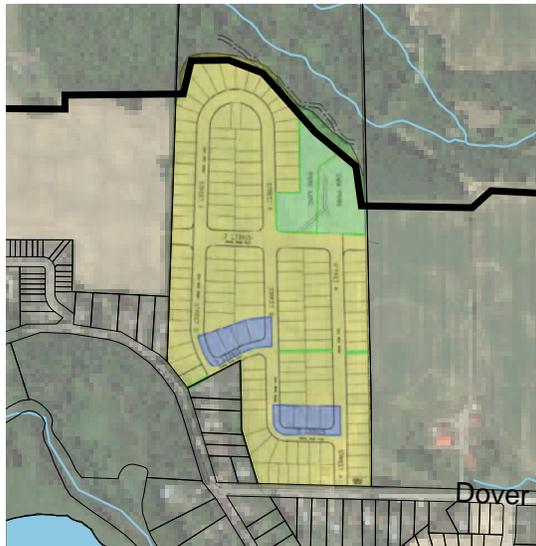


Map of Development Activity in Port Dover

**1 Lynn River Heights**



**4 Eggink**



## 2.2 Development Activity

### Development Applications

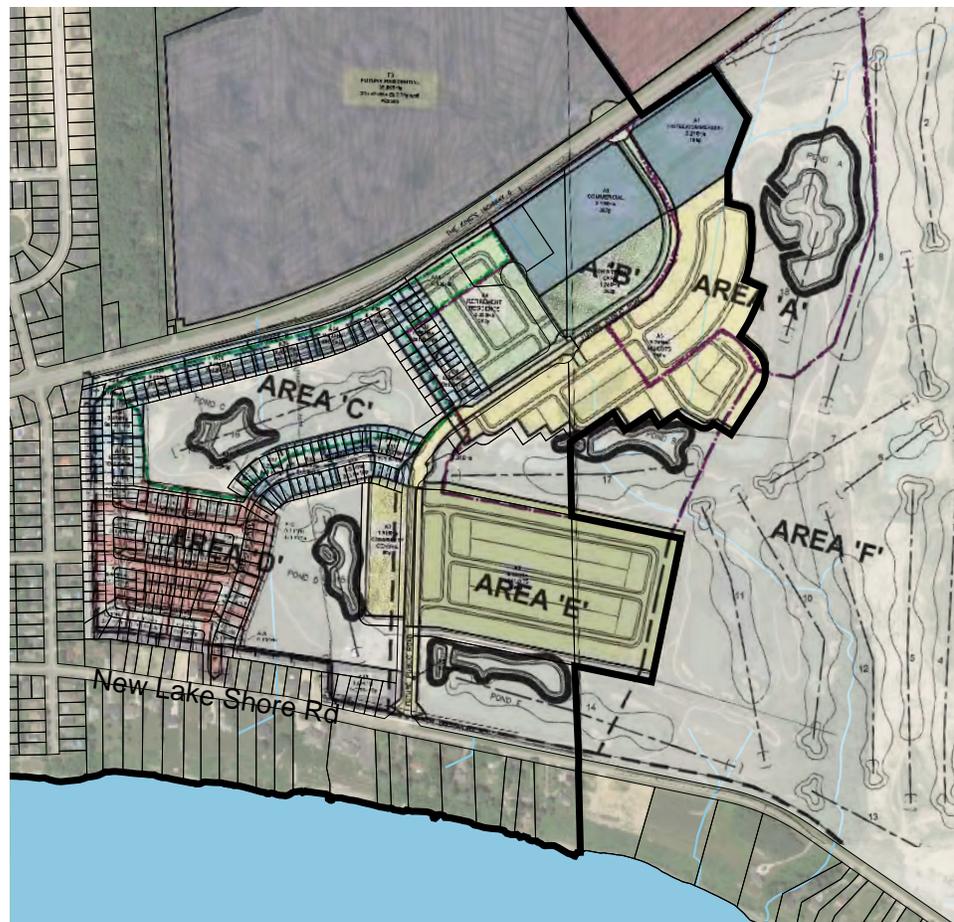
The following table summarizes the status of the development applications in Port Dover.

#	Name	Status
1	Lynn River Heights	On hold until the end of the ICB, but all comments addressed and ready for public meeting when ICB is lifted
2,3	Dover West	?
4	Eggink	Appealed for non decision
5	Dover Coast	<ul style="list-style-type: none"> <li>LTC is exempt from ICB (except it's approved for more beds)</li> <li>Site Plan for the hotel and convention Centre is on hold</li> <li>Residential south of Highway 6 is all approved - Phase 1 complete, Phase 2 under construction/complete</li> <li>No plans for lands north of Highway 6</li> </ul>
6	Gamble Shipyard	Has OPA and ZBL and wants to move ahead in spite of ICB
7	Misner Landing	Referred to LPAT
8	Dover Wharf	Under Construction

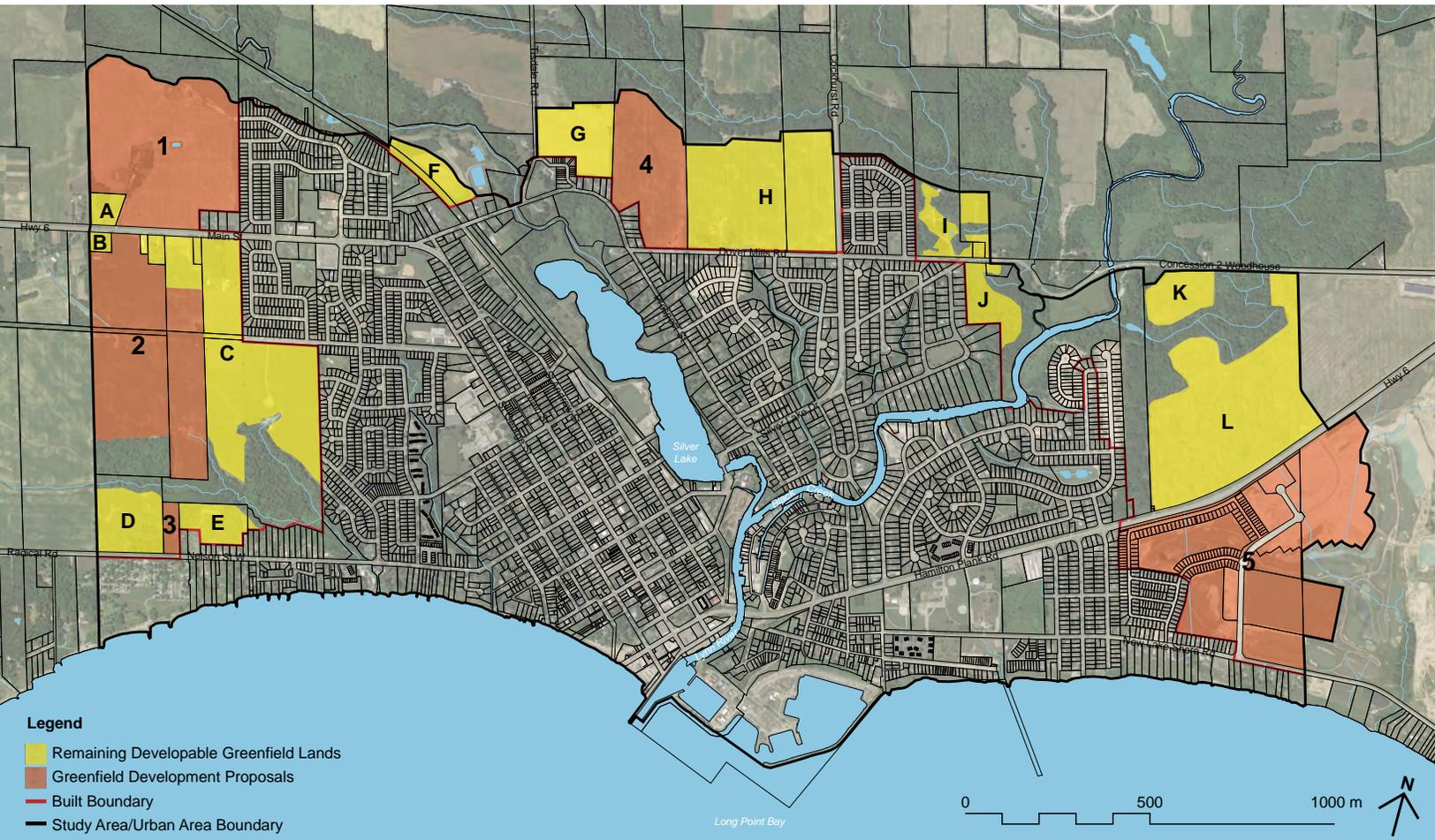
### 2,3 Dover West



### 5 Dover Coast



## Development Projections for Greenfield Lands



## Development Projections for Greenfield Lands

Name	Parcel	Area (Ha)	Units	Density
Lynn River Heights	1	35.5	499	14
Dover West	2	35.4	870	25
Dover West (south parcel)	3	1.4	34	24
Eggink	4	15.6	160	10
Dover Coast	5	52.3	473	9
<b>Totals</b>		<b>140.2</b>	<b>2,036</b>	<b>15</b>

Parcel	Area (Ha)	Units	Density
A	1.6	23	15
B	0.7	10	15
C	31.5	457	15
D	6.8	99	15
E	4	58	15
F	4.1	60	15
G	7.9	115	15
H	27.7	402	15
I	4.6	67	15
J	4.6	67	15
K	4.9	71	15
L	35.2	511	15
<b>Totals</b>	<b>134</b>	<b>1,940</b>	<b>15</b>

Population			
<b>Combined Totals</b>	<b>274</b>	<b>3,976</b>	<b>11,928</b>

## 2.3 Things to Think About

Based on the review of background information and the public input received so far, the following are things to think about as the team begins Phase 2: Concepts.

- Range of housing planned in Future Residential Areas and through intensification.
- Location of neighbourhood defining elements planned for in the Future Residential Areas in order to ensure that newly developing areas contribute to the character of Port Dover
- Mix of uses in Future Residential Areas to provide the opportunity for residents to walk to shops and services in their neighbourhood
- Protection of the natural heritage system in newly developing areas and in the current built up area
- Public access along or near the water's edge of the lake and rivers
- Provision of community amenities in newly developing areas
- Phasing of development to enable servicing
- How big does Port Dover want to be? There is tremendous potential for growth in the Future Residential Areas.

## Official Plan Land Uses for Downtown Port Dover



# 3 Downtown

Downtown Port Dover is identified as one of the specific areas to be considered in the Secondary Plan. The Secondary Plan is to promote economic diversification and to protect the area.

Parcel Land Use by MPAC Class for Downtown Port Dover



## Public Parks in Downtown Port Dover



## Land Ownership (Public/Accessible) in Downtown Port Dover



## Heritage in Downtown Port Dover



1



2



3



4



5



6

Designated Heritage in Downtown Port Dover

## Main Street Frontage in Downtown Port Dover



Frontage along Main Street



Frontage along Main Street

## Vacant Sites with Potential in Downtown Port Dover



### 3.1 Things to Think About

Based on the review of background information and the public input received so far, the following are things to think about as the team begins Phase 2: Concepts.

- Parking - determining the most appropriate number, type, location and form of management
- Pedestrian priority areas - clarifying the implications for traffic circulation and parking
- Determining the most appropriate form and character of new development
- How to celebrate the history and stories of Port Dover
- How to create a healthy vibrant downtown year round
- A range of housing to provide options in type and tenure for a all demographics – singles, couples, young families, seniors.
- Boundaries of the Downtown – should it be extended west to include St. George Street or south across the River or to include the waterfront?
- Managing the type of retail – understand the concerns related to brand retail and restaurants
- Design guidelines to protect the character of Main Street



Land Ownership in Port Dover's Central Waterfront Area

## 4 River & Lakefront

Lands along the Lynn River and Lake Erie waterfront are specific areas to be addressed in the Secondary Plan. Port Dover is well known as a waterfront community. The beach, cottage rentals, the harbour and marinas are core attractions in Port Dover.

A Waterfront Master Plan and Secondary Plan was prepared in 2001. It is based on three guiding principles:

**Accessible** – planning for continuous and safe routes for pedestrians and cyclists, connecting the larger trail systems, balancing vehicle access and demand for parking in peak use periods

**Diverse** – celebrate and promote the diversity of use and builds, in particular the commercial fishing port

**Attractive** – protect and plan for a mix of building styles and uses to add to the inherent charm in the character of the waterfront area.

The Waterfront Master Plan and Secondary Plan is based on a number of structuring elements:

- Provides a variety of key public places, linked together by a continuous pedestrian walkway along the waterfront
- Establishes a history walk along Harbour Street



Demonstration Plan from the 2001 Waterfront Master Plan & Secondary Plan

- Accommodates new private development to complement uses in the harbour commercial district, with 3 scenarios for redevelopment on key vacant or under used parcels.
- Maintains the commercial fishing port
- Opportunity for an expanded Recreational Marina with an extension to the breakwater that could provide a safe protected harbour for an additional 300 boats.

The Waterfront Master Plan and Secondary Plan includes urban design guidelines to provide general direction for development for streets,

public spaces and new buildings. They provide an expression of the vision for the waterfront, providing some flexibility in interpretation. Guidelines were provided for:

- Streets
- Walker Street - South of Main Street
- Harbour Street
- Pedestrian Routes
- Key Public Places
- The Pier
- Gateways
- Buildings
- Signage and Logo
- Parking Lots



Current Development Activity in Port Dover's Central Waterfront

## 4.1 Things to Think About

Based on the review of background information and the public input received so far, the following are things to think about as the team begins Phase 2: Concepts.

- Parking - determining the most appropriate number, type and location
- Pedestrian priority areas - clarifying the implications for traffic circulation and parking
- Determining the most appropriate form and character of new development
- Enabling public access to the water's edge
- Public ownership of the beach
- Celebrating the history and stories of Port Dover
- A range of housing to provide options in type and tenure for a all demographics – singles, couples, young families, seniors.
- Improved public access and amenities at the marina
- Consider including Silver Lake and it's parkland in the core waterfront lands
- Special policies for the river edge lands to the urban boundary

# 5 Natural Heritage

## 5.1 Introduction

The following provides an overview level description of the existing environmental conditions and opportunities/constraints to future urban development within the Port Dover Secondary Plan study area. A primary objective of the Secondary Plan should be to ensure there is no net loss of existing habitat cover within Port Dover.

The information provided was used to establish a defensible environmental protection/enhancement framework for the preparation of a land use concept plan and policies for Port Dover. The overview was based on existing background information sources obtained from the County of Norfolk, the Long Point Region Conservation Authority (LPRCA), the Land Information Ontario office, and the Natural Heritage Information Centre.

Port Dover is located on the north shore of Lake Erie in association with the Lynn River and Black Creek valleys. The secondary plan study area is mainly urban in character with cultivated agricultural land located along the west, east and north fringes of the village. Key natural heritage system features identified within the village include the following (Source: Norfolk County Official Plan 2020):

- Lake Erie and associated shoreline and beach ridge.
- Lynn River.
- Black Creek.
- Silver Lake.
- Black Creek Provincially Significant Wetland (PSW) Complex – in association with the Lynn River and Black Creek drainage systems.
- Significant woodlands (as defined by the County of Norfolk).

In addition, the following natural heritage system study areas are identified on Schedule G3 (Lakeshore Special Policy Area Secondary Plan-2009) of the Norfolk County Official Plan (2020):

- Unevaluated small forest patches study areas.
- Upland grass habitat study areas.
- Stopover habitat study areas (flanking the study area along the Lake Erie shoreline).

The County of Norfolk Official Plan (2020) separates environmental considerations into the following three categories:

- Provincially significant features, as defined by the Provincial Policy Statement (2020) and the companion Natural Heritage Reference Manual (2010).
- Natural heritage features.
- Hazard lands – as defined by the Long Point Region Conservation Authority policies and regulations (O. Reg. 178/06).

## 5.2 Existing Conditions

### Physiography, Drainage & Soils

The topography of the study area is shown below. The main topographic relief features are associated with the Lake Erie shoreline (beach scarp/ridge) and the major watercourses (e.g. Lynn River, Black Creek).

Port Dover is located within the following sub-watersheds that come under the jurisdiction of the Long Point Region Conservation Authority:

- Dedrick Creek-Young Creek (far western portion of the village)
- Lynn River-Black Creek (central core of the village)
- Fauries Creek-Stelco Creek (southeast corner of the village)

The location of the subwatersheds is shown on Schedule D1 (Source: Norfolk County Official Plan 2020).



#### LEGEND

	Study Area
	Watercourse
	Contour Line (1 m)

*Topography of Port Dover*

Soils within the study area are mapped below. The main soil properties and soil types within the study area consist of the following (Source: Present and Acton 1984 – *The Soils of the Regional Municipality of the Haldimand-Norfolk – Report No. 57 of the Ontario Institute of Pedology*):

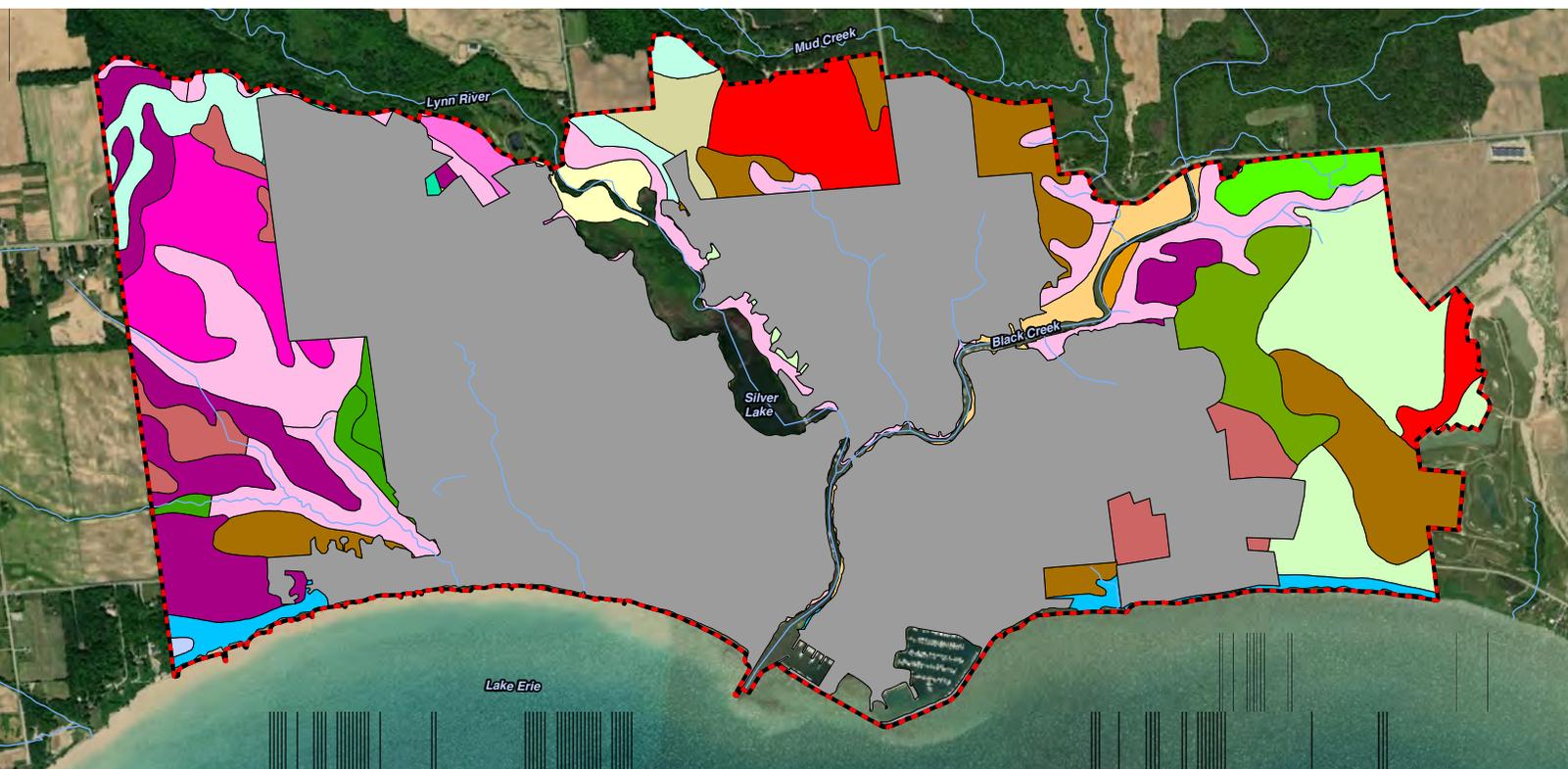
- Lacustrine silty clay (moderately well to poorly drained) – Brantford, Beverley, Toledo
- Lacustrine heavy clay (moderately well to poorly drained) – Haldimand, Lincoln
- Lacustrine silt loam (well drained) - Brant
- Sandy loam over clay loam (well drained) – Berrien, Bookton, Tavistock
- Lacustrine sand - wind modified (rapid-well to imperfectly drained) – Fox, Brady, Granby, Normandale, Watford
- Aeolian sands - often duned (rapid to well drained) - Plainfield

- Beach Ridge Complex (variable drainage) – Complex 1, Complex 6
- Alluvial floodplain deposits (variable drainage) – Alluvium 1

### Natural Resources

Natural resource features within the study area include the following (Source: Schedule J5 of the Norfolk County Official Plan, 2020):

- Closed Waste Disposal Site – east side of town.
- Bedrock resource area associated with Lynn River and Black Creek valleys.
- Suspended petroleum pools - west and east sectors of the Town.
- Abandoned petroleum pools - cover most of the Town south of County Rd. 6.



#### LEGEND

- Study Area
- Watercourse

#### Soil Type

##### Rapid to Well Drained

- FOX
- PLAINFIELD - DUNE PHASE

##### Well Drained

- BOOKTON
- BRANT
- WATTFORD

##### Moderately Well Drained

- BRANTFORD
- BRANTFORD - COARSE PHASE
- BRANTFORD - LOAMY PHASE

##### Imperfectly Drained

- BERRIEN
- BERRIEN - HEAVY CLAY PHASE
- BEVERLY - COARSE PHASE
- NORMANDALE
- TAVISTOCK

##### Variably Drained

- ALLUVIUM 1
- ALLUVIUM 1 - VERY SHALLOW PHASE
- BEACH
- BUILT UP AREA
- URBAN LAND

Soils in Port Dover

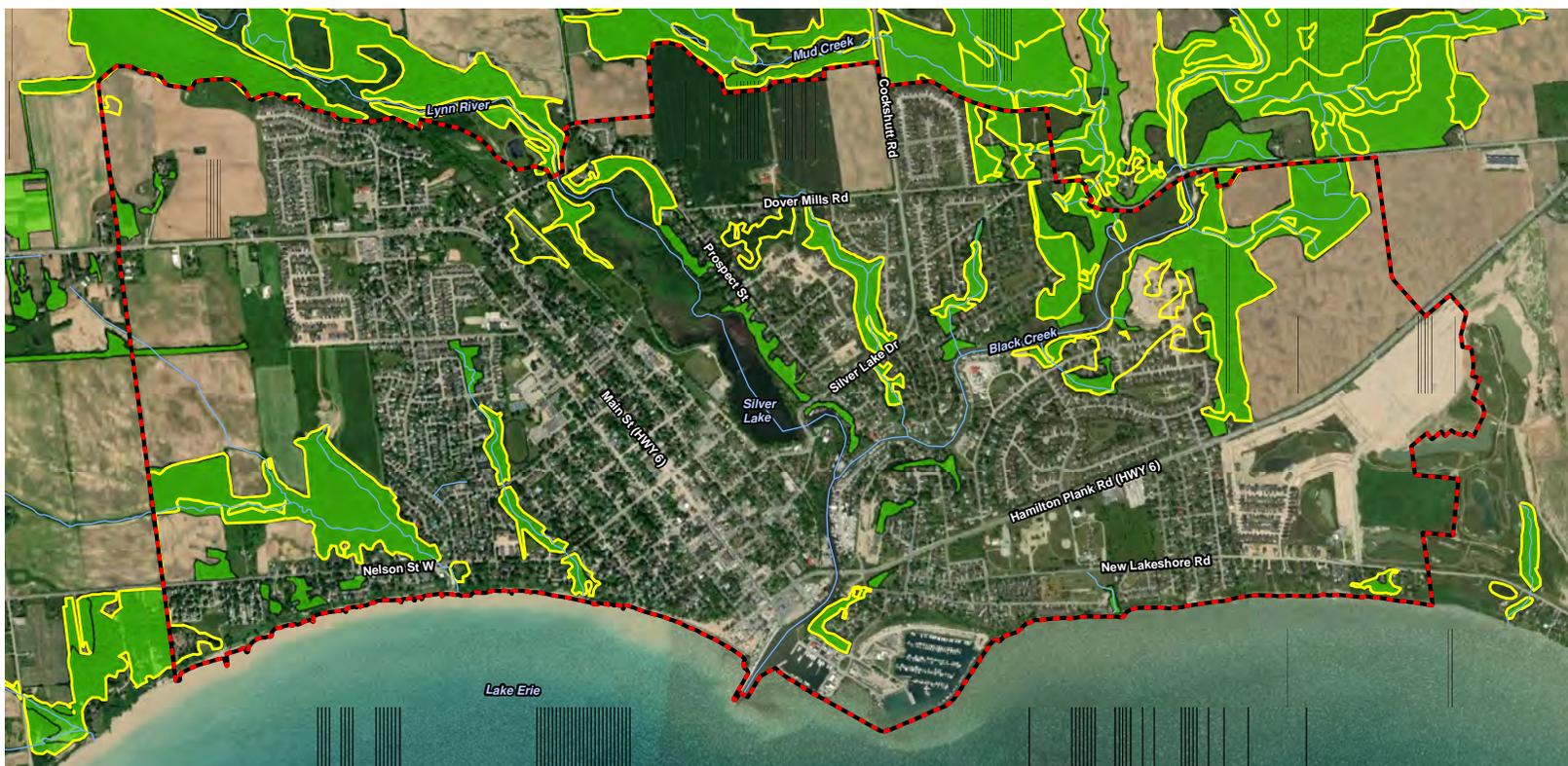
## Woodlands

Woodlands within the study area are mapped below. The woodlands are primarily associated with existing watercourses and their associated floodplains and/or defined valley systems. The main blocks of remnant forest habitat occur in the southwest and northeast sectors of Port Dover.

Woodlands within Port Dover are mainly deciduous in character and are comprised of species that are indicative of the Carolinian forest that occurs along the north shore of Lake Erie. Significant woodlands, as defined in the Norfolk County Official Plan (2020), are shown below with the yellow outline. The boundary of selective woodland features within the study area has been adjusted, where appropriate, to address previous development approvals or for

completeness and accuracy (based on aerial photo interpretation). The revised woodland boundaries are included on the map below.

It should be noted that woodland cover within the study area watersheds (i.e. Lynn River-13%, Black Creek-13%) is well below the Environment Canada (2013) minimum guideline of 30% coverage for healthy, sustainable watersheds (Source: LPRCA 2004 – Black Creek and Lynn River Watershed Report Card).



### LEGEND

	Study Area
	Watercourse
	Woodland
	Significant Woodland

*Woodlands and Significant Woodlands in Port Dover*

### Wetlands

Wetlands within the study area are mapped below. The wetland features are associated with the Lynn River and Black Creek drainage system and are part of the larger Black Creek PSW complex. Wetlands within the study area are comprised mainly of marsh and treed/thicket swamp communities.

Wetland coverage within the study area subwatershed (i.e. Lynn River/Black Creek) is well below the Environment Canada (2013) minimum target of 10% wetland cover for healthy watersheds.

### Valleylands

Valleyland features occur within the study area in association with the Lynn River, Black Creek and minor watercourses that drain into Lake Erie. The valleyland features are regulated by the LPRCA and generally support woodland and wetland features on slopes and bottomlands.



#### LEGEND

-  Study Area
-  Watercourse
-  Provincially Significant Wetland

Wetlands in Port Dover

## Hazard Lands

LPRCA regulated hazard land features within the study area are shown on the map below. Extensive hazard lands are associated with the major watercourses, low lying wetland areas, and the Lake Erie shoreline.

The following regulated features are shown:

### 100-Year Riverine Flooding Hazard Limit:

Defined as inundation along river, stream, and small inland lake systems. The flooding hazard limit is the one-hundred-year flood event, as approved by the Minister of the Natural Resources and Forestry, as the standard for the LPRCA watershed.

**Lake Erie Flooding Hazard Limit:** Defined as inundation along the shorelines of the Great Lakes - St. Lawrence River System and large inland lakes. The flooding hazard limit is based on the one-hundred-year flood event level plus an allowance for wave uprush and other water related hazards.

**Lake Erie Erosion Hazard Limit:** Defined as the loss of land, due to human or natural processes, that poses a threat to life and property. The erosion hazard limit is based on the 100-year erosion rate (i.e. the average annual rate of recession extended over a one-hundred-year time span) plus an allowance for slope stability. An allowance for erosion access has not been applied.



### LEGEND

	Study Area
	Watercourse
	100 Year Riverine Flooding Hazard Limit
	Lake Erie Dynamic Beach Hazard Limit
	Lake Erie Erosion Hazard Limit
	Lake Erie Flooding Hazard Limit
	LPRCA Regulated Area (20 Nov. 2019)

*Hazard Lands in Port Dover*

**Dynamic Beach Hazard:** Defined as areas of inherently unstable accumulations of shoreline sediments along the Great Lakes - St. Lawrence River System and large inland lakes, as identified by provincial standards, and as amended from time to time. The dynamic beach hazard limit consists of the flooding hazard limit plus a dynamic beach allowance.

The Lake Erie flooding hazard limit generally extends to the base of the beach scarp/ridge along the shoreline. The erosion hazard limit for Lake Erie extends approximately 100 m inland from the shoreline. A small area of dynamic beach hazard occurs to the west of the confluence of the Lynn River with Lake Erie.

The 100-year riverine flooding hazard is identified for the Lynn River and Black Creek. The LPRCA regulation limit for Lake Erie and watercourses/wetlands within the study area is also shown. These features are regulated by the LPRCA under O. Reg. 178/06 and its associated policies.

### Source Water Protection

There are no Highly Vulnerable Aquifers (HVA) or Source Water Protection Areas identified within the study area (Source: Schedule D of the County of Norfolk Official Plan, 2020). A Significant Groundwater Recharge Area (SGRA) however occurs in the extreme northwest corner of the study area, in association with the Lynn River subwatershed.

### Species at Risk

Based on a review of the Natural Heritage Information Centre data records, the study area has the potential to provide significant habitat for several species protected under the Endangered Species Act. Species at risk previously recorded within Port Dover include amphibians, reptiles, birds, fish, mussels, insects, and mammals (bats). Numerous provincially rare plant species have also been previously recorded within the study area.

Potential habitat for species at risk (including provincially/regionally rare species) within the study area is primarily associated with remnant woodlands and wetlands, Lynn River, Black Creek and Lake Erie. Cultural habitat

features such as old field meadows, shrub thickets, woodlands, and hedgerows also have the potential to support species at risk (e.g. grassland birds).

### Fish Habitat

The study area supports direct and indirect fish habitat features/functions associated with the Lynn River and Black Creek drainage systems, minor watercourses, and Lake Erie. The subwatersheds within the study area all drain towards Lake Erie, which supports a diverse fish community.

Black Creek and the Lynn River support habitat for warmwater and coldwater species. Surface water quality conditions within Black Creek are poor because of deforestation in the watershed, agricultural/urban runoff, lack of riparian cover, increased water temperatures and lack of in-stream cover. Water quality conditions in the Lynn River are better compared to Black Creek because of abundant in-stream cover and good substrate conditions (Source: LPRCA Watershed Report Card). The Ivy and Misner dams in Port Dover pose a barrier to upstream fish movement along the Lynn River.

## 5.3 Natural Heritage System

### Norfolk County Official Plan (2020)

Natural heritage system features within the study area are shown on Schedule C5 of the County of Norfolk Official Plan (2020). The natural heritage system features identified on Schedule C5 include provincially significant wetlands and woodlands. Hazard lands associated with the Lake Erie shoreline and bluff, the Lynn River/Black Creek valleylands, and minor watercourses are shown on Schedule B16 of the Official Plan.

The Lakeshore Special Policy Area Secondary Plan (2009) identifies unevaluated small forest patch study areas in the southwest, northwest, central and east sectors of study area. Upland grass habitat study areas are identified in the southwest, north, and northeast sectors

of the study area. Although not identified in the Lakeshore Special Policy Area Secondary Plan (2009), the study area supports stopover habitat (and potential study areas) for migrating birds and pollinators, particularly the wooded bluffs along the Lake Erie shoreline plus the wetlands and forests associated with the Lynn River, Black Creek and other watercourses. The opportunities/constraints to future development within or adjacent to small forest patches and upland grass habitat study areas is to be confirmed through an Environmental Impact Study (EIS) to be prepared at the draft plan of subdivision stage.

The Natural Heritage System in Port Dover



#### LEGEND

 Study Area

#### Natural Heritage System Components

-  Watercourse
-  Significant Woodland (Norfolk County OP)
-  Provincially Significant Wetland
-  Lake Erie

#### Other Environmental Features

-  Woodlands (Small/Isolated)
-  Unevaluated Small Forest Patches Study Areas (Lakeshore Special Study Area Secondary Plan)
-  Upland Grassland Habitat Study Areas (Lakeshore Special Study Area Secondary Plan)
-  Stopover Habitat Study Area (Lakeshore Special Study Area Secondary Plan)

The various components that comprise the natural heritage system for Port Dover are shown on the map on the previous page. An overlay of the various hazard limits, floodplains and LPRCA regulation area are shown on the map below.

The Natural Heritage System in Port Dover with Hazard Lands overlay



LEGEND		Natural Heritage System Components		Other Environmental Features	
	Study Area		Watercourse		Lake Erie/100 Year Riverine Flooding Hazard Limit
	Significant Woodland (Norfolk County OP)		Provincially Significant Wetland		Woodlands (Small/Isolated)
	Lake Erie				Upland Grassland Habitat Study Areas (Lakeshore Special Study Area Secondary Plan)
					Stopover Habitat Study Area (Lakeshore Special Study Area Secondary Plan)

## 5.4 Things to Think About

Most of the things to think about are reflected in the current policy framework for Port Dover – policies in the Norfolk County Official Plan and the Lakeshore Special Policy Area Secondary Plan. Based on the review of background information and the public input received so far, the following are things to think about as the team begins Phase 2: Concepts:

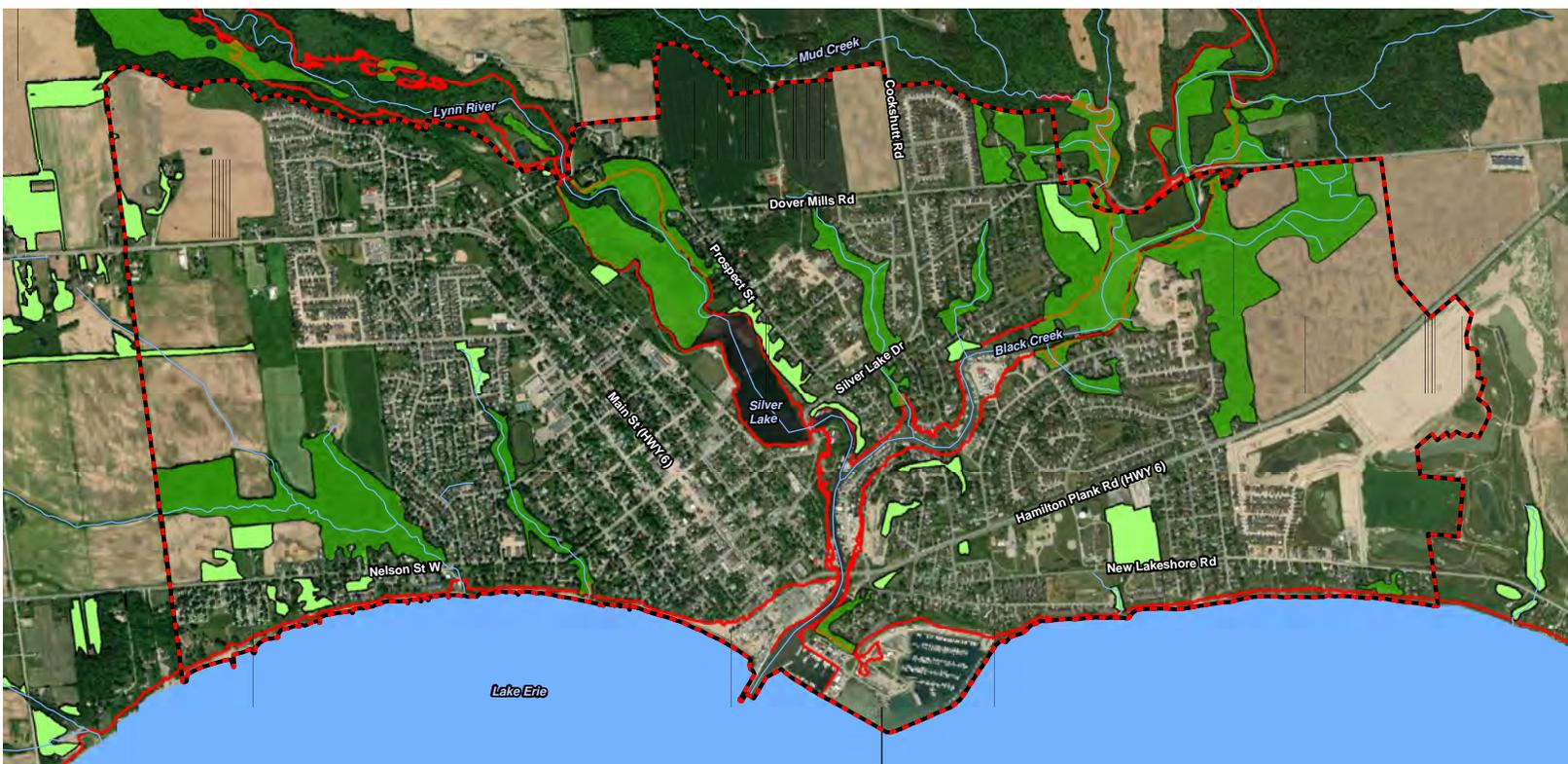
- A recommended natural heritage system (NHS) for Port Dover (see map below)

Minor refinements have been made to the boundary of various natural heritage system components to reflect current conditions and previous/current development approvals. The core

components of the recommended natural heritage system include the following environmental features and their related ecological functions:

- Provincially significant wetlands – Black Creek PSW complex.
- Provincially significant woodlands – as defined by the County of Norfolk (OP Schedule C6).
- Lake Erie shoreline and beach ridge/scarp – hazard lands.
- Floodplains associated with Lynn River and Black Creek – hazard lands.

*The Recommended Natural Heritage System in Port Dover*



### LEGEND

	Study Area
	Watercourse
	Lake Erie/100 Year Riverine Flooding Hazard Limit
	Lake Erie
	Natural Heritage System
	Other Environmental Features

Features not included as part of the natural heritage system have been identified as Other Environmental Features that require further investigation (i.e. EIS) and analysis to confirm opportunities/constraints to future development. Other Environmental Features within the study area include the following:

- Unevaluated small forest patches – as defined by the Lakeshore Special Policy Area Secondary Plan (2009).
  - Upland grass habitat – as defined by the Lakeshore Special Policy Area Secondary Plan (2009).
  - Small, isolated/fragmented woodlands and hedgerows not identified on Schedule B and C of the Official Plan, and Schedule G3 (Lakeshore Special Policy Area Secondary Plan).
  - Cultural habitat features such as old field meadow and shrub thicket.
  - Minor watercourses (LPRCA regulated feature).
- Development applications within or adjacent to a Natural Heritage System feature or Other Environmental Feature will be subject to the completion of an EIS to confirm existing conditions, constraint level, potential impacts, mitigation measures, and habitat compensation/off-setting opportunities. The terms of reference for the EIS should be developed in consultation with Norfolk County and LPRCA staff.
  - **NHS Buffers**  
To protect and enhance NHS features and functions for the long-term, a 30 m buffer is recommended. The application of 30 m buffers from NHS features is a widely recognized practice in Southern Ontario and is consistent with the natural heritage policies of the Growth Plan (2019) and the Greenbelt Plan (2017), among other policy documents.

A 30 m buffer is also recommended from the edge of watercourses to promote the protection of existing riparian vegetation, and to help achieve the Environment Canada (2013) target of a minimum of 75% vegetated riparian cover.

Buffers help to reduce the impacts of adjacent land uses on the ecological function of natural heritage features and assist in the creation of additional habitat. The naturalization of 30 m buffers, hazard lands, and linkage connections will provide a high level of protection and enhancement for Port Dover's NHS.

Consideration should be given to the application of flexible buffer widths for in-fill developments and conversion of existing developed parcels of land. Ecological off setting (i.e. compensation) is recommended for buffer reduction, where suitable alternatives are not feasible.

- **Restoration and Enhancement Opportunities**

Restoration and enhancement opportunities within the study area consist of the following:

- NHS buffers
- Lake Erie shoreline bluff
- Riparian floodplain zones (Lynn River, Black Creek, minor watercourses)
- Streetscape Plantings
- Urban parks

The above considerations provide an excellent opportunity to augment the existing bio-diversity within Port Dover and provide a higher level of protection for the NHS from the potential impacts of urban land use. Locally indigenous plant species should be used for naturalization projects, in consultation with County and LPRCA staff.

- **Natural Heritage Policy Considerations**

The secondary plan policies for Port Dover should be consistent with the direction of the *Norfolk County Official Plan (2020)*, the *Lakeshore Special Policy Area Secondary Plan (County of Norfolk 2009)*, the *Provincial Policy Statement (2020)* and the *Policies for the Administration of the Development, Interference with Wetlands and Alterations to Shorelines and Watercourses Regulation Ontario Regulation 178/06 (LPRCA 2017)*.

Consideration should also be given to including a policy in the secondary plan related to ecological off setting to address potential land use conflicts associated with development proposals on vacant lots of record, infill developments, conversions, and development proposals within or adjacent to NHS Features or Other Environmental Features. The goal of the ecological off setting policy is to ensure that there is no net loss of existing habitat cover within Port Dover.

Given the overall significance/sensitivity of the Lynn River, Black Creek, PSW's and Lake Erie, future stormwater management measures should include Low Impact Development (LID) techniques to protect surface and groundwater quality and quantity. The soil conditions within Port Dover appear to be generally conducive to the application of LID stormwater management measures.

- **Future Study Requirements**

Development proposals within or adjacent to a NHS feature or an Other Environmental Feature will be subject to the preparation of an Environmental Impact Study (EIS). The terms of reference for the EIS should be prepared in consultation with County and LPRCA staff. One of the key requirements of the EIS will be to address ecological off setting (habitat compensation) requirements where habitat protection

is not entirely feasible or warranted (e.g. vacant lots of record, unevaluated small forest patch study areas, upland grass habitat study areas).

Port Dover contains several small/ isolated forest patches and upland grassland habitat that have the potential to support natural heritage system features/functions and habitat for species at risk protected under the *Endangered Species Act*. These features were initially identified in the *Lakeshore Special Policy Area Secondary Plan (2009)*. Refinements to the boundary of these features was completed as part of the current secondary plan exercise. These features, as shown in the maps in Sections 5.3 and 5.4, should be subject to an appropriate level of environmental investigation through an EIS to confirm opportunities/constraints to development and ecological off setting requirements.

Development applications within or adjacent to LPRCA regulated areas will also be subject to various technical investigations, as outlined in *Policies for the Administration of the Development, Interference with Wetlands and Alterations to Shorelines and Watercourses Regulation Ontario Regulation 178/06 (LPRCA 2017)*.

As noted above, the study area has the potential to support habitat for several species at risk. A key requirement for future development applications will be to complete a thorough species at risk screening exercise, in consultation with the Ministry of Environment, Conservation and Parks, as part of the EIS investigation.

# 6 Servicing

## 6.1 Water Servicing

In 2016 the Norfolk County Integrated Sustainable Master Plan (ISMP) report was finalized. Norfolk County Council adopted the Integrated Sustainable Master Plan (ISMP) as policy for the planning and design of the facilities and programs discussed therein.

The ISMP identified constraints regarding Port Dover's existing water supply. Port Dover has one surface water intake and one water treatment plant. The Port Dover Water Treatment Plant (PDWTP) license was updated in 2016 to a gross capacity of 7.4 ML/d. The "Practical Firm Capacity" (which assesses the ability to meet drinking water quality and distribution needs with the largest pump out of service) for the PDWTP is 2.5 ML/d. The facility is not operating at its peak capacity, the clarifier is in poor condition and has approximately 2 years of useful life remaining, 1 of the 3 filtration units has failed twice in the past 12 months which has required extensive repairs, and the water system is currently unable to meet the current peak water demands or the future water supply needs of the community.

In February 2020, it was recommended to Norfolk County Council that WSP be retained to complete the engineering design, tender documents, project management and construction inspection of the PDWTP clarifier replacement.

Due to the lack of sufficient water servicing capacity, Port Dover is currently under a Development Moratorium and an Interim Control By-Law. Presently, the community draws up to twice the available water volume during peak periods. No Building Permits are being issued in Port Dover where new water uses are proposed.

### Proposed Upgrades to Water Servicing

In the ISMP, the existing conditions, future conditions and implementation of preferred water and water solutions were reviewed. Based on the preferred solutions, WSP was retained to complete an Inter-Urban Water Supply - Water Supply Operational Strategy Report (IUWS). The intention of this study is to review and provide

recommendations with respect to the long-term strategy for establishing a safe and secure water supply for Norfolk County.

With respect to Port Dover, the County's priorities are as follows:

- 1 Securing the current supply of water to Port Dover
  - a. To accomplish the immediate security of water supply, the County is proceeding with the replacement of the clarifier unit in the water treatment plant (WTP).
  - b. This upgrade will not increase the capacity of the WTP.
- 2 Add to the current supply of water to Port Dover
  - a. To accomplish the increase of water supply, the County is proceeding with several opportunities to provide upgrades within the existing systems.
  - b. A preliminary engineering feasibility study is underway to determine requirements for a connection between the Nanticoke WTP and Port Dover to supplement capacity, or to entirely replace the Port Dover WTP.

In addition to the overall water supply priorities, the ISMP recommends addressing other needs as follows:

- Reduce rate of water demands increase by implementing Water Conservation methods
- Booster Pumping Station required for development in the Northwest corner of the distribution system.
- Interconnection between Port Dover and Simcoe – Simcoe would see the greatest value from this interconnection; however, the interconnection could also provide an emergency back-up supply to Port Dover, reducing risks of a plant shut-down due to frazil ice, or fire event with the elevated tank out of service.

The opportunities to establish a safe and secure water supply for Norfolk County include:

- Additional investment in the existing local water treatment facilities.
  - will provide redundancy in the County's water system by interconnecting local Town systems within the County of Norfolk
- The construction of a new County-owned lake-based water treatment facility to replace the existing local water treatment facilities.
  - will provide redundancy in the County's water system by interconnecting local Town systems within the County of Norfolk
  - the existing local water treatment facilities will be abandoned
- The potential purchase of water from an adjacent County (meetings are being held with Haldimand County regarding a connection to the Nanticoke Water Treatment Plant).
  - will provide redundancy in the County's water system by interconnecting local Town systems within the County of Norfolk
  - the existing local water treatment facilities will be abandoned

The constraints to establish a safe and secure water supply for Norfolk County include:

- The IUWS is anticipated to take considerable time to work through; urgency may become a deciding factor.
- Each option has associated costs; costs may become a deciding factor.
- Cooperation with Haldimand County is essential to the purchase and transportation (new watermain(s)) of water.

The IUWS is currently underway looking at water quality and quantity issues across the County and options for the County's consideration to address these concerns. It is anticipated that the preferred solutions for the long-term future of Norfolk County's water systems will be presented to Council in late 2020. The ISMP will require updating, upon completion of the IUWS.

## 6.2 Wastewater Servicing

Wastewater generated in Port Dover is conveyed to eight (8) existing wastewater pumping stations and pumped to the Port Dover Wastewater Treatment Plant (WWTP).

### Proposed Upgrades to Wastewater Servicing

#### Conveyance System

The ISMP assessed the capacity of the wastewater collection system and noted capacity deficiencies in the sewer system at three (3) locations and the following improvements were recommended:

- 1 Replacement of 25 m of existing 450 mm diameter sanitary sewer immediately upstream of the Nelson Pumping Station (PS 5) with a 600 mm diameter sanitary sewer.
- 2 Replacement of 31 m of 200 mm diameter sanitary sewer on Grace / Water Street with a new 300 mm diameter sanitary sewer. It is recommended the County confirm the diameter of the existing sanitary sewer before proceeding as the upstream and downstream sanitary sewers are 300 mm in diameter.
- 3 Increase firm capacity of the Don Jon Pumping Station to a firm capacity of 31 L/s.
- 4 County should consider upsizing the existing 250 mm diameter sanitary sewer on Main Street downstream of Greenock Street West to match the upstream 450 mm diameter sanitary sewer when replacement is required.

#### Pumping System

The ISMP also assessed the predicted peak flows and the total capacity at each of the eight pumping stations and concluded that all of the stations, except the Don Jon PS had sufficient firm and total capacity to pump peak flows under existing and proposed peak design conditions. The Don Jon PS had insufficient firm capacity to pump the existing and proposed design flows.

#### Wastewater Treatment Plant

A Class Environmental Assessment (EA) was completed in 2012 to assess the upgrades required to the Port Dover WWTP and identified expansion works to increase the rated capacity to 5,800 m<sup>3</sup>/d, based on the servicing needs to 2034. However, the ISMP identified that with the ISMP objective to address servicing needs to 2041, the WWTP rated capacity needs to be increased to 6,062 m<sup>3</sup>/d. As this capacity is greater than the value in the Class EA, an addendum to the Class EA would be required.

Norfolk County retained R. V. Anderson (RVA) to design the WWTP upgrades. In early 2019, the County advised RVA that the ongoing development in Port Dover was proceeding at a rate larger than anticipated. The approved and pending developments in 2018 accounted for 1,500 m<sup>3</sup>/day of wastewater base flow. Based on the current flow of 4,000 m<sup>3</sup>/day as of 2019, the plant would be at 90% of its rated capacity (5,500 of the 6,100 m<sup>3</sup>/day capacity) once the proposed upgrades are completed. This posed a threat of a development freeze and constructing the currently designed upgrades would be required to begin construction again in 2023 for the next required capacity increase.

RVA suggested that Chemically Enhanced Primary Treatment (CEPT) be considered as a method to increase the treatment capability of the Port Dover WWTP without significant modifications to the proposed infrastructure under the current project. RVA was directed through approval of SC#9 to complete a revised basis of design for the WWTP and prepare a design report summarizing the findings utilizing this CEPT approach. The 'Port Dover WWTP Upgrades Capacity Re-Rating' report was completed in June of 2019.

The report summarized the anticipated additional capacity which could be achieved with addition of CEPT and the required design changes necessary to facilitate this upgrade in the future. The intent of the required design changes is not to obtain approval for an increase in capacity, but to provide the core

infrastructure while constructing the current upgrades to facilitate the future increase in capacity without immediate replacement of the upgraded equipment and infrastructure. This ensures that capital resources are utilized effectively to support future growth in Port Dover.

In March 2020, the Ontario Ministry of the Environment Conservation and Parks (MECP) issued a draft Environmental Compliance Approval (ECA) for the proposed upgrades.

The County anticipates finalizing the engineering design completed by RVA; however, the process is currently on hold subject to a peer review. The Request for Proposal and subsequent proposals have been received for the peer review. A decision regarding the firm who will complete the peer review is imminent. The peer review will need to be complete prior to any decision regarding the proposed upgrades to the WWTP.

## 6.3 Stormwater Servicing

Port Dover is within the Lynn River and Black Creek watersheds. The existing community is serviced by a series of trunk storm sewer networks and a seven (7) existing stormwater management facilities, two (2) of which drain to the Lynn River, four (4) drain to Black Creek and one (1) drains directly to Lake Erie. The soils within Port Dover mostly comprised of silt and clay, with some minor sand and gravel.

### Proposed Stormwater Servicing

The ISMP identified that:

- Subwatershed studies are recommended for future development areas west of Port Dover. The subwatershed studies should consider the need for an erosion threshold assessment for deeply incised/confined receiving watercourses.
- Future development areas need to provide adequate stormwater management measures to alleviate flooding within new development areas and within existing development where cumulative impacts are expected.
- Upgrading and designing new storm sewer systems should take into consideration the receiving watercourse during times of flooding. The receiving watercourse has the potential to impact infrastructure by flooding and backwater effects.

Stormwater management measures for future development/redevelopment may include source (lot level) controls, conveyance controls and end-of-pipe controls to meet the water balance, water quality and water quantity (flooding and erosion) requirements.

Examples of potential source controls include: roof leader disconnection, infiltration trenches, bioretention, permeable driveways and green roofs.

Examples of potential conveyance controls include: bioswales, perforated pipe systems and vegetated filter strips.

Examples of potential end-of-pipe controls include: dry ponds, wet ponds, engineered wetlands, manufactured treatment devices (i.e. oil/grit separator, sand filter, etc.), parking lot ponding, and underground storage facilities.

The ISMP recommended short term (0-5 years) improvements involving:

- Upgrading of storm sewers with significant flooding concerns in six (6) areas.
- Undertaking maintenance at the existing seven (7) SWM facilities.
- Constructing one (1) new SWM facility where no stormwater is currently proposed.
- Retrofitting one (1) dry pond to improve stormwater quality management.

The ISMP recommended medium term (6-15 years) improvements involving:

- Upgrading of storm sewers with flooding concerns:
  - Main St. (Catchment PD8)
  - Denby Road (Catchment PD29)
- Constructing one (1) new SWM facility where no stormwater control is provided.
- Retrofitting one (1) dry pond to improve stormwater quality management.

The ISMP recommended long term (16-25 years) improvements involving:

- Upgrading of storm sewers with flooding concerns:
  - St. George St. (Catchment PD28)
  - Main St. (Catchment PD10)
- Construct one (1) new SWM facility where no stormwater control is provided.
- Retrofit one (1) dry pond to improve stormwater quality management.

# 7 Transportation

A review has been completed of existing Norfolk County policies, traffic data provided by the County, active development application materials, and area context with respect to the following key transportation considerations:

- Roadways
- Active Transportation (Walking, Cycling, Trails)
- Transit
- Parking

The two primary policy documents that were reviewed to guide the analysis included the:

- 1 Norfolk Official Plan (OP), adopted in 2006, and
- 2 Integrated Sustainable Master Plan (ISMP), 2016 that also included direction from the County's 2016 Active Transportation Strategy.

The BA Group also undertook a review of parking availability in downtown Port Dover. Figures as they relate to parking restrictions and supply are provided in

## 7.1 Roads

Downtown Port Dover is served by a limited number of entry points, notably:

- Highway 6 West, ~6,300-7,700 AADT
- Highway 6 East, ~7,500-10,000 AADT
- Cockshutt Road – Silver Lake Drive (north), ~2,200-2,800 AADT
- Nelson Street (west), 2500-3500 AADT
- New Lakeshore Road (east)

Key Norfolk County Official Plan policies include:

- *The **efficiency of the transportation network** should be maximized by coordinating transportation planning initiatives and activities with other levels of government and transportation agencies.*
- **Connectivity** of the transportation network within the County and crossing into adjacent jurisdictions shall be **maintained and, where possible and feasible, improved.**
- *The impact of a development proposal on the transportation system, including the means of access, shall be examined through a traffic impact study. **Only those development proposals that can be accommodated in the existing system will be permitted.** Where the transportation system is not adequate, the County shall require, as a condition of development approval, that the proponent of the development:*
  - improve the system in the vicinity of the proposed development without the County incurring any costs;*
  - make the necessary financial contributions for the required improvements; and/or*iii) *dedicate rights-of-way for the development of roads.*

*The cost of traffic impact studies and any other required supporting documentation shall be borne by the proponent. Costs incurred by the County in engaging peer*

*review consultants to evaluate the proposal and supporting submissions shall be reimbursed by the proponent.*

The Integrated Sustainable Master Plan (ISMP) identifies Cockshutt Road as the only corridor identified for road improvements north of Port Dover (turn lanes and signalization between Concession 12 Townsend and Dover Mills Rd)

The ISMP notably identifies that Highway 6 sees a significant increase in traffic, 60% increase from AADT to Summer ADT (~15,000 vehicles) and that planning to construct new infrastructure or widen roadways to accommodate seasonal flows is not a sustainable approach.

The analysis revealed the following observations:

- Downtown streets are limited in their capacity to be widened or modified to accommodate additional capacity and potential capacity improvements could be in direct conflict with retaining important downtown on-street parking supply. Full buildout of planned and vacant lands may result in more regularly occurring congested times.
- With the exception of roads planned in relation to proposed development areas and improvements to Cockshutt Road, neither the ISMP or the Official Plan identify new roads on vacant lands or any significant road improvements related to growth or operational issues within Port Dover.
- Current road capacity is limited by the limited number of ways in/out of the community. There is significant development potential within the unbuilt areas of Port Dover. If developed, this has the risk of adding congestion within the Town given the lack of planned new infrastructure.

### Things to Think About

Based on the review of background information and the public input received so far, the following are things to think about as the team begins Phase 2: Concepts:

- 1 Look at making permanent some of the event measures already in place on Friday the 13th, notably one-way roads.
- 2 Build on the existing road framework and the planned road networks identified in current development plans to identify logical future collector and arterial road connections on vacant lands to anticipate growth.
- 3 Consider development of a Transportation Master Plan for Port Dover and its connecting/supporting communities (such as Simcoe) that would:
  - identify key areas for operational intersection improvements
  - identify potential new road improvements related to growth beyond those already identified in the ISMP
  - identify the benefit of a mix of land uses in promoting a reduced number and length of trips, and
  - would permit planning for the introduction of further sustainable transportation modes should the community continue to grow within the unbuilt/unplanned areas of Port Dover

## 7.2 Active Transportation

The County, including Port Dover, is a destination for cycling tour routes, running routes, and recreational uses. There are gaps in the existing active transportation road network, notably between Lynn Valley Trail and the Waterfront Trail.

Key Norfolk County Official Plan policies include:

- *The **automobile will continue to be the main mode of transportation** within the County due to its predominantly rural character, small and dispersed population, and size of the Urban Areas. Notwithstanding this, a land use pattern, **density and mix of uses** shall be promoted in the Urban Areas that reduces growth in the length and number of vehicle trips, and creates the **potential for public transit viability and other alternative and sustainable transportation modes** such as walking and cycling.*
- *The **connectivity** between sidewalks and trails shall be promoted.*
- *The opportunity for cyclists to travel either on the roadway or on separated bicycle paths **shall be recognized in the design of streets.***
- The official plan identifies a number of north-south connections east and west of downtown and a number of routings are considered in the OP that may help facilitate closing the active transportation gap in downtown.

The Integrated Sustainable Master Plan (ISMP) identifies St. George Street bike lanes and signed routes on St. Patrick to close the gap between Lynn Valley Trail and Waterfront Trail. It also establishes a hierarchy of routes:

- Primary Routes: designated or separated facilities
- Secondary Routes: shared or designated facilities if spaces is available

The ISMP identifies gaps in the sidewalk network where there are either no sidewalks or sidewalks on only one side.

The analysis revealed the following observations:

- 1 Current ISMP Priorities do not address unplanned future development lands.
- 2 Existing design road design standards do not include cycling facilities
- 3 Downtown streets are limited in their ability to facilitate dedicated cycling infrastructure given the competition for space with parking and road capacity.

### Things to Think About

Based on the review of background information and the public input received so far, the following are things to think about as the team begins Phase 2: Concepts:

- 1 Provide clarity on cycling facilities and direction with regard to the ISMP priorities within downtown.
- 2 Incorporate cycling facilities into the road cross-sections for new development areas and identify location of new links within future development areas.
- 3 Identify high priority areas within new development areas where sidewalks on both sides and potential growth of vacant sites may warrant enhanced pedestrian connectivity and formalized crossings such as collector roads, community destinations, schools.

## 7.3 Transit

Norfolk County runs limited transit services on Thursday and Friday to/from the Simcoe Library and downtown Port Dover with connections to Brantford.

Key Norfolk County Official Plan policies include:

- *The County will explore opportunities to enhance transit services in partnership with community and/or government partners, including adjacent municipalities.*
- *Development within Urban Areas shall, where appropriate, take into consideration the safe and convenient provision of transit services.*
- *The continued need for transit services will be considered on an on-going basis.*

The analysis revealed that service is limited and the ability to grow transit service is constrained by the limited demand and cost of providing service.

### Things to Think About

Based on the review of background information and the public input received so far, the following are things to think about as the team begins Phase 2: Concepts:

- 1 Provide policies that support review of transit and growth in service including encouragement for routing opportunities that connect areas outside of downtown and support for reviewing advancements in service like micromobility options (Autonomous vehicles, dial-a-ride) that could make transit a more accessible option for an aging population.

## 7.4 Parking

There is approximately 1444 spaces downtown, over half of which are in private off-street facilities with a number of large private off-street facilities directly adjacent the beach / waterfront.

Parking is at capacity during peak times, but appears to have capacity during typical off-peak periods.

Key Norfolk County Official Plan policies include:

- The County will support the expansion and development of transportation, parking and telecommunications infrastructure to increase the locational advantage of existing and proposed business and employment uses.
- The County recognizes and supports the development of tourism uses within the Urban Areas and specifically the Downtowns that will encourage visitor stops, provided such uses do not detract from the principal functions and uses of these areas. Such initiatives may include support for the provision of parking infrastructure, the management of parking needs and public
- The provision of adequate convenient parking is important to maintaining the economic viability of Downtowns.



*transit to ensure a positive experience for people visiting Norfolk County's key tourism nodes.*

The analysis revealed that the parking occupies a great deal of space downtown and on the waterfront. We understand it is constrained at peak times. The majority of Municipally Controlled spaces are within 500 metres of the beach and downtown.

	<b>Parking Spots</b>	<b>Parking Spots per Acre</b>
<b>Port Dover</b>	<b>1,389</b>	<b>23</b>
Stratford	1,096	27
Port Credit	2,209	24
Huntsville	895	24
Kincardine	1,144	18
Cobourg	2,238	18
Listowel	1,064	17.5
Georgetown	524	15.5

*Downtown Area Parking Comparables*

### **Project for Public Spaces**

Project for Public Spaces is a central hub of global place-making connecting people to ideas and resources. For 40 years they have helped to transform places in 3000 communities in 47 countries.

***"nobody goes to a place because it has parking"***

***"the current obsession with parking is one of the biggest obstacles to achieving livable cities and towns"***

### **Things to Think About**

Based on the review of background information and the public input received so far, the following are things to think about as the team begins Phase 2: Concepts:

- 1** Establish a greater proportion of publicly controlled parking throughout downtown to serve business and tourism needs.
- 2** Establish a greater presence of municipal controlled parking in waterfront area and identify remote parking locations that, in combination with a shuttle service, could serve parking needs at peak times, support space for active transportation, and reduce the amount of underutilized surface parking in the beach area during off-peak times..

# **APPENDIX**

## **Existing Transportation Plan, Policy and By-Law Research (BA Group)**





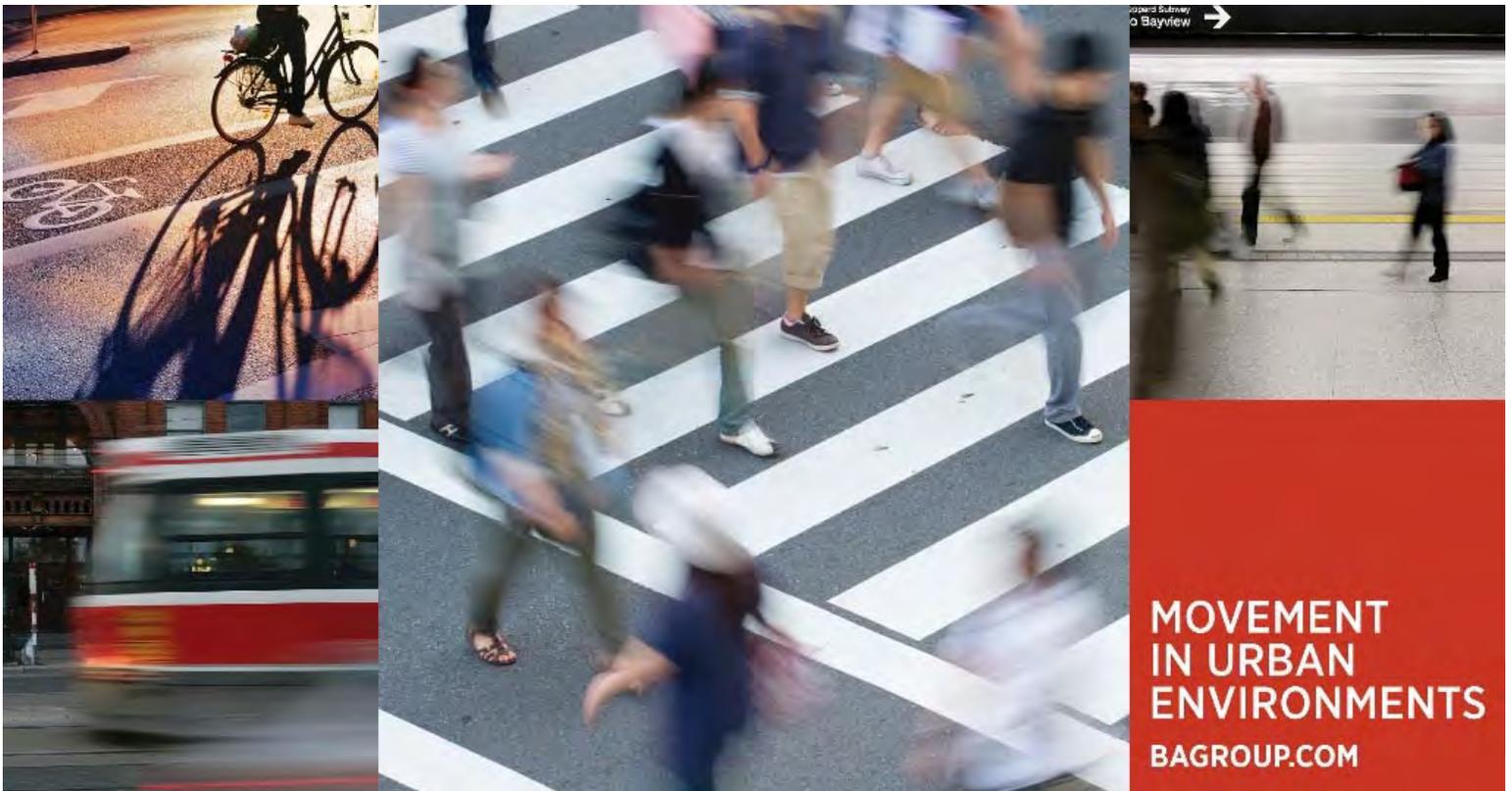
**BA Group**

# PORT DOVER SECONDARY PLAN

Existing Transportation Plan, Policy and By-Law Research (Draft)

Prepared For: Planning Partnership

July 2020



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## 1.1 THIS DOCUMENT

This document is a compendium of existing transportation policy and zoning bylaw information within Norfolk County, including where those policies and bylaws specifically refer to Port Dover.

Insights related to opportunities and constraints for the Port Dover Secondary Plan are provided under separate cover.

## 2.0 BACKGROUND DOCUMENTS

The following municipal planning documents were reviewed by BA Group to understand the transportation related policies and legislation applicable to the Port Dover Secondary Plan (PDSP) area.

- Norfolk County Official Plan, 2020 (Last Updated January 2020)
- Lakeshore Special Policy Area, 2009 (Consolidated October 2009)
- Integrated Sustainable Master Plan, 2016 (Consolidated September 2016)
- Active Transportation Strategy, 2016 (Consolidated August 2016)
- Norfolk County Trails Master Plan, 2009 (Consolidated October 2009)
- Parks, Facilities and Trails Master Plan, 2015 (Consolidated June 2015)
- Norfolk County Zoning By-law

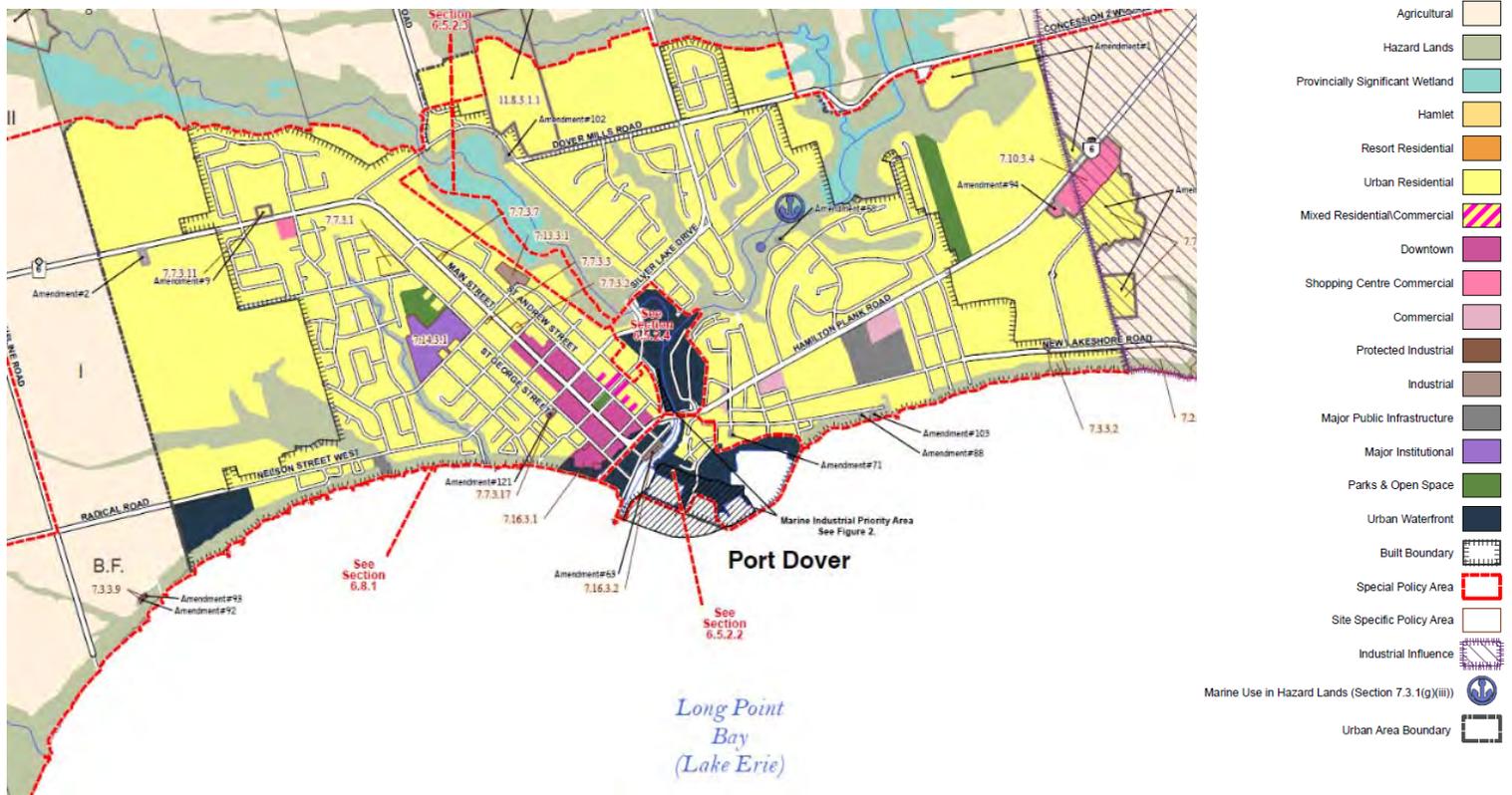
## 2.1 NORFOLK COUNTY OFFICIAL PLAN (NCOP)

The Norfolk County Official Plan (NCOP) identifies Port Dover as the second largest Urban Area in the County and a primary settlement area included in the Lakeshore Special Policy Area. These designations are described below.

*“Urban Areas...have historically functioned as the focal points for growth and development activity, as well as public and private sector investment. This role will continue in the future. The Urban Areas will accommodate the greatest amount of the targeted growth throughout the planning period, and will be the focus of residential, commercial, employment, government, institutional, office, entertainment, cultural, and health and social service activities.”*

*“Port Dover Urban Area – The County will support and promote the continued development of Port Dover as a significant urban waterfront community and tourism node in the County. The County will support and promote Port Dover as a sustainable waterfront urban community focused on port and lake-based activities, and containing an efficient pattern of development.”*

The Official Plan land-use designations within the Port Dover Urban Area are illustrated in **Figure 1**.



**FIGURE 1: EXCERPT FROM NCOP SCHEDULE B – LAND-USE**

## 2.1.1 NCOP – Transportation Related Policies

**TABLE 1 NORFOLK COUNTY OFFICIAL PLAN TRANSPORTATION POLICIES**

Chapter / Section Description	Transportation Related Policies
<p><b>3.6 Air Quality</b></p> <p>Air quality may be improved by reducing emissions of noxious gases, particulates, and dust. There are many emissions sources but primary contributors include industrial operations and motor vehicles.</p>	<p>The following shall be the policy of the County:</p> <p>a) The County encourages Provincial and Federal initiatives to develop and enforce improved emission standards for motor vehicles and industrial operations.</p> <p>g) The County shall strive to create a transit-supportive, compact urban form consisting of mixed uses and efficient transportation networks.</p>
<p><b>4.3 Locations of Economic Activity</b></p> <p>This Plan focuses specific types of economic activity in the environment best suited to their success.</p>	<p>The following shall be the policy of the County:</p> <p>b) Industrial land shall be focused in Urban Areas and will be located adjacent to or in close proximity to Provincial Highways and arterial roads, where an appropriate level of municipal services are provided.</p> <p>g) The County will support the expansion and development of transportation, parking and telecommunications infrastructure to increase the locational advantage of existing and proposed business and employment uses.</p>
<p><b>4.5 Tourism</b></p> <p>This Plan encourages growth in tourism and travel to the County, particularly to the Urban Areas, the Lake Erie shoreline, resort and recreation areas, and areas of agricultural, rural and natural amenity.</p>	<p>The following shall be the policy of the County:</p> <p>b) The County recognizes and supports the development of tourism uses within the Urban Areas and specifically the Downtowns that will encourage visitor stops, provided such uses do not detract from the principal functions and uses of these areas. Such initiatives may include support for:</p> <p style="padding-left: 40px;">ii) promotion of a multi-purpose trails system connecting Norfolk’s Urban Areas and natural amenities, including Simcoe as a destination on the recreational trail/utility corridor linking Simcoe and Port Dover, a waterfront trail linking Port Dover to Long Point, the Trans Canada Trail, the Simcoe-Waterford-Brantford trail and the Delhi-Simcoe-Brantford trail;</p> <p style="padding-left: 40px;">ix) the provision of parking infrastructure, the management of parking needs and public transit to ensure a positive experience for people visiting Norfolk County’s key tourism nodes.</p> <p>f) The County shall consider opportunities to establish the existing lakeshore roads as a parkway, providing a multi-purpose route along the waterfront.</p> <p>k) The County supports the development of appropriate infrastructure to accommodate visitors to Downtowns, the lakeshore and other tourism attractions throughout the County. Tourism infrastructure may include bicycle and vehicle parking, washroom facilities and handwashing facilities, picnic areas, boating slips and marinas.</p>
<p><b>4.7 Employment Activity</b></p>	<p>The following shall be the policy of the County:</p>

Chapter / Section Description	Transportation Related Policies
<p>The County shall maintain an appropriate employment land supply and shall use the planning policy framework of this Plan to encourage and support new industrial technologies, new employment sector development and local employment initiatives.</p>	<p>e) Council shall co-operate with the business community to ensure that Protected Industrial areas are well served by appropriate infrastructure, including municipal services, roads and modern telecommunication technologies.</p>
<p><b>5.0 Maintaining Healthy Communities</b></p> <p>Coupled with the sections of this Plan dealing with natural heritage, economic vitality, land use, growth management and infrastructure, the policies of this Section promote healthy communities through public services, community design, recreation, social planning, cultural heritage, housing and access to healthy food.</p>	
<p><b>5.3.1 Residential Intensification</b></p> <p>Urban residential intensification, infilling and redevelopment of existing areas allows for the efficient provision of urban services thereby helping to minimize the costs of provided services while meeting an important component of the County's housing needs.</p>	<p>The following shall be the policy of the County:</p> <p>f) The County shall consider applications for infill development, intensification and redevelopment of sites and buildings through intensification based on the following criteria:</p> <p>iii) the road network can accommodate the traffic generated</p>
<p><b>5.4 Community Design</b></p> <p>Safe and attractive neighbourhoods contribute to the overall community health of the County. Excellence in community design is essential to creating a physical environment where people have the appropriate places to interact, live, work, recreate and learn. The following policies relate to the physical design of communities, including new applications within the County for development, such as plans of subdivision, infill development proposals, and site plans.</p>	<p>The following shall be the policy of the County:</p> <p>b) Through the review of development applications, including plans of subdivision, site plans and other development proposals, the County:</p> <p>iii) shall promote the improvement of the physical character, appearance and safety of streetscapes, civic spaces, and parks;</p> <p>vi) shall strongly encourage design that considers and, wherever possible, continues existing and traditional street patterns and neighbourhood structure; and</p> <p>vii) may require, at the County's sole discretion, that proponents submit design guidelines with development applications, establishing how the policies of this Section have been considered and addressed. Such guidelines may also be required to address related issues of residential streetscaping, landscaping, setbacks, sidewalks, signage, garage placement, and architectural treatment.</p> <p>d) Development design that establishes reverse lotting on Provincial Highways and County Roads will not be permitted. Development design that requires features such as noise attenuation or privacy fencing will be discouraged. Wherever possible, new development will be oriented toward streets or parks.</p> <p>g) Streetscaping that reflects the intended character of settlement areas is encouraged. In particular, traditional streetscaping in the Downtown Designations of the Urban Areas will be encouraged.</p>

Chapter / Section Description	Transportation Related Policies
	<p>h) A high quality of park and open space design is strongly encouraged. The land for parkland dedication shall be carefully selected to facilitate their use as a central focal point for new or existing neighbourhoods.</p> <p>j) The County may require the provision of certain pedestrian, cycling and trail linkages through the development approvals process.</p> <p>k) The County, in consultation with a development proponent(s) and the Norfolk Heritage Committee, shall define a style of street furnishing that should include shared and accessible bicycle racks, garbage receptacles, benches and street lamps to be used in a new development.</p> <p>n) To promote environmental sustainable development, the County shall encourage the design of sustainable neighbourhoods in keeping with Leadership in Energy and Environmental Design - Neighbourhood Development (LEED ND) design principles in accordance with the policies under Section 11.8.2.1 Sustainable Neighbourhood Design of the Lakeshore Special Policy Area Secondary Plan.</p> <p>o) The County shall review site plans and drawings submitted in accordance with Section 41 of the Planning Act and Section 9.6.5 (Site Plan Control of this Plan) regarding accessibility for persons with disabilities including but not limited to areas of accessible parking, exterior paths of travel, lighting, ramps, entrances and street furniture.</p>
<p><b>5.6 Recreation</b></p> <p>The provision of a variety of recreational opportunities is critical to maintaining healthy communities in Norfolk County. The Plan strives to ensure that sufficient recreational, open space and park facilities are provided within the County to meet the leisure needs and desires of the present and future residents, businesses, as well as visitors to Norfolk County.</p> <p><b>5.6.1 Parks</b></p> <p>The County shall carry out programs required to acquire new parks, improve existing parks and facilities and provide public parks to meet the needs of the community, as well as to address existing park deficiencies. Parks, community recreation centres and trails should be located so that they are easily accessible from residential neighbourhoods, preferably by walking by the following policies:</p> <p><b>5.6.1.2 Neighbourhood Parks</b></p> <p><b>5.6.1.3 Community Parks</b></p> <p><b>5.6.1.4 Lakeshore Parks</b></p>	<p>The following shall be the policy of the County:</p> <p>a) The County shall promote appropriate recreational development in parks, open spaces, and other similar areas of the County that provide opportunities for active, passive and programmed community recreation and leisure, and contribute to the preservation and protection of open space and the natural environment.</p> <p><b>5.6.1</b> The following shall be the policy of the County:</p> <p>a) The County shall secure the maximum benefit of the Planning Act with respect to parkland dedication from development. Parkland dedication shall be conducted in accordance with Section 9.10.5 (Parkland Dedication) of this Plan.</p> <p>b) Provision for parks shall generally be in accordance with the standards provided in Section ..5.6.1.1, 5.6.1.2, 5.6.1.3, and 5.6.1.4.</p> <p>c) Where parks are located adjacent to existing and proposed residential areas, appropriate measures may be taken to minimize potential adverse effects associated with recreation activity areas and parking areas.</p> <p>d) Waterfront park needs shall be considered in conjunction with future recommendations and planning activities associated with the Lakeshore Special Policy Area and the major river valley systems.</p>

Chapter / Section Description	Transportation Related Policies
<p><b>5.6.2 Linked to Open Space and Natural Heritage System</b></p> <p>It is the County's intention to create a linked parks and open space system, where possible.</p>	<p><b>5.6.1.2</b> requires that the neighborhood parks be designed to accommodate the recreational needs of local residents. Facilities should be tailored more specifically to the particular needs and composition of the neighbourhood, and linked to other public neighbourhood uses. Neighbourhood parks should be centrally located within safe and convenient walking distance of the majority of neighbourhood residents, with an optimum service radius of 800 metres or a 10 minute walking distance. The park should be designed with extensive street frontage for visibility and safety and should be coordinated with elementary school sites where possible. Backlotting onto neighbourhood parks will be discouraged. Neighbourhood parks may include a limited amount of off-street parking. Neighbourhood parks should be of sufficient size to accommodate a variety of recreational facilities. These may include, but are not limited to junior sports fields (softball, soccer), outdoor rinks, multi-purpose courts, playground equipment, paved areas for informal games and shaded areas for passive recreation. Sports fields within neighbourhood parks should not be lit due to the potential impacts on adjacent residential areas. Trails and paths through neighbourhood parks and linking public uses may be lit.</p> <p><b>5.6.1.3</b> requires that community parks be designed to accommodate the social, cultural, education and physical activities of particular interest to several neighbourhoods with emphasis on facilities for organized sports, both indoor and outdoor. Smaller areas should be set aside for passive recreation. The community park should be located in a manner which is reasonably central to the service population and directly accessible by automobile and bicycle with an optimum service radius is 1.6 kilometres or a 20 minute walking distance. Frontage on a major traffic route is desirable with facilities provided for off-street parking. Backlotting onto community parks will be discouraged. The provision of community level parks should be coordinated with secondary school sites where possible. The parks should be developed as focal points for organized and non-organized team sports, athletic activities and casual recreation. This requires the provision of high quality sophisticated facilities, which may include regulation-size softball and baseball diamonds, soccer fields with night lighting and spectator facilities, major playgrounds, tennis courts, and visitor service centres.</p> <p><b>5.6.1.4</b> requires that lakeshore parks be designed and managed for the enjoyment of a diverse population including individuals, family groups, tourists and community organizations. Opportunities for both active and passive recreation, social and cultural activities, and the conservation and protection of Natural Heritage Features and Provincially Significant Features, historic sites and wildlife habitats should be encouraged. Lakeshore parks should be developed to capitalize on the unique waterfront setting, preserving access, vistas, views and lookouts to the waterfront. Facilities for residents and visitors should be provided. Development may include boat docking and launching, seating and boardwalks, playgrounds, informal sports fields, picnic shelters, interpretative features and passive recreation areas.</p>

Chapter / Section Description	Transportation Related Policies
	<p><b>5.6.2</b> In addition to those options for the acquisition of land outlined in Section 6.4.3 (Natural Heritage System Strategy), the County may create linked open space through the integration of:</p> <ul style="list-style-type: none"> <li>b) abandoned rail lines in public ownership;</li> <li>c) existing rights-of-way;</li> <li>d) established and proposed service and utility corridors;</li> <li>e) existing parkland;</li> <li>f) linkages provided through the draft plan of subdivision approval process and site plan control process;</li> <li>g) agreements with private land owners;</li> <li>h) retention or acquisition of access easements; and</li> <li>i) land acquisition.</li> </ul> <p>Throughout the County there are railway lands that have been linked together to form a continuous recreational trail system. The County may expand this trails networks as opportunities come available.</p> <p>Land deemed by the County to be significant to the linear park system shall be retained in public ownership for the purpose of expanding the system. The County shall also investigate the feasibility of acquiring portions of the railway corridors currently in private ownership.</p> <p>Design principles should be established to accommodate parking at strategic locations along the linked system.</p> <p>The Norfolk Environmental Advisory Committee shall be partially responsible for municipal initiatives in relation to the policies of this Section.</p>
<p><b>6.0 Managing Growth</b></p> <p>The vision of sustainability forms the basis for the growth management principles of this Plan. The following outlines a growth management approach that establishes growth and development policies and urban structures for the six Urban Areas, provides a management and monitoring framework for the 42 Hamlet Areas, ensures the long-term protection of the County's agricultural and rural resources, conserves natural heritage features and provides a basis for future planning activities along the lakeshore.</p>	
<p><b>6.2 Targeting Growth</b></p> <p>Norfolk County's population has historically increased at a very moderate rate. The</p>	

Chapter / Section Description	Transportation Related Policies
<p>population, household and employment forecasts in this Plan are based on achieving the County's proactive economic development and tourism objectives.</p> <p>Projections of population and employment growth are based on the assumptions relating to historical population and demographic patterns, fertility and mortality rates, migration trends, housing-related demographic trends and employments levels.</p> <p>Changes in the economy or federal immigration policies can have a significant impact on the population and employment forecasts. The policies of the Plan recognize that the forecasts are partially dependent on factors outside of the County's control. Consequently, the County's planning and growth management activities will ensure flexibility to accommodate growth, but will avoid inefficient and costly development patterns.</p>	
<p><b>6.3 Community Structure and Growth Framework</b></p> <p>Growth management and settlement structure of the County, which is divided into two major interrelated parts: the Settlement Areas and the Rural Area. The Settlement Areas include two components:</p> <ul style="list-style-type: none"> <li>i) Urban Areas; and</li> <li>ii) Rural Settlement Areas which are comprised of two sub-components: <ul style="list-style-type: none"> <li>Hamlet Areas; and</li> <li>Resort Areas.</li> </ul> </li> </ul> <p>b) Schedule "A" illustrates the Community Structure. Two policy areas that overlay this framework: the Industrial Influence Area; and the Lakeshore Special Policy Area.</p> <p>Population, household and employment forecasts to 2036 for each of the six urban areas and for the combined hamlet and rural areas of the County are provided in Table 5. The forecast growth to 2036, as allocated in Table 5, can be accommodated within each of the urban areas and the combined hamlet and rural areas.</p> <p>The County will regularly update its population, household and employment forecasts and monitor the supply of potential development lands in the urban area, hamlets and rural areas to ensure that the anticipated growth can be accommodated.</p>	

Chapter / Section Description	Transportation Related Policies
<p><b>6.4 Urban Areas</b></p> <p>The six Urban Areas within the County have historically functioned as the focal points for growth and development activity, as well as public and private sector investment. This role will continue in the future. The Urban Areas will accommodate the greatest amount of the targeted growth throughout the planning period, and will be the focus of residential, commercial, employment, government, institutional, office, entertainment, cultural, and health and social service activities.</p> <p><b>6.4.1 Downtown Areas</b></p> <p>The Downtown Areas are identified as a specific land use designation on Schedule “B” to this Plan. The planned function of Downtown Areas shall be to serve as primary activity centres for the County and the location for a wide range of uses, including retail, service commercial, cultural, recreational, entertainment, business and professional, governmental, institutional, arts and cultural, community, employment, and residential uses. Downtowns are the historic heart of the community and serve as a primary gathering place and location for events and festivities that give identity to the community. The diversity of land uses shall be promoted to foster the health and continued growth of the Downtown Areas, while conserving and enhancing cultural heritage resources and community identity. It is intended that the planned function of the Downtowns will be maintained as the County’s Urban Areas continue to grow and develop.</p>	<p>The following shall be the policy of the County:</p> <p>b) It is the policy of this Plan that the Urban Areas will incorporate the following:</p> <ul style="list-style-type: none"> <li>iii) full municipal services, as feasible and appropriate, and an appropriate level of transportation infrastructure;</li> <li>iv) a concentration of community services for the County, including social, cultural, entertainment, health, educational and other supporting facilities; and</li> <li>v) an open space, natural heritage and recreational network that is integrated with open spaces throughout the County, and provides appropriate passive, natural and active areas.</li> </ul> <p>c) The County shall ensure through its planning activities that each Urban Area develops with efficient land use patterns that minimize the extension of municipal services and infrastructure and will sustain the community and financial well-being of the County over the long-term.</p> <p>e) Boundaries of the Urban Areas are established on Schedules “B” and “G” to this Plan. Within these boundaries there is sufficient land to accommodate projected growth and development for the 20-year planning horizon to 2036.</p> <p>f) Subject to the policies of this Plan, the boundaries of the Urban Areas shall be permitted to expand as-needed, based on a demonstrated lack of available designated land and development trends. Proposals to expand the boundaries of an existing Urban Area shall be considered through a comprehensive review of this Plan. Proposals shall be considered in the context of whether:</p> <ul style="list-style-type: none"> <li>iv) the proposed expansion will have a compact form, an appropriate mix of land uses, where practical, and densities that efficiently use land, infrastructure, and public facilities, while providing for adequate parks and open spaces;</li> <li>v) the transportation network can reasonably accommodate the additional volume of traffic and demand for services;</li> </ul> <p>i) Development of vacant land within the Urban Area boundary on the edge of the existing built-up portions of the Urban Areas shall include efficient provision of water supply, sanitary sewers, roads, parks, schools, and other public, community and municipal services. Development shall proceed in a staged and sequential manner, considering the existing pattern of the Urban Area.</p> <p>j) While specific land uses are identified and delineated for each of the Urban Areas in Section 7 (Managing Land Use) of this Plan, the County may choose to undertake a detailed Secondary Plan for an Urban Area, in accordance with Section 9.5.1 (Secondary Plans) of this Plan. A Secondary Plan may be prepared to comprehensively address the arrangement of the urban development pattern for new development areas, to rationalize development within the existing Urban Area, to promote redevelopment or intensification within an</p>

Chapter / Section Description	Transportation Related Policies
	<p>area, or for any other reason identified by Council. A Secondary Plan may also be prepared to address specific land use issues or implement a specific planning initiative, thereby providing additional guidance to private and public sector investment in the Urban Area. At the County's discretion, a Secondary Plan may be prepared for a portion of an Urban Area, provided the Secondary Plan is comprehensive in scope and considers matters of integration with adjacent urban land.</p> <p><b>6.4.1</b> The following shall be the policy of the County:</p> <p>e) The County shall protect and enhance, where possible and appropriate, the historic architectural character of the Downtown Areas and main street areas, particularly focusing on the pedestrian scale and design of the buildings and streetscapes, and the arrangement of windows and treatments on the building facades.</p> <p>f) The County may undertake, by both direct municipal action and by encouraging the actions of other parties, a program of on-going improvements in the Downtown Areas, including streetscape improvements, facilities for off-street and on-street parking, improved vehicular connections and circulation patterns, and facilities for those not traveling by private automobile, including improvements for pedestrians, cyclists, the elderly and physically-challenged, and those using taxis, transportation for the disabled and transit services.</p> <p>i) The County shall encourage the development, redevelopment and rehabilitation of the Downtown Areas. To encourage this, the County may undertake a downtown revitalization strategy for each of the County's Downtown Areas. The revitalization strategies may include design guidelines that respect the unique physical character of each Downtown Area and which will assist in shaping public and private investments. The revitalization strategies will also include a strategy for the provision and management of parking in the Downtown Area.</p> <p>j) Linkages between the Downtown Areas and recreation trails shall be encouraged to foster pedestrian activity and encourage tourism in the Downtown Areas.</p>
<p><b>6.5 Specific Urban Areas</b></p> <p><b>6.5.2 Port Dover Urban Area</b></p> <p>Port Dover is the second largest Urban Area in the County. The County will support and promote the continued development of Port Dover as a significant urban waterfront community and tourism node in the County. The County will support and promote Port Dover as a sustainable waterfront urban community focussed on port and lake-based activities, and containing an efficient pattern of development. The policies of the Plan, however, recognize the inherent flooding</p>	<p><b>6.5.2.2</b> The following policies are intended to complement the general development policies and are more specific to the Port Dover Waterfront Area:</p> <p>a) The Port Dover Waterfront Area is intended to evolve over time as a mixed-use area.</p> <p>b) A Community Improvement Project Area may be designated and delineated to permit the development of a Community Improvement Plan in accordance with Section 9.5.2 (Community Improvement) of this Plan.</p> <p>c) The LSPA Community Design Guidelines provide general design guidelines related to the entire Urban Area of Port Dover, including the Port Dover Waterfront Area. Development within the Port Dover Waterfront Special Policy Area is also subject to the waterfront</p>

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<p>hazards in the Port Dover Urban Area emanating from Lake Erie and the Lynn River.</p> <p><b>6.5.2.2 Port Dover Waterfront Special Policy Area</b></p> <p>The Port Dover Waterfront Area is an important tourism and economic resources. The area will continue to fulfill its multiple roles as a tourist attraction, a working commercial port, an international gateway and a recreational and cultural resources for the County.</p> <p>Port Dover Waterfront Special Policy Area will provide direction to maintain and support tourism, the commercial fishery and provides recreational, commercial and cultural opportunities for residents. To achieve this, the policies of the Plan shall promote:</p> <ul style="list-style-type: none"> <li>• public accessibility to the shorelines and to, from and within the Area through the possible establishment of a boardwalk and/or waterfront promenade;</li> <li>• attractive and diverse public and private sector development that is compatible with the character and charm of the existing community; and</li> <li>• the protection and enhancement of the Lynn River and Lake Erie shoreline.</li> </ul> <p><b>6.5.2.3 Silver Lake Special Policy Area</b></p> <p>The Silver Lake Special Policy Area is an area in transition which has traditionally included certain industrial and open space uses. There is the potential to create a specific linkage between the Downtown Area of Port Dover with the open space areas and the Lynn Valley Trail. It is the policy of this Plan to encourage a transition to open space and recreational uses in this area.</p> <p><b>6.5.2.4 Lynn River / Black Creek Special Policy Area</b></p> <p>The Lynn River/Black Creek Special Policy Area has the potential to support a variety of interests relating to tourism, recreation and innovative residential development, while at the same time recognizing that the established marine industries will remain. A large portion of the Lynn River/Black Creek Special Policy Area are considered Hazard Lands due to</p>	<p>master plan and urban design guidelines outlined in the Port Dover Waterfront Master Plan and Secondary Plan (January, 2001). These guidelines are referenced within the LSPA Community Design Guidelines and must be read in conjunction with the LSPA Community Design Guidelines. The County will review and evaluate development proposals, with regard to the LSPA Community Design Guidelines. The design guidelines will be implemented through the Zoning By-law, site plan control and development agreements. General design policies that apply to all development in all land use designations within the Port Dover Waterfront Area include:</p> <ul style="list-style-type: none"> <li>i) buildings should be designed to front adjacent streets and to provide interest and comfort at ground level for pedestrians. In instances where corner lots or through lots are provided, buildings shall be designed so that all elevations facing a street present a 'front' elevation. The Zoning By-law will establish appropriate setbacks, or build-to lines for development;</li> <li>ii) parking lots should not take up street frontage along the main commercial streets and should be designed and buffered from adjacent pedestrian routes and roads through landscaping;</li> <li>iii) streets should be designed to accommodate all modes of transportation, including walking, cycling, cars, service vehicles, and public transit;</li> <li>iv) streets and sidewalks shall create a pedestrian environment through defined standards for landscaped areas, paving, street trees, and other appropriate street furniture, and shall form a connected system of optional routes to, from and within the area;</li> <li>v) parks and open spaces should be distributed throughout the Port Dover Waterfront Area, and these facilities should be linked together and connected with the broader County network of natural and open space areas; and</li> <li>vi) the development of a trail system to link open space, natural areas and park areas within and adjacent to areas of development shall be encouraged throughout the Port Dover Waterfront Area.</li> </ul> <p>d) The County shall encourage and facilitate partnership arrangements, joint ventures, and facility sharing with the Marina Board of Management, the Harbour Authority, Provincial agencies, school boards, other institutions and community groups to expand the supply of parks, open spaces and parking facilities.</p> <p><b>6.5.2.3</b> The following shall be the policy of the County:</p> <ul style="list-style-type: none"> <li>a) Notwithstanding the underlying land use designations, permitted uses in the Silver Lake Special Policy Area, delineated on Schedule "B", shall be limited to: <ul style="list-style-type: none"> <li>i) passive and active open space and recreational uses;</li> <li>ii) public and/or private commercial recreational facilities;</li> </ul> </li> </ul> <p><b>6.5.2.4</b> The following shall be the policy of the County:</p>

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<p>potential flooding and that the risk of flooding may impact existing and future land uses.</p> <p>The intent of this Plan is to create a continuous link for commercial and tourism related uses between the Downtown Area and the waterfront through the Lynn River/Black Creek Area and back to the Downtown Area through a connection along Market Street, Chapman Street and Powell Park. The Lynn River/Black Creek Special Policy Area is in transition and none of the changes in land use are expected to occur immediately.</p>	<p>b) In the consideration of any application to amend the Zoning By-law proposing to establish a commercial or residential use, the following shall be addressed:</p> <ul style="list-style-type: none"> <li>iii) adequate access to the property particularly for emergency vehicles; and</li> <li>iv) the provision of adequate parking and loading facilities.</li> </ul>
<p><b>6.8 The Lakeshore</b></p> <p><b>6.8.1 Lakeshore Special Policy Area</b></p> <p>The Lakeshore Special Policy Area (LSPA) is identified on Schedule "A" to this Plan. The Lakeshore Special Policy Area Secondary Plan, included in Chapter 11 of Part II – The Secondary Plans, complements the general policies and land use designations of Part I – The Primary Plan and provides more detailed planning strategies and land use policies to address the special lakeshore attributes and the unique local land use arrangements to ensure the community, environmental and economic sustainability of the County's lakeshore. The LSPA Secondary Plan must be read in conjunction with the policies of the Part I – The Primary Plan. Where a conflict arises between the Part I – The Primary Plan and the LSPA Secondary Plan, the policies of the LSPA Secondary Plan shall prevail.</p>	
<p><b>7.7 Urban Residential Designation</b></p> <p>The Urban Residential Designation applies to the Urban Areas of the County. The Urban Areas are expected to continue to accommodate attractive neighbourhoods which will provide for a variety of residential forms as well as neighbourhood facilities such as elementary schools, parks, places of worship and convenience commercial uses integral to and supportive of a residential environment.</p> <p>A variety of housing types are needed to meet the needs of a diverse population. Opportunities to provide housing for individuals or groups with special needs including the elderly and those with special physical, social or economic needs within the County will be encouraged.</p> <p><b>7.7.2 Land Use Policies</b></p>	<p><b>7.7.2</b> The following policies apply to land designated Urban Residential:</p> <p>b) Triplex, fourplex, townhouses, and other medium density housing forms, shall generally have a net density of between 15 and 30 uph, save and except for in the Courtland Urban Area where private servicing limitations shall determine the density of development. New medium density residential development and other uses that are similar in terms of profile, shall meet the following criteria:</p> <ul style="list-style-type: none"> <li>iii) the development will be encouraged to have direct access to an arterial or collector road, where possible and appropriate;</li> <li>viii) except for a triplex dwelling, fourplex dwelling or other similar small scale developments, a report on the adequacy of the road network to accommodate the expected traffic flows, and the adequacy of water and sewer services may be required from the proponent and approved by the County</li> </ul>

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	<p>c) High density residential uses, including apartment buildings and other forms of multiple housing of a similar density shall be carefully located. The following criteria shall be addressed in the consideration of such applications, especially in proximity to lower density residential development:</p> <ul style="list-style-type: none"> <li>ii) the ability of the site to accommodate necessary facilities and amenities, such as garbage storage, parking and landscaped areas;</li> <li>iii) the height, form and density of the proposed development is such that no undue adverse impacts in terms of overshadowing, increased traffic or loss of amenity area are created for surrounding residential uses;</li> </ul> <p>vii) the proximity of the site to arterial or collector roads, and/or pedestrian accessibility to a Downtown Area or, in the case of the Simcoe Urban Area, a Secondary Centre, or other locations of supporting services and facilities;</p> <p>d) In evaluating applications for neighbourhood commercial uses in the Urban Residential Designation, the following criteria shall be met:</p> <ul style="list-style-type: none"> <li>iv) all required parking shall be provided on the site, and cash-in-lieu of required parking in accordance with the policies of Section 8.2.3 (Parking) of this Plan, shall not be accepted by the County;</li> <li>v) driveway access shall be approved by the County;</li> <li>vi) the facility shall be located at or in proximity to the intersection of arterial or collector roads, and shall not be located mid-block within a residential area</li> </ul>
<p><b>7.8 Downtown Designation</b></p> <p>The Downtown Designation includes retail, office, service, entertainment and other commercial uses, as well as government, institutional, residential and community activities. Downtowns will be the focus of compact and focused development oriented to the pedestrian scale.</p> <p><b>7.8.2 Land Use Policies</b></p>	<p><b>7.8.2</b> The following policies apply to land designated Downtown:</p> <p>d) Historic streetscape patterns such as block lengths, building heights, setbacks and separations will be maintained.</p> <p>f) Linkages between the Downtown Areas and recreation trails shall be encouraged to foster pedestrian activity and encourage tourism in the Downtown Areas. Council will implement the Active Transportation recommendations of the Integrated Sustainable Master Plan as they apply to Downtown Areas, to the extent practical.</p> <p>l) The provision of adequate convenient parking is important to maintaining the economic viability of Downtowns. It is recognized that in Downtowns, it may not be feasible or desirable for all developments to provide the parking on site. The provision of parking spaces shall be subject to the following policies:</p> <ul style="list-style-type: none"> <li>i) private and public parking lots will be encouraged at locations convenient to the main shopping streets. Parking lots should not be visible from or have direct frontage on a major commercial street in the Downtown;</li> <li>ii) parking areas will be encouraged to locate to the rear of buildings;</li> </ul>

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	<ul style="list-style-type: none"> <li>iii) parking facilities shall be appropriately designed and landscaped to complement the downtown streetscape;</li> <li>iv) on-street parking is important to the economic vitality of Downtowns and will be maintained to the extent practical. The temporary use of on-street parking spaces for pop-up uses or outdoor cafes or eating areas may be permitted subject to the policies and procedures that may be established by Council;</li> <li>v) parking will be encouraged for new residential developments;</li> <li>vi) the County may reduce or eliminate parking requirements for all developments or particular classes of development in any or all of the Downtown designations; and</li> <li>vii) the County may accept cash-in-lieu of parking as an alternative to providing any required parking.</li> </ul> <p>m) Drive-through commercial facilities may be permitted in the Downtown Designations subject to the following requirements:</p> <ul style="list-style-type: none"> <li>i) ingress and egress from the site will not conflict with the safe movement of pedestrians;</li> <li>ii) where adequate sight lines can be maintained to ensure safe turning movements that will not conflict with safe pedestrian and vehicular traffic;</li> <li>iii) where adequate stacking lanes can be provided on-site to ensure that pedestrian and vehicular traffic will not be impeded;</li> <li>iv) where the drive-through facilities will not restrict the development or redevelopment of adjacent properties; and</li> <li>v) that adequate and appropriate buffers be provided where the drive-through facility is adjacent to properties with residential units.</li> </ul>
<p><b>7.9 Mixed Residential/Commercial Designation</b></p> <p>The Mixed Residential/Commercial are generally areas of transition from the relatively focused and primarily commercial developments in Downtown areas to the lower-density surrounding residential neighbourhoods.</p> <p><b>7.9.2 Land Use Policies</b></p>	<p><b>7.9.2</b> The following policies apply to land designated Mixed Residential/Commercial:</p> <ul style="list-style-type: none"> <li>a.) It is intended that in the process of introducing commercial uses into Mixed Residential/Commercial areas that the residential character of the individual buildings and the general area shall be maintained by sensitive conversion of existing buildings with a residential character and by development of complementary additions or new buildings, by control of signs visible from the exterior and by properly locating and screening off-street parking.</li> <li>c) Adequate parking shall be provided on-site and shall generally be located to the rear of the main front wall of the building. Parking shall be adequately screened and buffered so as not to impact on adjacent residential uses.</li> <li>f) Comprehensive block development of lands in separate ownerships will be encouraged, to achieve well-designed and integrated development, including: <ul style="list-style-type: none"> <li>i) integrated internal circulations systems;</li> <li>ii) coordinated access points, to minimize the total number of access points to abutting roads;</li> <li>v) integrated parking areas</li> </ul> </li> </ul>

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<p><b>7.10 Shopping Centre Commercial Designation</b></p> <p>The Shopping Centre Commercial Designation applies to the existing shopping centres located outside of areas designated as Downtown. These areas are primarily retail in nature, and provide a less diverse array of goods and services than provided in the Downtown Designation.</p> <p><b>7.10.2 Land Use Policies</b></p>	<p><b>7.10.2</b> The following policies apply to land designated Shopping Centre Commercial:</p> <p>b) Adequate off-street parking and loading spaces shall be provided in accordance with Zoning By-law.</p> <p>c)_ Shopping Centre Commercial uses shall generally only located on Provincial Highways, subject to the approval of the Province and the County, or arterial roads, subject to the approval of the County.</p> <p>d) A high standard of site design, including landscaping, buffer areas, building design and loading, parking and loading areas, pedestrian and vehicular circulation, lighting and signage shall be required and shall be subject to the Site Plan Control Bylaw.</p> <p>e) Any enlargement of an existing Large Retail Use in the Shopping Centre Commercial Designation shall only be permitted by way of site-specific amendment to the Zoning By-law. Prior to considering a Zoning By-law amendment to accommodate an enlarge Large Retail Use, the following criteria shall be met to the satisfaction of the County:</p> <p style="padding-left: 40px;">iii) a traffic impact study shall be completed in accordance with the specific requirements of the County and the Province, as appropriate</p> <p>e) Establishment of a new Large Retail Use, of more than 3,000 m<sup>2</sup> of gross leasable floor area, in the Shopping Centre Commercial Designation shall only be permitted by way of site-specific amendment to the Zoning By-law. Prior to considering a Zoning By-law amendment to accommodate a new Large Retail Use, the following criteria shall be met to the satisfaction of the County:</p> <p style="padding-left: 40px;">iii) a traffic impact study shall be completed in accordance with the specific requirements of the County and the Province, as appropriate</p>
<p><b>7.11 Commercial Designation</b></p> <p>Commercial areas are accessible locations along the County's major transportation routes offering suitable accommodation for a specific range of commercial uses that are primarily highway-oriented, space-extensive uses that cannot be accommodated within the Downtown areas, and are mixed residential and commercial uses that have historically located along major transportation routes.</p> <p><b>7.11.2 Land Use Policies</b></p>	<p><b>7.11.2</b> The following policies apply to land designated Commercial:</p> <p>b) Adequate off-street parking and loading spaces shall be provided in accordance with Zoning By-law</p> <p>c) Commercial uses shall only locate on Provincial Highways, subject to the approval of the Province and the County, or arterial or collector roads, subject to the approval of the County.</p>
<p><b>7.13 Industrial Designation</b></p> <p>The Industrial Designation applies to older industrial sites that are under-utilized and poorly situated to attract new industrial investment. Generally, areas designated as Industrial are located near to residential areas and their intensive use for industrial purposes may conflict with neighbouring sensitive uses.</p>	<p><b>7.13.2</b> The following policies apply to land designated Industrial:</p> <p>b) The conversion of lands designated industrial to other land use designations of this Plan shall be subject to an amendment to this Plan and to the rezoning of the subject lands. Applications for the conversion of lands designated Industrial to other land uses shall be subject to the consideration of the following policies:</p>

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<p>The conversion of lands designated as Industrial to other uses more compatible with the neighbourhood context in which the lands are situated is encouraged.</p> <p><b>7.13.2 Land Use Policies</b></p>	<ul style="list-style-type: none"> <li>v) Adequate parking for residents, employees and visitors shall be provided on-site;</li> <li>vi) A planning rationale report assessing the compatibility of the proposed use and potential impact on adjacent uses as well as addressing any other relevant matter outlined in Section 9.6.1 (Official Plan Amendments) of this Plan shall be provided;</li> <li>vii) A traffic impact study, in accordance with the requirements of the County and the Province, shall be provided</li> </ul>
<p><b>7.14 Major Institutional Designation</b></p> <p>The Major Institutional Designation is intended to recognize those major public institutions that benefit the residents of Norfolk County, and occupy large and prominent sites. Major Institutional uses provide important specialized medical, educational, governmental and correctional services and are a significant source of employment for the entire County. Major Institutional uses may include hospitals, elementary and secondary schools, post-secondary educational facilities, correctional facilities, courthouses, and government complexes. These Major Institutional uses tend to be intensely built and attract high traffic volumes; consequently, they require an independent land use designation.</p> <p><b>7.14.2 Land Use Policies</b></p>	<p><b>7.14.2</b> The following policies apply to land designated Major Institutional:</p> <ul style="list-style-type: none"> <li>a) Specific development standards for permitted uses and ancillary uses shall be included in the Zoning By-law and may include floor space limitations, parking requirements, and limitations on signage and advertising.</li> <li>c) Any proposal to add or enlarge an Major Institutional Designation shall be evaluated on the basis of: <ul style="list-style-type: none"> <li>ii) the adequacy of social and physical services, including roads to accommodate the proposed use.</li> </ul> </li> <li>d) Adequate off-street parking areas shall be provided in accordance with the provision of the Zoning By-law, and access to parking areas shall be limited and designed to provide maximum safety for pedestrian and vehicular traffic.</li> <li>e) Larger scale Major Institutional Uses, listed in Section 7.14.1 a) (Permitted Uses) shall be subject to the following policies: <ul style="list-style-type: none"> <li>i) Larger scale Major Institutional Uses shall be located where there is direct access to an arterial or collector road to discourage traffic from using local roads</li> </ul> </li> </ul>
<p><b>7.16 Urban Waterfront Designation</b></p> <p>The Urban Waterfront Designation is intended to accommodate commercial facilities and services in convenient locations to serve the needs of residents and tourists. It is intended that the Urban Waterfront Designation only be applied in the Urban Areas in direct proximity to the water's edge of Lake Erie, in order to promote the ongoing development of marine industries, tourist destinations, and support existing residents. It is recognized that the inherent flood prone areas and hazard lands influence this Designation, affecting both existing and future land uses.</p> <p>The Urban Waterfront Designation recognizes that marine industrial uses and activities are important and will continue to be important to the local economy and heritage of Port Dover, particularly with respect to the commercial fishing and marine industry. It is the intent of</p>	<p><b>7.16.2</b> The following policies apply to land designated Urban Waterfront:</p> <ul style="list-style-type: none"> <li>d) In the consideration of any Zoning By-law amendment proposing to establish any of the permitted uses, the following shall be addressed: <ul style="list-style-type: none"> <li>ii) adequate access to the property from a public road; and</li> <li>iii) the provision of parking and loading facilities.</li> </ul> </li> </ul>

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<p>this Plan to protect, maintain and promote the Commercial Fishing Port as an ongoing viable commercial/industrial activity that has significant historic relevance and adds to the charm and appeal of the community.</p> <p><b>7.16.2 Land Use Policies</b></p>	
<p><b>7.17 Major Public Infrastructure Designation</b></p> <p>Major Public Infrastructure uses provide public services, such as drinking water, waste water treatment and public works yards that are essential to the health and well-being of County residents and visitors and which support local economic activities and institutions. Major Infrastructure uses are to be protected from intrusions by sensitive land uses that may restrict the operations of existing or expanded municipal facilities.</p> <p><b>7.17.2 Land Use Policies</b></p>	<p><b>7.17.2</b> The following policies apply to land designated Major Public Infrastructure:</p> <p>c) Development of lands designated as Major Public Infrastructure shall have adequate parking, loading and garbage collection/storage facilities provided on site.</p>
<p><b>8.0 Networks and Infrastructure</b></p> <p>Norfolk County’s networks and infrastructure, including all roads, trails, sewers, watermains, electric, utility, waste disposal, and telecommunications facilities, play an important role in defining the County and ensuring its sustainability. These systems are vital to economic competitiveness, community health and environmental responsiveness. The policies of this Plan, and specifically this Section, strive to ensure the efficient and cost-effective co-ordination between long-term growth management and the provision of infrastructure.</p>	
<p><b>8.2 The Transportation Network</b></p> <p>Transportation plays an important role in determining the quality of life within a community through the level of service and accessibility to employment, social, recreational and shopping opportunities provided by the transportation network. Road, rail, air and water transportation all play roles in goods movement throughout the County.</p> <p>The Transportation Plan, set out in Schedule “E” is based on the inter-relationship of land use and transportation. The transportation system is intended to be compatible with and supportive of the land use pattern. Roads have different characteristics, depending on the nature of the surrounding land use. A road in the rural and agricultural portion of the County</p>	<p>The following shall be the policy of the County:</p> <p>a) The efficiency of the transportation network should be maximized by coordinating transportation planning initiatives and activities with other levels of government and transportation agencies.</p> <p>b) All transportation services shall be planned and constructed in a manner that supports the policies of this Plan. Upgrades to transportation services may be required to be approved in accordance with the Environmental Assessment Act.</p> <p>c) Corridors and rights-of-way for significant transportation facilities shall be planned for and protected to serve the long-term needs of the County. Development that could preclude the use of a corridor or right-of-way for its long-term purpose shall not be permitted.</p> <p>d) The preservation and reuse of abandoned transportation corridors for purposes that maintain the corridor’s continuous linear</p>

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<p>will exhibit rural characteristics, matching rights-of-way with their expected function and use. However, roads that, for example, traverse historic Hamlet Areas or Urban Areas are expected to maintain the character of those areas and provide both lower speed access as well as a pedestrian environment.</p> <p>The roads also facilitate the safe and efficient movement of both people and goods through the County at minimal economic, environmental and social cost. Additionally, the network is intended to promote the development pattern in the County and be supportive of economic activity.</p> <p><b>8.2.1 Movement of Goods</b></p> <p>The limitations on Provincial Highways and the lack of freeway access to the County have limited the accessibility to high-order road transportation. Consequently, every effort shall be made to ensure an efficient and effective road, air and water transportation system.</p> <p><b>8.2.2 Hierarchy and Classification of Roads</b></p> <p>Public roads in the County shall be classified into a hierarchy on the basis of jurisdiction, function, traffic characteristics, speed and interconnections.</p> <p><b>8.2.2.1 Provincial Highways</b></p> <p>Provincial Highways No. 3, 6 and 24 are Provincial Highways illustrated on Schedule “E” and are under the jurisdiction of the Ministry of Transportation.</p> <p><b>8.2.2.2 County Roads</b></p> <p>County roads have a range of functions, carrying high, medium and low volumes of traffic on arterial, collector and local roads, respectively. County roads serve the purpose of carrying traffic, as well as providing access to land.</p> <p><b>8.2.2.3 Arterial Roads</b></p> <p>Arterial Roads identified on Schedule “E” are roads designed to carry high volumes of traffic from Provincial Highways and other interregional roads to the collector road system, and vice-versa.</p> <p><b>8.2.2.4 Collector Roads</b></p>	<p>characteristics shall be encouraged, whenever appropriate and feasible.</p> <p>e) Connectivity of the transportation network within the County and crossing into adjacent jurisdictions shall be maintained and, where possible and feasible, improved.</p> <p>f) The County shall ensure that adverse environmental effects, such as noise, vibration and air quality deterioration, will be mitigated in the planning, design, and construction of elements of the transportation network in accordance with the policies of Section 8.8 (Noise, Vibration, Odour and Light Emissions). This shall apply to all air, rail, road and water transportation facilities.</p> <p>g) The automobile will continue to be the main mode of transportation within the County due to its predominantly rural character, small and dispersed population, and size of the Urban Areas. Notwithstanding this, a land use pattern, density and mix of uses shall be promoted in the Urban Areas that reduces growth in the length and number of vehicle trips, and creates the potential for public transit viability and other alternative and sustainable transportation modes such as walking and cycling.</p> <p>h) Safe and convenient pedestrian interfaces with roads shall be encouraged.</p> <p>i) The impact of a development proposal on the transportation system, including the means of access, shall be examined through a traffic impact study. Only those development proposals that can be accommodated in the existing system will be permitted. Where the transportation system is not adequate, the County shall require, as a condition of development approval, that the proponent of the development:</p> <ul style="list-style-type: none"> <li>i) improve the system in the vicinity of the proposed development without the County incurring any costs;</li> <li>ii) make the necessary financial contributions for the required improvements; and/or</li> <li>iii) dedicate rights-of-way for the development of roads.</li> </ul> <p>The cost of traffic impact studies and any other required supporting documentation shall be borne by the proponent. Costs incurred by the County in engaging peer review consultants to evaluate the proposal and supporting submissions shall be reimbursed by the proponent.</p> <p>j) The County may implement traffic calming measures near schools, nursing homes, hospitals and in downtowns. The County will apply the Canadian Guide to neighbourhood Traffic Calming as the guiding document for the design and installation of traffic calming measures. The County may create specific design guidelines appropriate to the local context.</p> <p>k) Complete streets are designed and operated to enable safe access for all users, including pedestrians, cyclists, motorists, transit users, commercial and emergency vehicles. The County will consider the implementation of complete street guidelines, appropriate to the local context, as roads are reconstructed and new roads built.</p>

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<p>Collector roads are designed with the dual function of carrying moderate volumes of local traffic to arterial roads, and distributing arterial traffic to local roads, while providing access to abutting properties.</p> <p><b>8.2.2.5 Local Roads</b></p> <p>The remainder of the streets in the County road system are classified as local roads. Local roads are intended to carry low volumes of traffic and to provide access to individual properties.</p> <p><b>8.2.3 Parking</b></p> <p>Given that the automobile will continue to be the principle mode of transportation within the County, the provision of sufficient parking, in terms of size, location and quantity is an important consideration in the Plan.</p> <p><b>8.2.4 Public Transit</b></p> <p>Transit provides opportunities to enhance the mobility options for people who do not or cannot drive to access goods, services, amenities, and local attractions.</p>	<p><b>8.2.1</b> The following shall be the policy of the County:</p> <ul style="list-style-type: none"> <li>a) The County shall ensure the co-ordination between growth management and the transportation systems.</li> <li>b) The County shall work with transportation industries to facilitate the efficient movement of goods by road, water and air, by improving the level of service while maintaining community safety and minimizing risk.</li> <li>c) The County shall ensure that appropriate road service is provided to employment areas.</li> <li>d) The County shall ensure that designated commercial areas are serviced by roads with appropriate traffic volume capacity.</li> <li>e) Through the passing of by-laws, the County may establish truck routes along Provincial Highways, arterial roads and non-residential collector roads, thereby avoiding local roads in residential neighbourhoods, in order to protect residents from noise and corridor emission pollutants.</li> <li>f) The County shall maintain and encourage the protection of abandoned rail corridors for other linear uses.</li> </ul> <p>Public roads in the County shall be classified into a hierarchy on the basis of jurisdiction, function, traffic characteristics, speed and interconnections. The classification of roads and the existing and proposed road system is shown on Schedule “E”.</p> <p><b>8.2.2</b> The following shall be the policy of the County:</p> <ul style="list-style-type: none"> <li>a) The County shall review road corridors, in consultation with the Province, to determine if a change in road classification is necessary. The transfer of a road from one jurisdiction to another shall not require an amendment to this Plan, notwithstanding the change in classification.</li> <li>b) Any change in the function of a road shall not require an amendment to this Plan to change the classification depicted on Schedule “E”. The Official Plan may be modified as a result of such a change in the function of a road at the time of a review in accordance with Section 9.3 (Official Plan Monitoring and Review). Similarly, minor road widenings, re-alignments, by-passes, establishment of new roads, road reclassification or alteration of a proposed alignment shall not require an amendment to this Plan.</li> <li>c) The County shall, as needed, enact access control by-laws for specified County roads with present or anticipated high traffic flows. Access control by-laws may be developed for any County road where high traffic or a significant percent of truck traffic develops.</li> <li>d) Development shall only be permitted where frontage and access is to an open and public road that is maintained on a year-round basis, as determined by the County. Any road improvement required to bring a road up to a standard deemed appropriate by the County shall be at the expense of the benefiting landowner(s).</li> </ul>

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	<p>e) Road access points shall be designed to the satisfaction of the County and be in locations that will not create a hazard due to impaired line of sight, or any other geometric, transportation or land use planning consideration.</p> <p>f) Minimum right-of-way widths are provided for each classification of road. However, in the Urban Areas and Hamlet Areas, and in certain other circumstances, the County may consider alternative development standards including reduced right-of-way widths. The County recognizes that in some existing developed areas the reconstruction of roads to the standards required by the policies of this Plan may be economically or physically infeasible due to constraints of existing buildings, existing services, access driveways and other conditions. In order to secure needed road improvements in such cases, it will be necessary to find a realistic balance between accepted engineering standards and the disruptive effects upon existing conditions. In particular, in considering improvements in the Downtown Areas, as defined on Schedule "B", careful attention shall be given to the effects of transportation improvements upon the heritage buildings, open pedestrian areas and general character of the area.</p> <p>g) Where additional land is required for road widenings and extensions, such land shall be dedicated wherever possible, in the course of approving draft plans of subdivision or condominium, consents or site plan agreements, without amendment to this Plan. The dedication of such land shall take into account the following:</p> <ul style="list-style-type: none"> <li>i) the classification of the road as indicated on Schedule "E" of this Plan and the extent of the right-of-way that may be required for that road classification as established by the policies of this Plan;</li> <li>ii) road widenings being taken equally on either side of the centre line of existing roads. However, unequal widenings may be required where factors, such as topography, historic building locations, grade separation, channelization, existing development or other unique conditions make the dedication of equal widenings infeasible;</li> <li>iii) the need to provide acceleration and deceleration lanes, left-turn storage lanes, medians, traffic signals or other traffic control devices, sight triangles at intersections including intersections of an arterial road and a railway line, railway grade separations, and/or any other traffic or road engineering consideration. The extent of the widening shall be based on specific characteristics of the intersection and shall be determined in accordance with accepted traffic engineering design criteria;</li> <li>iv) any required facilities to ensure accessibility for all members of the public, including people with disabilities;</li> <li>and</li> <li>v) requirements as established by County.</li> </ul> <p>h) No development or redevelopment of land shall be approved in close proximity to an intersection or railway crossing which is scheduled for improvement until the improvement has been sufficiently designed to determine the land requirement for the improvement.</p>

Chapter / Section Description	Transportation Related Policies
	<p>i) As a measure of maintaining a satisfactory road system, the County shall pursue a program of improving road alignments, surfaces, and pavement widths, and establishing adequate road allowances for new roads as future development occurs. Provision shall be made in the Zoning By-law for adequate setbacks for all new development, having regard for the width and function of the abutting road.</p> <p>j) Any proposals to widen, extend, realign or improve roads shall consider Natural Heritage Features and cultural heritage landscape factors and attributes of adjacent land, or views created by the road. The County may require a landscape assessment prior to approval or endorsement of any proposals to widen, extend, realign or improve roads.</p> <p>k) Paved streets, curbs and gutters shall be provided for any major new development or redevelopment in the Urban Areas to the satisfaction of the County. Sidewalks may be required to be provided in new development or redevelopment as determined by the County in accordance with the policies of this Plan. The County may consider lower development standards outside of the Urban Areas, subject to the policies of this Plan.</p> <p><b>8.2.2.1</b></p> <p>The following shall be the policy of the County:</p> <p>a) Transfer of any Provincial Highway to another jurisdiction shall not require an amendment to this Plan.</p> <p>b) The Ministry of Transportation shall have jurisdiction and control over access to Highways No. 3, 6 and 24, and development within the Ministry's permit control area.</p> <p>c) Direct private access to the Provincial Highway shall be restricted and shall require the approval and permit(s) from the Ministry of Transportation. Developments and private access driveways shall be encouraged to use existing County roads and service roads wherever possible. Where access from the Provincial Highway is feasible, it shall only be considered for those properties that meet the minimum safety and geometric requirements of the Ministry of Transportation. Where access is available to a County road, access to the Provincial Highways shall not be permitted.</p> <p>d) The County shall pursue control from the Province over the Provincial Highway corridor connecting links through the Urban Areas.</p> <p>e) The Ministry of Transportation shall determine the right-of-way width for the Provincial Highways.</p> <p>f) All development adjacent to the Provincial Highway shall be subject to the safety and geometric requirements and permits of the Ministry of Transportation.</p> <p>g) Generally, open storage and loading areas shall be screened from Provincial Highways.</p>

Chapter / Section Description	Transportation Related Policies
	<p><b>8.2.2.2</b> The following shall be the policy of the County:</p> <p>a) The County shall address the adequacy of sight distances related to the road alignment and shall avoid multiple individual access points when reviewing development applications on County roads.</p> <p>b) On certain County roads that exhibit high traffic volumes, access limitations may be required. Access to all County roads shall be subject to the approval of Norfolk County.</p> <p><b>8.2.2.3</b> The following shall be the policy of the County:</p> <p>a) Access from abutting properties to arterial roads shall be approved by the County.</p> <p>b) Except as noted in Section 8.2.2(g) (Hierarchy and Classification of Roads) of this Plan, the minimum basic road right-of-way width for an arterial road shall be 36 metres. Setbacks from arterial roads shall be established in the Zoning By-law.</p> <p>c) The flow of traffic on arterial roads shall take precedence over on-street parking, except in the Downtown Areas, where traffic and on-street parking needs shall be balanced.</p> <p>d) In the Urban Areas, sidewalks shall generally be constructed on both sides of an arterial road.</p> <p><b>8.2.2.4</b> The following shall be the policy of the County:</p> <p>a) On-street parking may be permitted on collector roads in the Urban Areas, although location and time restrictions may be enforced in specific instances; for example, adjacent to schools or during peak periods of traffic demand.</p> <p>b) Direct access to collector roads shall be permitted, subject to geometric design considerations.</p> <p>c) Except as noted in Section 8.2.2(g) (Hierarchy and Classification of Roads) of this Plan, the minimum basic road right-of-way width for collector roads shall generally be 26 metres.</p> <p>d) In the Urban Areas, sidewalks shall generally be constructed on both sides of collector roads.</p> <p><b>8.2.2.5</b> The following shall be the policy of the County:</p> <p>a) Except as noted in Section 8.2.2(g) (Hierarchy and Classification of Roads) of this Plan, local roads shall generally be two traffic lane roads with a minimum road right-of-way width of 20 metres and be designed to carry local traffic and to provide land access to abutting properties. In instances where more than two lanes of traffic are to be accommodated, the minimum right-of-way width shall be 23 metres.</p> <p>b) Local roads shall be designed to discourage high speed traffic through frequent stop signs or other appropriate design measures.</p>

Chapter / Section Description	Transportation Related Policies
	<p>c) Sidewalks may generally be required on one side of local roads. In instances where local roads lead directly to a school, park, community facility or other facility which generates pedestrian traffic, or where safety of pedestrians is of particular concern, sidewalks may be required on both sides of the road.</p> <p>d) Some local roads are only seasonally maintained. Nothing in this Plan shall be construed to imply specific seasonal road or maintenance improvement initiatives within the planning horizon of the Official Plan.</p> <p><b>8.2.3</b> The following shall be the policy of the County:</p> <p>a) Except in Downtown Areas, all new development and redevelopment, including re-use of existing buildings, shall be required to provide adequate off-street parking and loading spaces in accordance with standards established in the Zoning By-law. Access and egress to all off-street parking or loading spaces shall be limited in number and designed to minimize danger to vehicular and pedestrian traffic.</p> <p>b) Parking needs in the Downtown Areas will be assessed in order to provide adequate and convenient on- and off-street parking.</p> <p>c) All new development or redevelopment in the Downtown Areas shall be encouraged to provide sufficient parking on-site to accommodate the proposed use. Parking in Downtown Areas shall be subject to the policies of Section 7.8.2 I) (Downtown Designation) of this Plan.</p> <p>d) Where the required parking cannot be provided on site for a proposed development within the Downtown or Urban Waterfront Designations, the County may collect a cash-in-lieu payment, pursuant to Section 40 of the Planning Act, to be used expressly for the provision of additional parking spaces in an appropriately defined area.</p> <p>e) The provision of off-site parking, may be considered provided that the off-site parking will be within a convenient walking distance of the proposed development and subject to the satisfaction of the County. The County may reduce the Zoning Bylaw parking requirements for affordable and senior housing projects where the development proponent is able to demonstrate, to the satisfaction of the County, that the provision of a reduced number of parking spaces will adequately accommodate the on-site parking needs of residents, visitors and staff.</p> <p><b>8.2.4</b> The following shall be the policy of the County:</p> <p>a) The County will explore opportunities to enhance transit services in partnership with community and/or government partners, including adjacent municipalities.</p> <p>b) Development within Urban Areas shall, where appropriate, take into consideration the safe and convenient provision of transit services.</p> <p>c) The continued need for transit services will be considered on an on-going basis.</p>

Chapter / Section Description	Transportation Related Policies
<p><b>8.3 Walking, Cycling and Trails</b></p> <p>Bicycle and pedestrian trails and paths contribute to healthy communities. This Plan recognizes and supports cycling and walking as alternative sustainable modes of travel. The County encourages the development of enhanced walking and cycling routes designed for people of all ages and abilities.</p>	<p>The following shall be the policy of the County:</p> <ul style="list-style-type: none"> <li>a) Existing and proposed trails for walking and cycling are generally illustrated on Schedule “I” to this Plan. The County shall work towards providing bicycle and pedestrian paths, separated from the roadway, on existing and proposal roads, on abandoned rail corridors, on utility corridors, and within parks and open spaces, as appropriate. Priority shall be given to the development and enhancement of routes illustrated on Schedule “I” of this Plan.</li> <li>b) The County shall consider adapting roads to provide safer travel for bicycles, where feasible and appropriate.</li> <li>c) The County shall undertake to interconnect existing walking trails and bicycle paths, where feasible and appropriate.</li> <li>d) The County shall encourage the integration of bicycle path and walkway systems into the design of transportation facilities, including transit services, by including facilities such as protected bicycle storage areas at places of employment and major community, institutional, educational, cultural and shopping locations, where appropriate.</li> <li>e) The County shall encourage the continued use and development of the multi-purpose trail system connecting Simcoe, Delhi, Waterford and other municipalities, as well as connections to the Trans Canada Trail.</li> <li>f) The County shall work towards the development of a waterfront trail between Port Dover and Long Point, the details of which shall be established through the Lakeshore Secondary Plan, further to Section 11 (Lakeshore Special Policy Area).</li> <li>g) The County shall use the design guidelines in the Integrated Sustainable Master Plan for the further development and enhancements of walking, cycling and trail facilities.</li> <li>h) The County will promote streetscape improvements that encourage the use of active transportation including the installation of benches, waste receptacles, bicycle racks, crosswalks, pedestrian scale lighting and shade.</li> <li>i) The County will require that sidewalks be well lit and be accessible for all users, including people with disabilities.</li> <li>j) The County will, where appropriate, include requirements for walking, cycling and trail infrastructure in the approval of site plan and other development applications.</li> <li>k) Schedule “I” shows off-road trails that may be located within neighbourhood and community parks, conservation lands and on other public and private lands. Changes to the off-road trail system, including the re-routing, removal and addition of off-road trails will not require an amendment to this Plan.</li> <li>l) The County may, where appropriate and in consultation with the Conservation Authority, provide infrastructure to access creeks, rivers and lakes for recreational boating.</li> </ul>

Chapter / Section Description	Transportation Related Policies
	m) The County shall prepare and regularly up-date a Walking, Cycling and Trails Master Plan.
<p><b>8.4 Air Transportation</b></p> <p>Convenient access to local and regional air travel and transportation facilities would be valuable to the residents and businesses of Norfolk County. It is anticipated that any air fields, air strips or airports within the County will play a minor general aviation role.</p>	<p>The following shall be the policy of the County:</p> <p>a) The County shall investigate alternatives for securing a fully functioning regional airport that is viable in the long term and shall work with all adjacent jurisdictions to study the existing level of service and operational deficiencies in the system of airports and airfields.</p>
<p><b>8.5 Rail Lines</b></p> <p>Norfolk County views the existing rail line in the County as an asset, and supports the continuation of a safe and efficient railway network. While a number of rail lines have been eliminated, the remnant rail line traversing the County from the Town of Tillsonburg to the east of the Courtland Urban Area will be supported and promoted.</p>	<p>The following shall be the policy of the County:</p> <p>a) Any proposed new residential or other sensitive use within 300 metres of an active railway right-of-way shall be supported by noise and compatibility studies, completed to the satisfaction of the County, in consultation with the appropriate railway company and the Ministry of the Environment and Climate Change.</p> <p>b) Any proposed new development within 75 metres of an active railway right-of-way shall be supported by a vibration impact study, completed to the satisfaction of the County, in consultation with the appropriate railway company and the Ministry of the Environment and Climate Change.</p> <p>c) For development adjacent to a railway right-of-way, appropriate safety measures such as setbacks, berms and security fences will be provided to the satisfaction of the County in consultation with the appropriate railway company.</p> <p>d) Proponents of development within 300 metres of an active railway right-of-way are encouraged to consult with the appropriate railway company prior to filing formal development applications.</p>
<p><b>8.6 Water Transportation</b></p> <p>Lake Erie provides an important opportunity for recreational transportation, goods movement, and ferrying services.</p>	<p>The following shall be the policy of the County:</p> <p>a) The planning and development of port, docking, harbour and associated road facilities whether initiated by federal, provincial and other agencies, or the private sector, should be carried out comprehensively and should consider such matters as the:</p> <ul style="list-style-type: none"> <li>iii) public use of and access to port facilities;</li> <li>iv) most effective intermodal linkages with transportation facilities;</li> <li>v) integration of functions into multi-use facilities;</li> <li>viii) traffic impacts and the coordination of roads and parking</li> </ul>
<p><b>8.12 Emergency Services</b></p>	<p>The following shall be the policy of the County:</p> <p>a) The County shall ensure that emergency services are provided in a manner that achieves appropriate response time targets and standards. To support the effective and efficient delivery of emergency management services, the County shall consider the following:</p> <ul style="list-style-type: none"> <li>i) locate emergency services where effective delivery of services can meet established targets and standards;</li> </ul>

Chapter / Section Description	Transportation Related Policies
	<ul style="list-style-type: none"> <li>ii) plan for a transportation system that facilitates meeting emergency response time targets;</li> <li>iii) the potential impact on traffic calming initiatives on emergency response times;</li> <li>iv) review the proposed access for emergency services in all development applications;</li> <li>v) require the provision of a secondary access for emergency vehicles for all new cul-de-sacs that exceed 250 metres in length, as a condition of development approval</li> </ul>
<p><b>9.5 Planning Tools</b></p> <p><b>9.5.1 Secondary Plans</b></p> <p>Secondary plans shall be prepared to plan for growth and development on large tracts of land within the County or existing portions of the County that require special land use policies. Should tracks of land for growth and development be identified that are beyond the existing Urban Area boundaries as a result of subsequent reviews of this Plan, a secondary plan shall be required prior to development.</p> <p><b>9.5.1.1 Supporting Requirements for Secondary Plan</b></p> <p>The County, in order to provide the appropriate background information for the any secondary plan, may require the undertaking of a number of background reports.</p> <p><b>9.5.1.3 Contents of Secondary Plans</b></p> <p><b>9.5.2 Community Improvement</b></p> <p>The County shall work to maintain and promote an attractive, well maintained and safe living and working environment through community improvement. The Community Improvement provisions of the Planning Act give Norfolk County planning tools to proactively stimulate community improvement, rehabilitation and revitalization. In designated Community Improvement Project Areas, the preparation of Community Improvement Plans will provide Norfolk County with various powers to promote community improvement. This includes incentives to stimulate or leverage private and/or public sector investment.</p>	<p><b>9.5.1.1</b> These may include, but are not limited, to:</p> <ul style="list-style-type: none"> <li>a) A determination of environmental protection and natural heritage areas;</li> <li>b) A master servicing plan;</li> <li>c) A stormwater management study;</li> <li>d) A traffic impact analysis;</li> <li>e) An urban design master plan;</li> <li>f) An agricultural impact assessment; and</li> <li>g) A planning rationale report.</li> </ul> <p>The requirements of Subsections (a) through (e) may be embodied in an integrated and comprehensive study known as a Master Environmental Servicing Plan (MESP).</p> <p><b>9.5.1.3</b> Contents of a Secondary Plan should include the following:</p> <ul style="list-style-type: none"> <li>a) A statement of the basis or rationale for the preparation of the Secondary Plan;</li> <li>b) A description of the area under study and the role and relationship of the area to the County as a whole;</li> <li>c) A description of the current land use, ownership, built and natural environment, and infrastructure in the area;</li> <li>d) A statement of the desired land use arrangement for the area;</li> <li>e) Goals and objectives appropriate for the area including a statement of how they are in keeping with the Goals and Objectives of this Plan;</li> <li>f) Concept plan(s) showing, where appropriate, the following: <ul style="list-style-type: none"> <li>i) land use designations of the desired type and pattern of development with due consideration to the community design policies of this Plan,</li> <li>ii) the nature and location of public facilities,</li> <li>iii) the desired transportation network for the area and its links to the existing transportation network of the County,</li> <li>iv) the nature and location of municipal services,</li> <li>v) the identification, protection and integration of significant cultural heritage resources and Natural Heritage Features, and</li> <li>vi) the phasing of development and infrastructure;</li> </ul> </li> <li>g) Specific policies and strategies for achieving the goals and objectives established for the area that complement the policies of this Plan; and</li> </ul>

Chapter / Section Description	Transportation Related Policies
	<p>h) Implementation measures to ensure the orderly delivery of the planned development.</p> <p><b>9.5.2</b> The following shall be the policy of the County:</p> <p>b) The designation of Community Improvement Project Areas shall be based on one or more of the following conditions being present:</p> <p style="padding-left: 40px;">iv) poor road access and/or traffic circulation</p> <p>c) Community Improvement Plans shall be prepared and adopted to:</p> <p style="padding-left: 40px;">vi) improve traffic circulation within the Community Improvement Project Areas;</p> <p style="padding-left: 40px;">vii) encourage off-street parking and provide municipal parking facilities where feasible and appropriate;</p>
<p><b>9.6 Development Control</b></p> <p><b>9.6.5 Site Plan Control</b></p> <p>Norfolk County shall utilize site plan control as provided for in the Planning Act. Site plan control applications can vary significantly in their complexity and need for review by County departments and external agencies. The Site Plan Control Bylaw for the County may establish different categories of site plan control applications, with a corresponding fee structure, relating to their complexity.</p>	<p><b>9.6.5</b> The following policies shall apply to site plan control:</p> <p>f) The County shall acquire the widening of road rights-of-way, turning lanes, sight triangles at intersections and other road widening requirements, in accordance with the policies of Section 8.2.2 (Hierarchy and Classification of Roads), as a condition of site plan approval.</p>

## 2.1.2 NCOP – Area Transportation Context

### 2.1.2.1 Provincial Highways

**Highway 6** is identified as a provincial highway under the jurisdiction of the Ontario Ministry of Transportation (MTO). At the intersection of Walker St to the west, Highway 6 becomes an Arterial Road.

### 2.1.2.2 Arterial Roadways

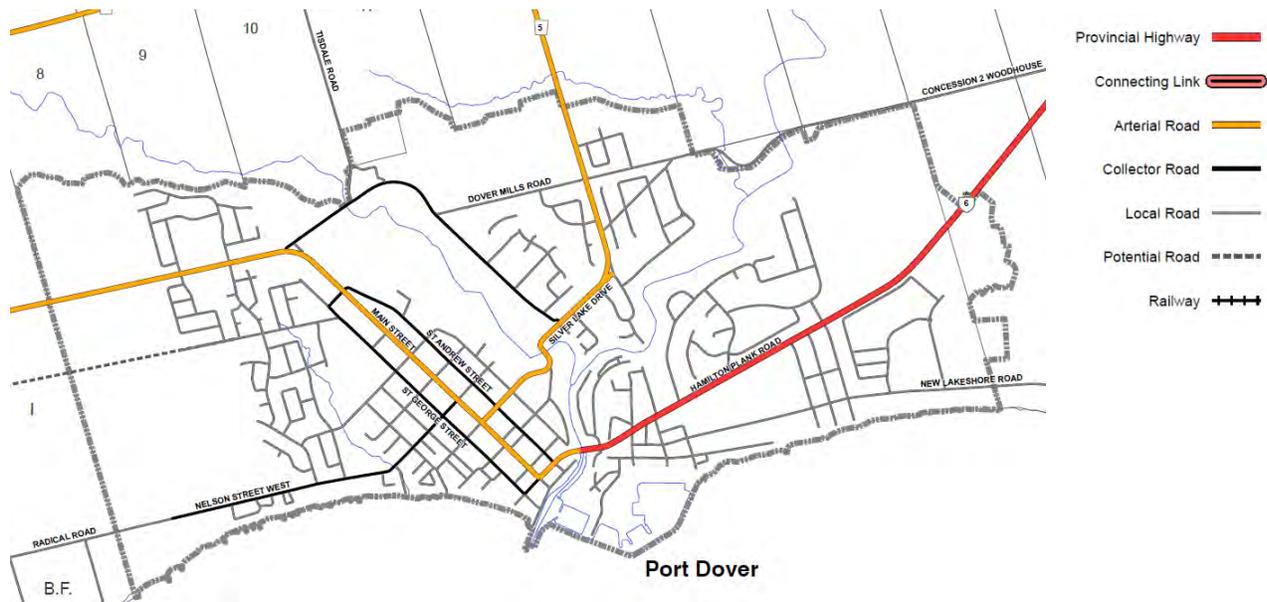
**Highway 6** west of Walker St and **County Rd 5** are identified as arterial roads, bisecting Port Dover from the east-west and north-south, respectively. The basic minimum right-of-way for arterial roads is 36 meters.

### 2.1.2.3 Collector Roadways

**Nelson Street West, St. George Street, St. Andrew Street, and Prospect Street** are identified as collector roads. The basic minimum right-of-way is 26 meters.

### 2.1.2.4 Local Roadways

The remainder of the roads are identified as local roads. They are generally two traffic lane roads with a minimum road right-of-way width of 20 meters. In instances where more than two lanes of traffic are to be accommodated, the minimum right-of-way width shall be 23 meters.



**FIGURE 2: EXCERPT FROM NCOP SCHEDULE E – TRANSPORTATION**

### 2.1.3 NCOP – Active Transportation Network

Active transportation facilities in the NCOP include but are not limited to multi-use trails, off-road trails, paved shoulders, bike paths, and pedestrian paths, and other facilities to be determined through the Integrated Sustainability Master Plan (ISMP).

Figure 3 of the NCOP identifies the County's active transportation routes. Regional active transportation routes within Port Dover include:

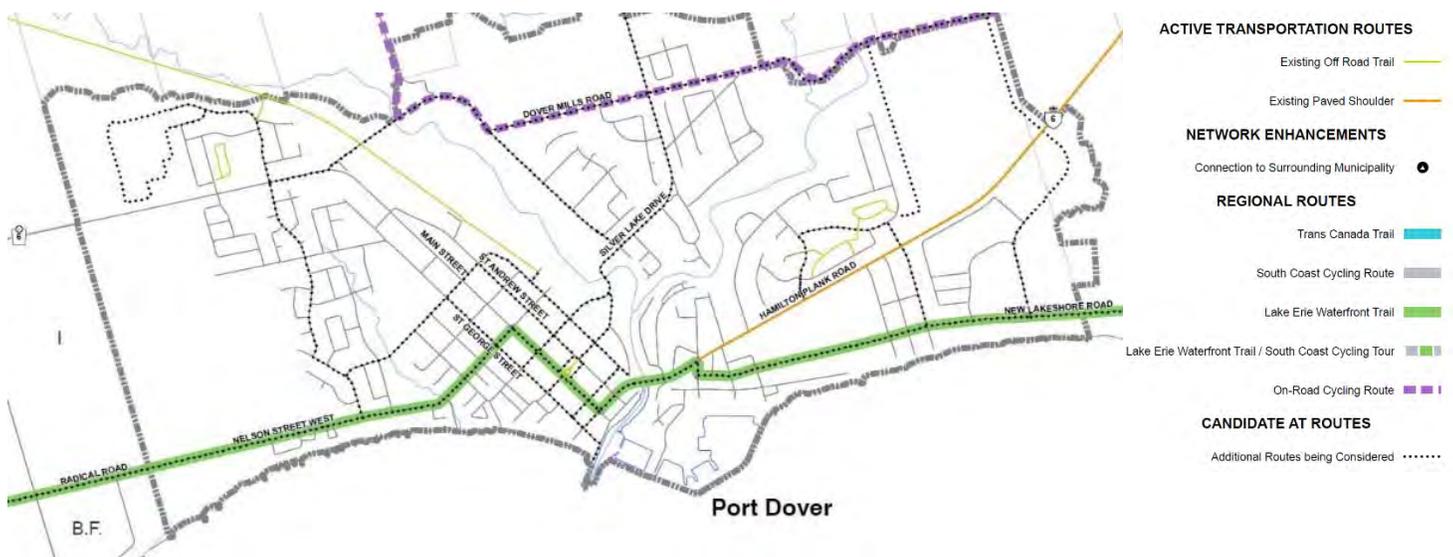
- Lake Erie Waterfront Trail running east-west along New Lakeshore Road
- On-road cycling tour routes running primarily east-west along Marburg Road, Concession 2 Woodhouse, Dover Mills Road, Tisdale Road and County Road

Existing active transportation routes within Port Dover include:

- Off-road local trails including the Lynn Valley Trail linking Port Dover to Simcoe
- Existing paved shoulder along Highway 6 / Hamilton Plank Road to the east

Council will implement the Active Transportation recommendations of the Integrated Sustainable Master Plan as applicable. Additional active transportation routes are being considered, with proposed routes predominantly located along the waterfront and downtown area, with a few routes proposed in residential neighborhoods to bridge connectivity with existing facilities.

A more detailed review of the existing and proposed active transportation facilities is further outlined in Section 2.4.



**FIGURE 3: EXCERPT FROM NCOP SCHEDULE I – ACTIVE TRANSPORTATION**

## 2.1.4 NCOP – Transit Network

Ride Norfolk Transit operates a Monday-Friday bus service year round with a daily Simcoe route and daily Brantford route. Various communities in the County are also serviced on an alternating schedule each day of the week.

Port Dover is serviced on the following days:

- Thursday – Simcoe, Brantford, Delhi, Bill's Corner, Port Dover
- Friday – Simcoe, Brantford, Port Dover, Port Ryerse, Turkey Point, Vittoria

Figures 4 and 5 show the routes and stops running through Port Dover on Thursday and Friday, respectively.

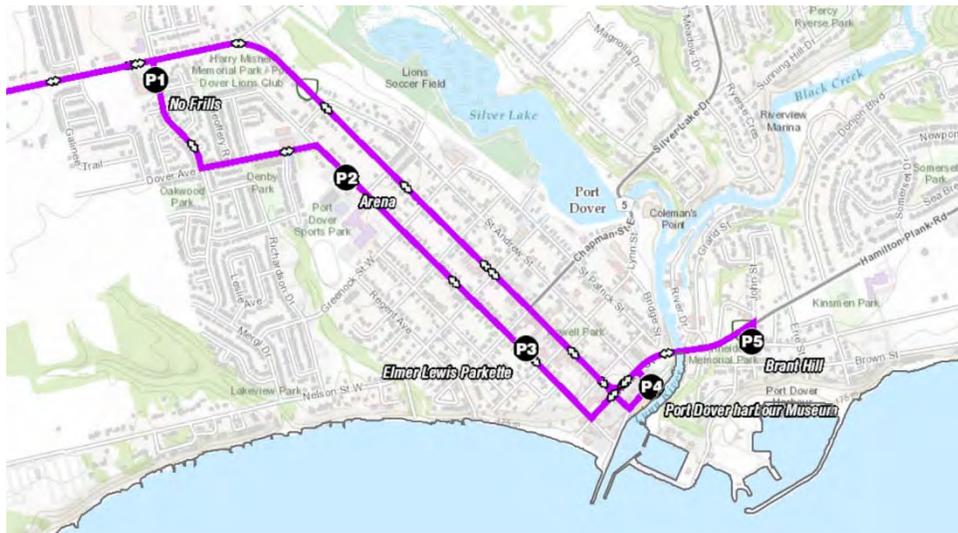


FIGURE 4: RIDE NORFOLK TRANSIT GIS MAP – THURSDAY SCHEDULE

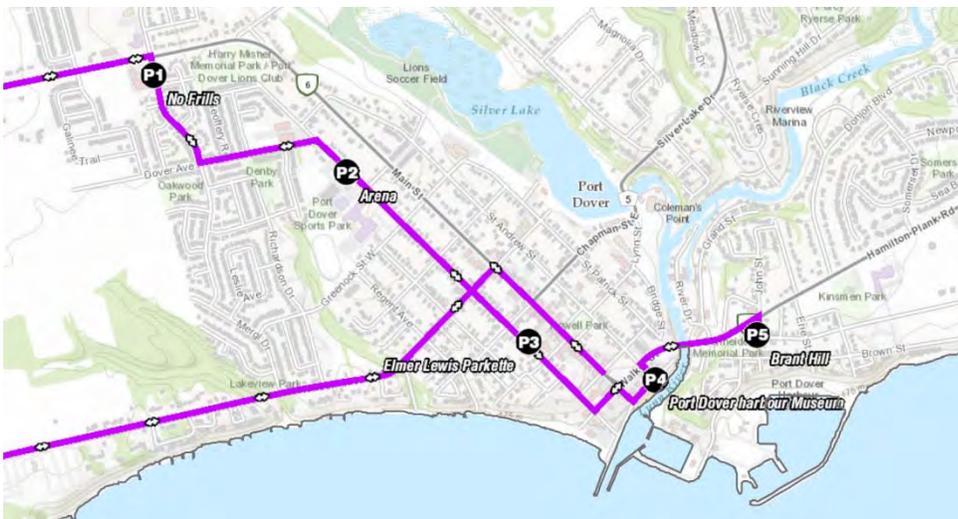


FIGURE 5: RIDE NORFOLK TRANSIT GIS MAP – FRIDAY SCHEDULE

## 2.2 LAKESHORE SPECIAL POLICY AREA SECONDARY PLAN (LSPASP)

As a primary Urban Tourism Node, Port Dover accommodates a range of tourism related uses, including commercial establishments, accommodations, restaurants, and recreational uses.

Transportation-related policies pertaining to Port Dover center on supporting tourism-based activities through the development of Ontario's South Coast Scenic Route. These policies largely consist of the following initiatives (and are reproduced in Section 2.2.2 below):

- Decreasing speed limits to manage scenic views for a slow-travel scenic route
- Strictly controlling access points along the South Coast Scenic Route to maintain safety
- Increasing pedestrian and trail safety through roadway improvement opportunities
- Promoting connections with other trails such as the Lynn Valley Trail
- Working to bring accessible lands and trails along the Lakeshore into public ownership for enjoyment of County residents and visitors
- Creating off-path pedestrian and hiking footpaths along the South Coast Scenic Route along Lake Erie

### 2.2.1 LSPASP – Cycling Context

Notwithstanding the OP policies in 8.2.2 (Hierarchy & Classification of Roads), the LSPASP states that the minimum right-of-way for Ontario's South Coast Scenic Route should be 36 meters, wherever feasible, in order to accommodate on-road trails.

Ontario's South Coast Scenic Route is intended to consist of a scenic drive accommodating vehicular traffic, in addition to a combined on-road/off-road recreational trail where feasible, to accommodate a variety of non-motorized activities, including cycling, walking, roller-blading, and running. This route shall provide a network that links major tourist, recreation, agriculture and natural environment destinations along the lakeshore.

## 2.2.2 LSPASP – Transportation-Related Policies

The Lakeshore Special Policy Area Secondary Plan is a complement to the Norfolk County Official Plan, setting forth a more detailed framework for the lakeshore's unique local land-use arrangements.

**TABLE 2 LSPA SECONDARY PLAN TRANSPORTATION POLICIES**

Chapter / Section Description	Policies
<p><b>11.2.1.1 Growth Management Strategy</b></p> <p>The Growth Management Strategy outlined in Section 11.3 (Growth Management Strategy) identifies the means to accommodate sustainable lakeshore-related growth and development over the planning horizon.</p>	
<p><b>11.2.1.4 Tourism and Recreation Strategy</b></p> <p>The Tourism and Recreation Strategy outlined in Section 11.6 (Tourism and Recreation Strategy) provides a planning framework for the promotion of sustainable tourism and recreation opportunities and activities within the lakeshore. Sustainable tourism activities will promote the County's significant resources while balancing the needs to protect the LSPA's agricultural resources, cultural and natural heritage features and environment.</p>	<p>The Tourism and Recreation Strategy planning directions include:</p> <p>f) Promote public access to the lakeshore and associated water-based uses.</p> <p>g) Promote the development of gateways to mark significant entry points into the LSPA or settlement areas within the LSPA.</p>
<p><b>11.2.1.5 Infrastructure Strategy</b></p> <p>The Infrastructure Strategy outlined in Section 11.7 (Infrastructure Strategy) provides guidance with respect to the adequate provision and requirements for water and wastewater services, and transportation infrastructure including the road network, cycling and trails.</p>	<p>The Infrastructure Strategy planning directions include:</p> <p>b) Promote sustainable multi-modal, non-automotive transportation modes through the recreational, cycling and shared-use trail component of Ontario's South Coast Scenic Route and waterfront trails.</p> <p>c) Provide an efficient and safe transportation network.</p> <p>d) Promote the creation of Ontario's South Coast Scenic Route as a scenic drive and combined recreational trail which links major tourism, recreation, agriculture and natural heritage features and destinations along the lakeshore.</p> <p>e) Promote a trails and cycling network to support tourism objectives and promote healthy lifestyles.</p> <p>f) Encourage the preparation of a County Trails Master Plan to identify preferred trail routes and associated design guidelines.</p>
<p><b>11.3.2.1 Urban Areas</b></p> <p>The two Urban Areas of Port Dover and Port Rowan shall continue to be the primary Settlement Areas where the majority of growth and development activity within the LSPA shall be directed over the planning horizon. These Urban Areas shall</p>	<p>The following shall be the policy of the County:</p> <p>d) Development of large tracts of vacant land within the Urban Area boundary may be subject to the preparation of a Neighbourhood Plan in accordance with the policies of Section 11.8.3 (Neighbourhood Planning) of this Plan to address future development coordination and comprehensive community planning within the future development areas, including the efficient provision of municipal water and sanitary</p>

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<p>accommodate a range of residential, commercial, employment, institutional, entertainment, cultural and health and social service activities. The Urban Areas provide a unique lakeshore character which has been derived from their original fishery port function, and nautical and recreational assets. The Urban Areas of Port Dover and Port Rowan shall function as the primary Urban Tourism Nodes within the LSPA and accommodate a range of tourism related uses, including commercial establishments, accommodations, restaurants, and recreational uses.</p>	<p>services, roads, parks, schools and other public, community and municipal services.</p> <p>g) Lifestyle communities, resort communities, and similar type developments in the LSPA shall be defined as developments, principally residential and recreational in nature. Typical features include, but are not necessarily limited to, smaller residences with extensive recreational and community amenities, and abundant open space. The development of such communities shall be directed to the Urban Areas, and to a lesser extent to the Hamlet Areas. The development of lifestyle communities in the Lakeshore Special Policy Area, and outside of an Urban Area or Hamlet Area, may only be permitted through a County-wide comprehensive review of the Plan and in consideration of the growth projections, availability of designated land in the LSPA, and the location and market considerations associated with land within the LSPA, and subject to an amendment to this Secondary Plan, in accordance with the policies of Section 9.6.1 (Official Plan Amendments), and the following:</p> <p style="padding-left: 40px;">viii) the development shall be located on an arterial or collector road;</p> <p style="padding-left: 40px;">x) any waterfront portion or Ontario’s South Coast Scenic Route frontage of the development shall include an appropriate trail system providing some form of public trail access to the water and connection to the broader lakeshore trail system, which shall be conveyed to the County at no cost</p>
<p><b>11.6 Tourism and Recreation Strategy</b></p> <p>The Tourism and Recreation Strategy provides a planning framework for the promotion of sustainable tourism and recreation opportunities within the lakeshore. Sustainable tourism activities will promote the County’s significant resources while balancing the needs to protect the lakeshore’s agricultural resources, cultural and natural heritage features and environment. This Plan supports sustainable tourism and recreational opportunities that can be sustained over the long term and result in a net benefit to the social, economic, natural and cultural environments within the lakeshore.</p>	<p>e) The County supports public access to the lakeshore through the provision of trails, lookouts, access points, beach areas, and marina areas and supportive areas (such as parking) to promote recreational opportunities associated with the lakeshore. The County shall support the use of County owned Lake Erie shoreline property for the recreational use of all County residents and tourists, where feasible.</p>
<p><b>11.6.4 Water Access and Water-Based Uses</b></p> <p>It is the County’s objective to provide public access to the lakeshore, through the provision of trails, beach areas, and marina areas to promote recreational opportunities associated with the lakeshore.</p>	<p>The following shall be the policy of the County:</p> <p>a) This Plan supports the provision of public access to the waterfront and Lake Erie shoreline by encouraging the creation and retention of a linked waterfront trail. The waterfront trail is generally identified on Schedule “E”. It is recommended that the County undertake a County Trails Master Plan to identify opportunities for the creation of a linked waterfront trail system, in accordance with the policies of Section 11.7.3 (County Trails Master Plan).</p> <p>b) The County shall endeavour to maintain County owned land, public right-of-ways and unopened road allowances where they abut the</p>

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	<p>lakeshore to protect important views and accommodate opportunities for lakeshore access where appropriate.</p> <p>c) The County may close, retain and designate as park any road allowances to provide access to the Lake Erie shoreline.</p>
<p><b>11.7.2 Transportation</b></p> <p>This Plan promotes a sustainable transportation strategy for the LSPA that provides opportunities for non-automotive transportation methods through the development of Ontario’s South Coast Scenic Route and related recreational trail to accommodate cycling, walking, roller-blading, and running activities.</p>	
<p><b>11.7.2.1 Ontario’s South Coast Scenic Route</b></p> <p>Envisaged as a scenic drive and combined recreational trail which provides a network that links major tourist, recreation, agriculture and natural environment destinations along the lakeshore. The combined recreational trail shall accommodate a variety of non-motorized uses, including cycling, walking, roller-blading, and running. Ontario’s South Coast Scenic Route is designated on Schedule “F” and generally extends across the lakeshore along Lakeshore Road and Front Road into Port Ryerse, then north along Port Ryerse Road and east along Radical Road/Nelson Road to Main Street, Port Dover. Ontario’s South Coast Scenic Route also extends south along the Long Point Causeway (Highway 59) from Lakeshore Road to Long Point.</p>	<p>The following shall be the policy of the County:</p> <p>a) The County shall support the development of Ontario’s South Coast Scenic Route as illustrated on Schedule “F”. Ontario’s South Coast Scenic Route is intended to consist of a scenic drive accommodating vehicular traffic, in addition to a combined on-road/off-road recreational trail where feasible, to accommodate a variety of non-motorized activities, including cycling, walking, roller-blading, and running. Ontario’s South Coast Scenic Route shall provide a network that links major tourist, recreation, agriculture and natural environment destinations along the lakeshore.</p> <p>b) Ontario’s South Coast Scenic Route is identified on Schedule “E”. Notwithstanding the policies of Section 8.2.2 (Hierarchy &amp; Classification of Roads), the minimum basic road right-of-way width for Ontario’s South Coast Scenic Route will be 36 metres, where feasible, to accommodate on-road trails.</p> <p>c) The County shall promote the development of recreational, commercial and tourism related uses along Ontario’s South Coast Scenic Route by providing a range of appropriate development opportunities in accordance with the policies of this Plan.</p> <p>d) The County shall work to bring accessible land and trails along Ontario’s South Coast Scenic Route into public ownership for the use and enjoyment of County residents and visitors. Acquisition of the land for the development of Ontario’s South Coast Scenic Route shall be achieved through a number of means including, but not limited to:</p> <ul style="list-style-type: none"> <li>i) the natural heritage policies of Section 6 (Sustainable Natural Heritage) of this Plan;</li> <li>ii) acquisition through the development approvals process;</li> <li>iii) bringing Natural Heritage Features and associated land into public ownership in accordance with the policies of Section 6.4 (Natural Heritage Systems) of this Plan; and</li> <li>iv) negotiating conservation easements.</li> </ul> <p>Nothing in this Plan shall be construed as compelling the County to purchase any private land as a means to achieve these policies.</p> <p>e) The County supports the continuity of Ontario’s South Coast Scenic Route as a slow-travel scenic route and shall look at opportunities to</p>

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	<p>decrease speed limits where appropriate to address safety concerns related to visibility and road alignment.</p> <p>f) The County shall promote the creation of a safe environment and monitor the level and volume of traffic along Ontario's South Coast Scenic Route to ensure adequate traffic movement and safety, in this regard the County shall strictly control the number and locations of access points along Ontario's South Coast Scenic Route.</p> <p>g) The County shall promote the creation of an aesthetically pleasing route, with such attention to building orientation, access and egress, outside storage, landscaping, fencing, and signage.</p> <p>h) The County shall work with the interested parties and relevant agencies to look at measures for the Long Point Causeway to address pedestrian, animal and reptile safety, as well as possible roadway enhancements benefiting roadway safety and improved trail and recreational opportunities, and the water quality of the adjacent Big Creek Marsh and Long Point Bay.</p> <p>i) The County shall promote the creation of the on-road/off-road recreational trail component to Ontario's South Coast Scenic Route, to achieve the following objectives:</p> <ul style="list-style-type: none"> <li>i) seek opportunities to bring it closer to the lakeshore, in consideration of the public interest to provide access to the lakeshore, more diverse recreational opportunities, and protection and enhancement of sensitive natural environmental features, and agricultural areas;</li> <li>ii) consider implementing recreational trail components to Ontario's South Coast Scenic Route when resurfacing or upgrading the roadways;</li> <li>iii) where development along Ontario's South Coast Scenic Route of the lakeshore is proposed, a strip of land along Ontario's South Coast Scenic Route or along the shoreline above the stable top-of-bank should be dedicated by the landowner to the County to form part of the recreational trail; and</li> <li>iv) promote connections with other trails in proximity to Ontario's South Coast Scenic Route, including the Lynn Valley Trail in Port Dover.</li> </ul> <p>j) The County shall support the preparation of a County Trails Master Plan and associated design guidelines in accordance with the policies of Section 11.7.3 (County Trails Master Plan).</p> <p>k) The County shall encourage the protection of views and vistas along Ontario's South Coast Scenic Route, through their review of development applications along the route, particularly along the south side of the route where views of the lakeshore are prominent. In this regard, development applications shall be reviewed and evaluated based on minimizing their potential impact on views and the landscape, particularly with respect to building profile, height, massing, materials and colour.</p> <p>l) Signage along Ontario's South Coast Scenic Route should orient travellers to the lakeshore's attractions.</p>

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<p><b>11.7.2.2 Trails and Cycling</b></p> <p>This Plan promotes the creation of a comprehensive trails and cycling network to achieve the County’s objectives with respect to tourism and economic development, promoting healthy lifestyles, and the recreational opportunities associated with the lakeshore. The County shall support the preparation of a County Trails Master Plan.</p>	<p>The following shall be the policy of the County:</p> <ul style="list-style-type: none"> <li>a) The County shall support the preparation of a County Trails Master Plan in accordance with the policies of Section 11.7.3 (County Trails Master Plan).</li> <li>b) In addition to, or in combination with Ontario’s South Coast Scenic Route, the County shall support the creation of an off-road pedestrian walking and hiking footpath, established along the lakeshore between Ontario’s South Coast Scenic Route and Lake Erie, as generally identified on Schedule “E”.</li> <li>c) The County shall work to bring accessible land and trails along the lakeshore into public ownership for the use and enjoyment of County residents and visitors. Acquisition of the land for the development of Ontario’s South Coast Scenic Route shall be achieved through a number of means including, but not limited to: <ul style="list-style-type: none"> <li>i) the natural heritage policies of Section 6 (Sustainable Natural Heritage) of this Plan;</li> <li>ii) acquisition through the development approvals process;</li> <li>iii) bringing Natural Heritage Features and associated land into public ownership in accordance with the policies of Section 6.4 (Natural Heritage Systems) of this Plan; and</li> <li>iv) negotiating conservation easements.</li> </ul> Nothing in this Plan shall be construed as compelling the County to purchase any private land as a means to achieve these policies. </li> </ul>
<p><b>11.8.2.1 Sustainable Neighborhood Design</b></p> <p>Leadership in Energy and Environmental Design for Neighbourhood Design (LEED ND) is a set of standards for neighbourhood location and design based on the principles of smart growth, new urbanism, and green building. It provides a rating system that is voluntary and grounded in energy and environmental principles. LEED ND, prepared by the Canadian Green Building Council, promotes numerous sustainability principles to encourage the design of sustainable neighbourhoods. LEED ND provides a tool for assessing and rewarding environmentally superior development practices within the rating framework of the LEED Green Building Rating System.</p> <p>This Plan promotes air, water and energy efficiency to promote the environmental sustainability of the lakeshore, which may be achieved through land use and transportation strategies to reduce vehicle emissions, appropriate neighbourhood, building and landscape designs.</p>	<p>The following shall be the policy of the County:</p> <ul style="list-style-type: none"> <li>a) The County shall encourage the design of sustainable neighbourhoods in keeping with LEED ND sustainability principles.</li> <li>b) The County shall promote the use of the LEED ND Project Checklist and encourage the consideration and application of principles in the design and development of neighbourhoods</li> <li>c) The following criteria should be addressed in the development of sustainable neighbourhood forms: <ul style="list-style-type: none"> <li>i) the creation of more compact, walkable oriented neighbourhoods;</li> <li>ii) the promotion of mixed use developments as opposed to isolated single-use commercial or institutional complexes;</li> <li>iii) the promotion of the creation of human scale environments;</li> <li>iv) the provision of opportunities for residents daily activities to occur within walking distances;</li> <li>v) the creation of a modified grid pattern with interconnected road networks; and</li> <li>vi) the continuation or extension of existing street patterns and neighbourhood forms where feasible and appropriate.</li> </ul> </li> <li>d) The following measures shall be promoted to improve air and energy efficiency: <ul style="list-style-type: none"> <li>i) encourage higher density uses along arterial and collector roads;</li> </ul> </li> </ul>

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	<ul style="list-style-type: none"> <li>ii) provide bicycle facilities, particularly in combination with higher density uses and large traffic generating uses;</li> <li>iii) promote an interconnected street network to minimize trip lengths;</li> <li>iv) identify a strategy to accommodate public parking facilities at key locations to minimize vehicular trips;</li> <li>v) promote the development of green roofs to improve energy efficiency;</li> <li>vi) promote the use of porous pavement materials to increase water infiltration;</li> <li>vii) encourage the reduction of energy and residential combustion emissions through a range of approaches including the development of R-2000 homes and similar commercial construction standards such as LEED, and the incorporation of Energy Star appliances;</li> <li>viii) review the layout of new developments to promote energy conservation measures;</li> <li>ix) encourage landscape design that promotes the use of native species and enhancement of ecological functions; and</li> <li>x) encourage the development of buildings powered by renewable energy systems, both passive and active.</li> </ul> <p>e) The County shall support the promotion and identification of dark sky communities, thereby reducing light emissions to the sky and encouraging energy conservation initiatives, through the use of directional, low-level lighting and other means, where appropriate.</p>
<p><b>11.8.2.2 Streetscape</b></p> <p>While streets provide for the primary movement of traffic and pedestrians they also facilitate the daily interactions of residents, neighbours and visitors and define our experience of the public realm. Streets are important public spaces which shape our perception of the environment, and as such, they should be carefully designed to enhance our sense of community. Streetscapes consist of various elements, including streets and public frontages and are defined by the character of the sidewalks, curbing, planters, trees, landscaping and signage.</p>	<p>The following shall be the policy of the County:</p> <ul style="list-style-type: none"> <li>a) Sidewalks shall be generally provided to ensure public safety where pedestrian traffic warrant them and they do not conflict with the rural character of the environment.</li> <li>b) Suitable lighting shall be provided for the illumination of vehicles, pedestrians and cyclists, while reducing light emissions to the sky in support of the promotion of dark sky communities.</li> <li>c) Street furniture, including lighting, signage, bicycle parking facilities, benches, newspaper boxes, utilities, and garbage facilities shall be designed and placed within a consistent form, pattern, shape and material to avoid clutter and facilitate readability.</li> <li>d) Public and private utilities within the streetscape shall be grouped or clustered where possible.</li> <li>e) The length of blocks shall contribute to a more pedestrian environment by encouraging short and regular block lengths to make walking efficient and allow variation in routes.</li> <li>f) The connectivity between sidewalks and trails shall be promoted.</li> <li>g) The opportunity for cyclists to travel either on the roadway or on separated bicycle paths shall be recognized in the design of streets.</li> <li>h) The comfort of the pedestrian environment shall be considered, particularly with respect to the provision of appropriate shade, lighting and shelter.</li> </ul>

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	<p>i) Provide features which contribute to the definition of public open spaces, through the framing of views and focal points, direction of pedestrian movements, and demarcation of different public spaces and functions.</p>
<p><b>11.8.3 Neighborhood Planning</b></p> <p>A Neighbourhood Plan shall be prepared for the Neighbourhood Planning Areas identified on Schedule “H” prior to development occurring, to ensure orderly and efficient development patterns. Neighbourhood Plans shall provide a tool to develop more detailed land use plans and specific policies to guide and manage the future development of these areas in an integrated and the comprehensive manner that promotes the creation of complete communities.</p>	<p>The following shall be the policy of the County:</p> <p>a) A Neighbourhood Plan shall be prepared for the Neighbourhood Planning Areas identified on Schedule “H” prior to any development occurring, which include:</p> <p>i) Port Dover:</p> <ul style="list-style-type: none"> <li>• Port Dover West Neighbourhood Planning Area; and</li> <li>• North Dover Mills Neighbourhood Planning Area.</li> </ul> <p>b) The Neighbourhood Plan shall demonstrate conformity with the policies and Schedules of this Plan, and shall provide more detailed guidance with respect to the following matters:</p> <p>i) development coordination and extension of transportation, servicing and community infrastructure;</p> <p>ii) the phasing and timing of development to ensure orderly growth;</p> <p>iv) the detailed road pattern;</p> <p>v) the density and distribution of housing types and a range of housing forms;</p> <p>ix) the sustainable neighbourhood and building design policies as outlined in Section 11.8.2 (Sustainable Neighbourhood and Building Design)</p> <p>c) The County shall encourage the design of sustainable neighbourhoods in keeping with LEED ND sustainability principles, in accordance with the policies of Section 11.8.2.1 of this Plan.</p> <p>h) The following supporting studies may be required in the preparation of a Neighbourhood Plan, at the discretion of the County:</p> <p>i) Transportation Study;</p> <p>ii) Functional Servicing Study;</p> <p>iii) Community Services and Facilities;</p> <p>iv) Environmental Impact Study; and</p> <p>v) Other studies as determined by the County.</p>

## 2.3 INTEGRATED SUSTAINABLE MASTER PLAN (ISMP)

The Transportation Strategy for Norfolk County's ISMP sets forth a set of principles and guidelines for maintaining and developing existing and future transportation infrastructure, identifying infrastructure requirements to the 2041 horizon year.

### 2.3.1 ISMP - Existing Conditions

Figure 6 shows the identification of the primary, secondary, and tertiary road network. These important links are needed for the efficient movement of goods and people, and will also be used to determine the priority of road network improvements



**FIGURE 6: EXCERPT FROM ISMP APPENDIX L FIGURE 5-3 – EXISTING ROAD RATIONALIZATION**

There is no existing goods movement network, but the following section on transportation recommendations identifies a proposed network integrated with the overall road network and active transportation hierarchy.

A road network assessment was undertaken by the County to forecast network capacity under future conditions. The study found that only 1.2% of total vehicle kilometres traveled and 1.1% of vehicle hours traveled are spent in congestion, indicating little congestion in the network. Average travel time within the network is completed within approximately 20.88 minutes per trip within the peak hour period, with the majority of trips made within 25 minutes.

## 2.3.2 ISMP - Recommendations

**TABLE 3 ISMP IMPROVEMENTS TO BE EVALUATED AND RECOMMENDED TIMELINES**

Improvement	Horizon	Description
North-South Traffic east of Simcoe and north of Port Dover	Long Term (16 – 25 Years)	<ul style="list-style-type: none"> <li>Cockshutt Intersection turn lanes and signalization between Concession 12 Townsend and Dover Mills Rd</li> </ul>

**TABLE 4 ISMP TRANSPORTATION STRATEGY RECOMMENDATIONS**

Phasing	Name	Improvement Type	Location	Description
<b>Short Term (0 – 5 Years)</b>	Traffic Control Guidelines	Policy	County-Wide	Adoption of OTM Books for traffic control device and system standards within the County.
	Traffic Calming	Policy	County-Wide	Develop County-specific Traffic Calming Policy based on the Canadian Guide to Neighborhood Traffic Calming, and other municipality specific guidelines.
	Special Event Protocol	Policy	County-Wide	Develop Special Events protocol to accommodate regularly scheduled tourist events, in consultation with tourism agencies and OPP.
	TIS Guidelines	Design Criteria	County-Wide	Adopt TIS Guidelines as shown in <b>Appendix J</b> as part of a new subsection of the Norfolk County Design Guidelines.
	Sidewalks	Design Criteria	County-Wide	Include a requirement for sidewalks on both sides of major and minor roads. Change Section 6.6.00 of the Design Criteria to reflect this recommendation.
	Roundabouts	Design Criteria	County-Wide	Adopt the text and figure provided in Appendix K as part of Section 6 of the Design Criteria, to reflect the standards to be used in the County for roundabout selection and construction.
	Designate Goods Movement Network	Policy	County-Wide	Designate Goods Movement network as proposed in Section 5.2.1.
	Designate Road Rationalization Network	Policy	County-Wide	Designate Primary, Secondary and Tertiary Road networks as proposed in Section 5.2.1, indicating the relative importance of roads within the County to be used for future prioritization of maintenance and improvement plans.
<b>Medium Term (6 – 15 Years)</b>	Transportation Strategy Update	Policy	County-Wide	The conclusions of this Transportation Strategy should be reviewed on a 5-year basis to ensure that the assumptions are still valid.
<b>Long Term (16 – 25 Years)</b>	Reconstruct Goods Movement Network	Infrastructure	Various County-Wide	At the appropriate time for rehabilitation and reconstruction, the roads identified as goods movement corridors should be reviewed to determine whether they need to be reconstructed to a standard which would preclude the need for a half-load restriction. The need would be based on industry requirements in the adjacent industrial lands. See Fig X below.

### 2.3.2.1 Proposed Road Network Hierarchy

Figure 7 identifies the recommended goods movement network.

Figure 8 identifies the recommended transportation network that encompasses the road network, the proposed goods network, and the proposed active transportation route.



**FIGURE 7: EXCERPT FROM ISMP APPENDIX L FIGURE 5-4 – RECOMMENDED GOODS MOVEMENT**



**FIGURE 8: EXCERPT FROM ISMP APPENDIX L FIGURE 5-30 – RECOMMENDED GOODS MOVEMENT, ROAD IMPROVEMENTS, AND AT HIERARCHY**

## 2.4 ACTIVE TRANSPORTATION STRATEGY (AT STRATEGY)

The Active Transportation (AT) Strategy is a subset of the ISMP that outlines in greater detail the existing conditions and proposed strategies to achieving an integrated AT network.

The AT objectives are:

- **Build on Successes:** reinforce policies, plans and recommendations from the 2009 Trails Master Plan
- **Provide Policy Support:** establish supportive planning and design policies and guidelines for AT
- **Create a Connected System:** identify connections between community destinations (e.g schools) to create a continuous system of facilities
- **Design for Safety & Comfort:** design facilities with safety and comfort in mind as well as various user groups (e.g youth)
- **Identify Priorities:** identify short, medium and long-term priorities for implementation
- **Increase Awareness:** develop strategies and actions that increase awareness and educate people on AT options

### 2.4.1 AT - Existing Conditions

There are currently 9 km of paved shoulder, 153 km of sidewalks, and 289 km of off-road trails throughout the County. Port Dover's existing AT facilities consist of regional trails, cycling tour routes, community destinations, and supportive signage as shown in Figure 9 below.



FIGURE 9: EXCERPT FROM ISMP APPENDIX M MAP 1-B – EXISTING AT CONDITIONS



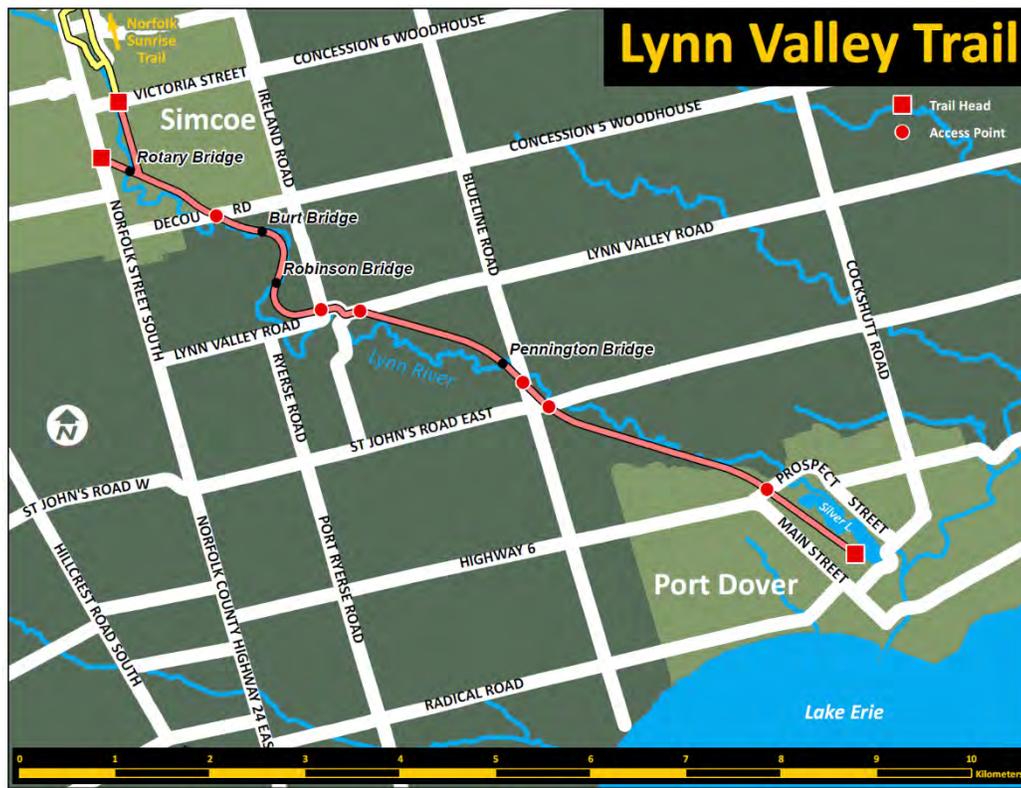


FIGURE 11: EXCERPT FROM NORFOLK COUNTY RAIL TRAILS – LYNN VALLEY TRAIL



**FIGURE 12: EXCERPT FROM AT STRATEGY – HEAT MAPPING OF DOCUMENTED CYCLING ROUTES IN NORFOLK COUNTY**

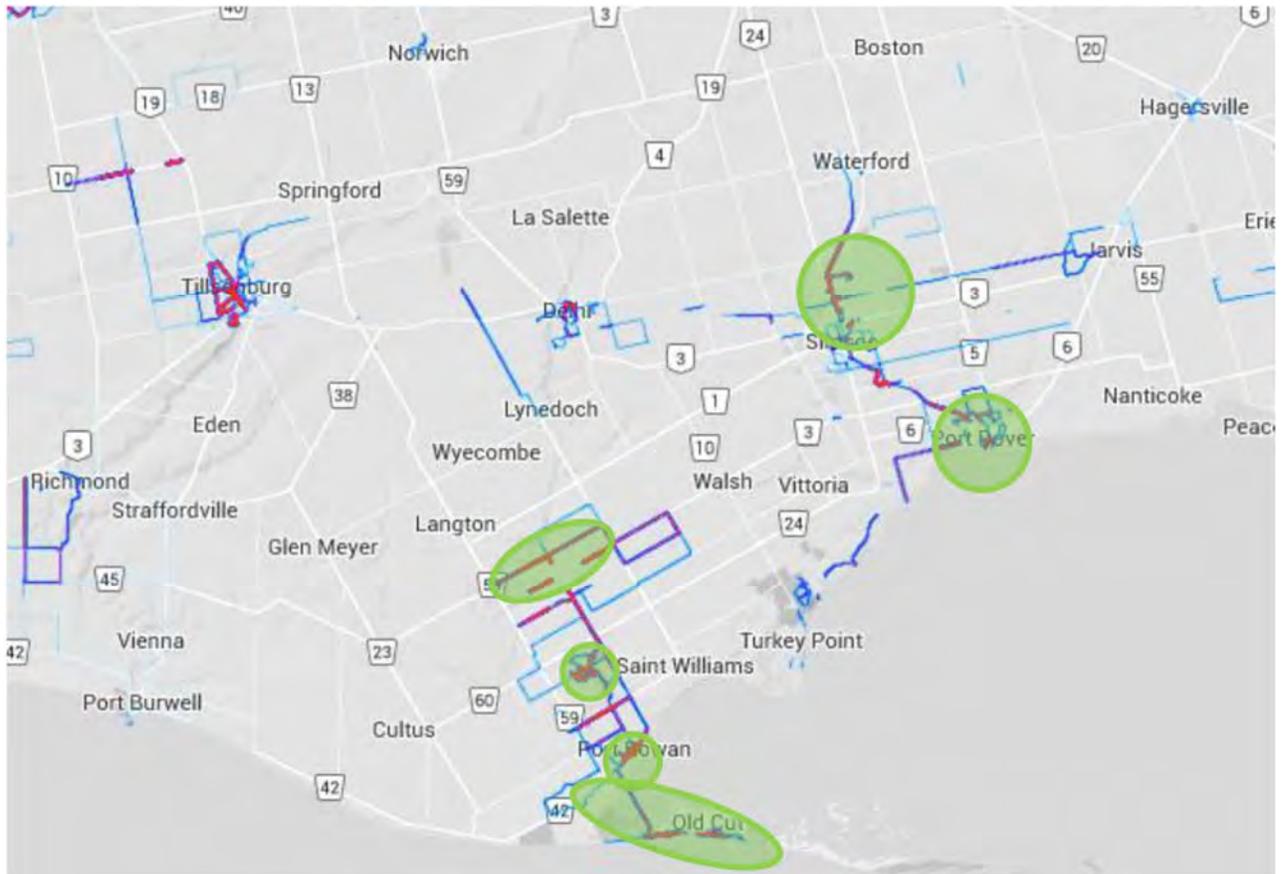
### 2.4.1.2 Running Routes

There are 4 different asphalt running routes in Port Dover. Figure 13 shows routes with distances of 10 km, 15 km, 20 km, and 30 km.

Figure 14 shows HEAT mapping of documented running routes throughout the County.



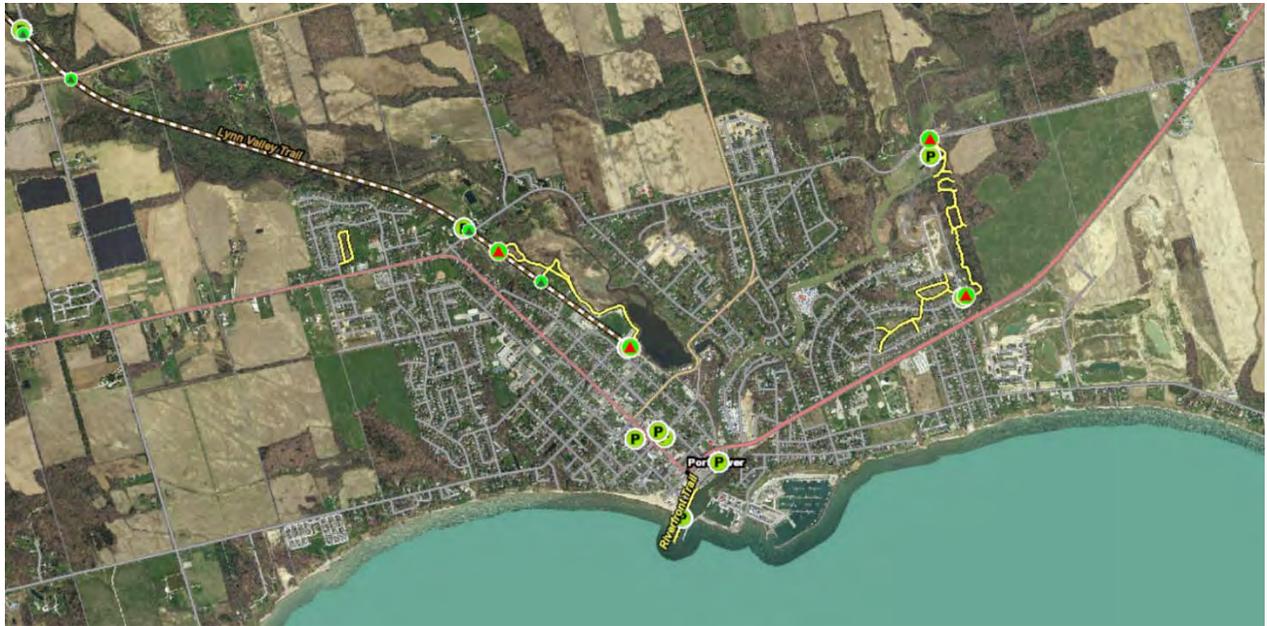
**FIGURE 13: NORFOLK COUNTY ONLINE GIS MAP – RUNNING ROUTES**



**FIGURE 14: EXCERPT FROM AT STRATEGY – HEAT MAPPING OF DOCUMENTED RUNNING ROUTES IN NORFOLK COUNTY**

### 2.4.1.3 Supportive Signage and Parking Facilities

Supportive signage have been implemented at trail heads along with trail parking facilities as illustrated in Figure 15.



**FIGURE 15: NORFOLK COUNTY ONLINE GIS MAP – SUPPORTIVE SIGNAGE AND PARKING FACILITIES**

### 2.4.1.4 Existing Sidewalks Gaps

Sidewalks are considered a key component in the AT network and one of the primary facilities to accommodate pedestrians. They are also one of the main facilities that provide direct access to schools.

Figure 16 identifies existing sidewalk gaps in the network.



**FIGURE 16: EXCERPT FROM ISMP APPENDIX M MAP 5-A – EXISTING SIDEWALK GAPS**

## 2.4.2 AT - Proposed AT Strategy

The AT Strategy identified preferred facility types along the AT routes by taking into consideration both physical and operational characteristics that are unique to the context of the proposed facility design.

Figure 17 illustrates the proposed hierarchy of AT routes, with the primary route running along New Lakeshore Road, Hamilton Road as it transitions into Highway 6, and Silver Lake Drive.

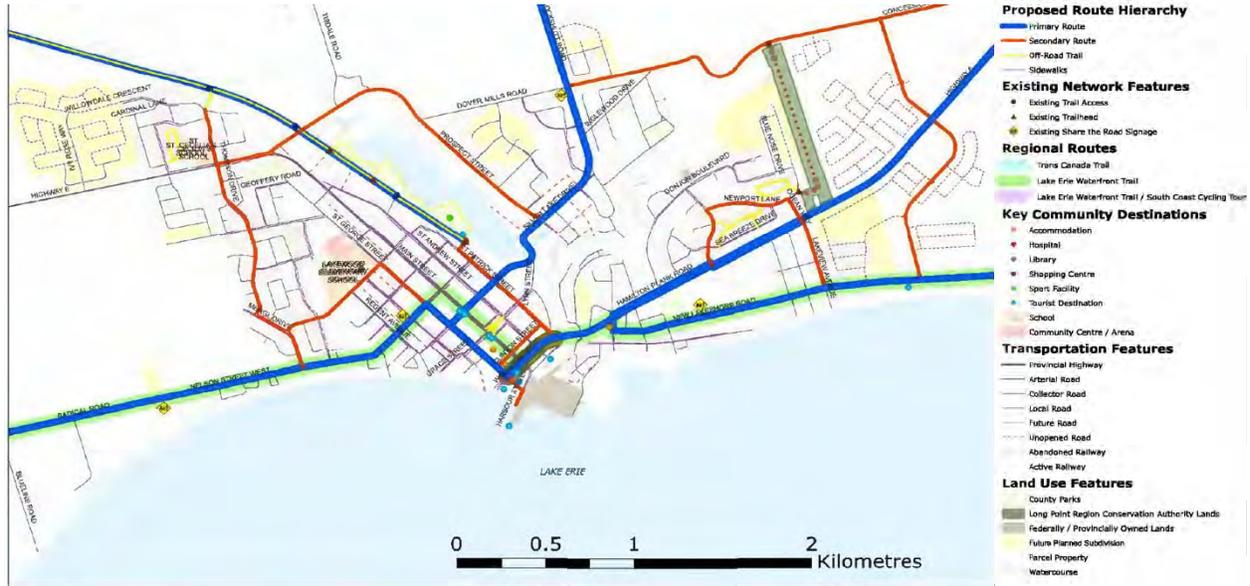


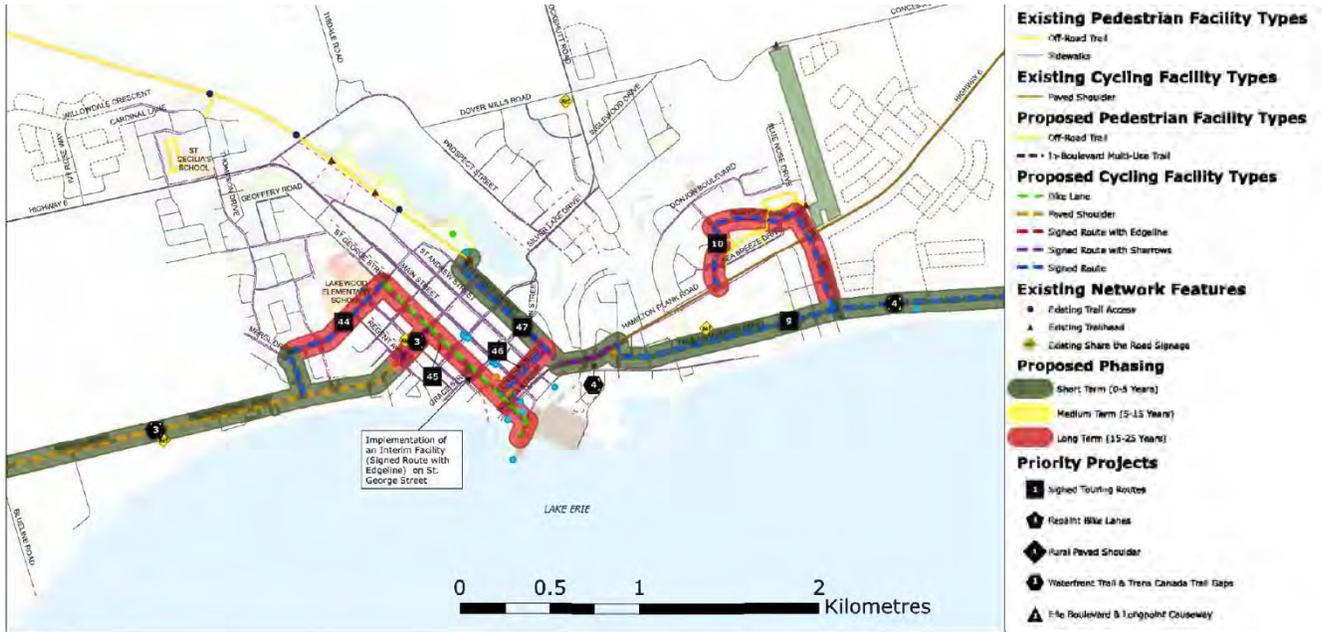
FIGURE 17: EXCERPT FROM ISMP APPENDIX M MAP 3-B – PROPOSED ROUTE HIERARCHY

Figure 18 illustrates the proposed AT facility types.



FIGURE 18: EXCERPT FROM ISMP APPENDIX M MAP 4-B – PROPOSED FACILITY TYPES

Figure 19 illustrates the proposed priorities and phasing for County-wide implementation of the AT strategy.



**FIGURE 19: EXCERPT FROM ISMP APPENDIX M MAP 8-B – PROPOSED AT PRIORITIES AND PHASING**

### 2.4.2.1 AT Proposed Cycling Facility Types

The proposed cycling facility types in Port Dover are identified below in Table 5.

**TABLE 5 AT CYCLING FACILITY TYPES**

Location	Pre-Selection Design		Final Design	Rationale
St. George Street / Harbour Street	Shared		Bike Lane	<ul style="list-style-type: none"> <li>Provides key north-south link through Port Dover and connects residents to key destinations (e.g. school, shops, waterfront, etc.)</li> <li>Interim solution proposed (signed route with edgeline) as opportunity to assess user interest in a long-term designated cycling facility.</li> <li>Sufficient road space to implement 1.5m bikes</li> </ul>

### 2.4.2.2 AT Interim Facilities

AT interim facilities have been identified in Table 6 due to a combination of factors ranging from insufficient space, environmental constraints, unavailable land, or a lack of available budget within the preferred timeline.

**TABLE 6 AT INTERIM DESIGN FACILITY SOLUTIONS**

Route Segment & Description	Interim Solution	Preferred Solution	Length (km)
St. George Street (Nelson Street to Clinton Street)	Signed Route with Edgeline	Bike Lane	0.55
St. George Street (Greenock Street to Nelson Street West)	Signed Route with Edgeline	Bike Lane	0.36

### 2.4.2.3 AT Sidewalk Implementation

Priority is given to designated AT routes that currently exist within the "walkable" (1.6 km) community near local schools for a more connected pedestrian network. Table 7 lists the planned sidewalk implementation projects that should be given priority.

**TABLE 7 AT PRIORITY SIDEWALK IMPLEMENTATION PROJECTS**

Project Description	Length (km)
Chapman Street West (St. George Street to St. Annie Street North)	0.2
Clinton Street (St. George Street to St. Patrick Street)	0.0
Concession 2 Woodhouse (Cockshutt Road to Urban Limit)	1.9
Future Planned Road in Port Dover (Concession 2 Woodhouse to New Lakeshore Road)	1.9
Prospect Street (Main Street to Silver Lake Road)	0.5
Silver Lake Drive / Cockshutt Road (Dover Mills Road to Prospect Street)	0.9
Somerset Dr / Newport Ln / Ocean Wy / Lakeview Ave (Hamilton Plank Road to New Lakeshore Road)	0.6
St. George Street (Clinton Street to Harbour Street)	0.3
Willowdale Cres/ Ivey Rose W/Cardinal Ln (Willowdale Crescent to Main Street)	0.3

## 2.5 NORFOLK COUNTY TRAILS MASTER PLAN (TRAILS MP)

### 2.5.1 Trails MP - Existing Trail Network

Port Dover contains a few urban trails with varying lengths and ownership as illustrated in Figure 20 below:

- Lynn Valley Trail – 10 km, owned by the Lynn Valley Trail Association
- Lion's Trail – 1.74 km, owned by Port Dover Lion's Club
- Powell Park Trail – 0.23 km, owned by Norfolk County
- Riverfront Trail – 0.52 km, owned by Norfolk County
- Somerset Trail – 1.19 km, owned by Norfolk County
- Black Creek Trail – 1.86 km, owned by Long Point Region Conservation Authority (LPRCA)

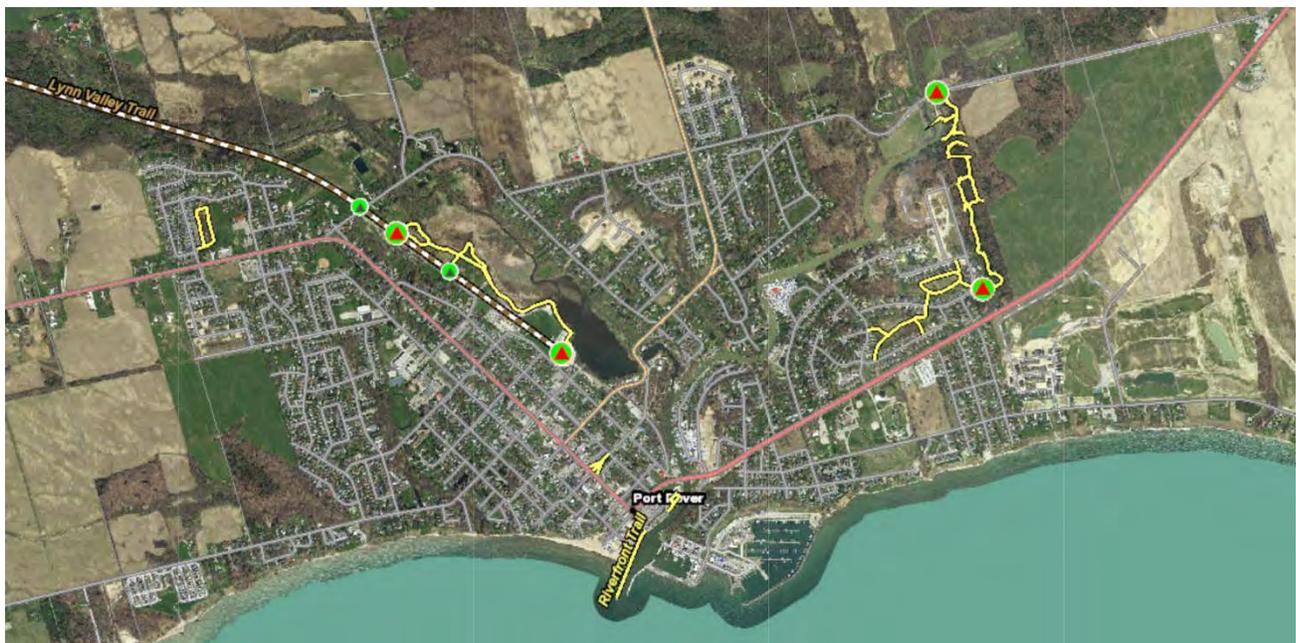
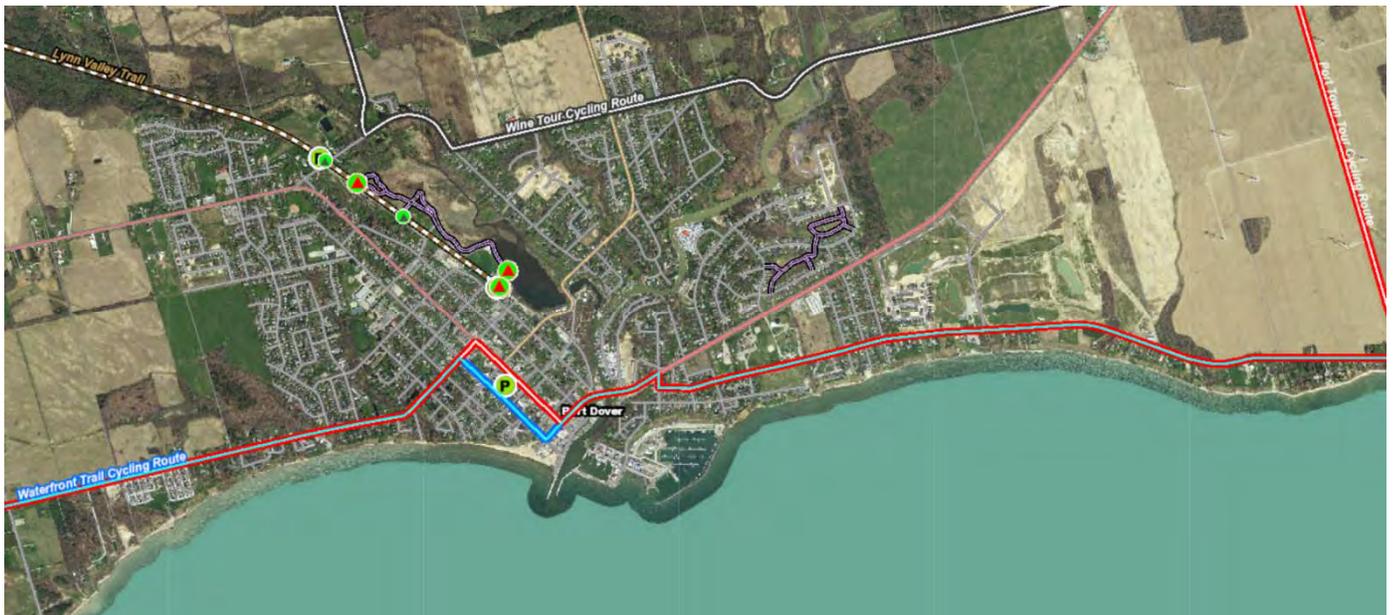


FIGURE 20: NORFOLK COUNTY ONLINE GIS MAP – TRAIL NETWORK

## 2.5.2 Trails MP - Existing Cycling Network

Port Dover's existing cycling network consists of the following regional cycling route, cycling tour routes from local interest groups, and local off-road trails as shown in Figure 21:

- Waterfront Trail Cycling Route – on-road waterfront cycling route; 64 km, mostly asphalt
- Port Town Tour Cycling Route – on-road cycling route; 60 km; asphalt
- Wine Tour Cycling Route – on-road cycling route; 115 km; asphalt
- Lynn Valley Trail – off-road cycling trail; asphalt and crushed stone
- Lion's Trail – off-road cycling trail; compacted soil
- Somerset Trail – off-road cycling trail; gravel



**FIGURE 21: NORFOLK COUNTY ONLINE GIS MAP – CYCLING ROUTES**

Cycling routes shown are suggested routes promoted by local cycling groups and may not include signage or paved shoulder.

## 2.5.3 Trails MP - Future Trail Links

### 2.5.3.1 Trails MP Recommendations

The New Lakeshore Road has been identified as a future link to the Haldimand County trail network. Recommendations were posed according to the guiding principles and strategic directions of the master plan. Table 8 identifies the recommendations related to connectivity and Table 9 identifies the recommendations related to accessibility and affordability.

**TABLE 8 NORFOLK COUNTY TRAILS MP – CONNECTIVITY RECOMMENDATIONS**

Guiding Principle 4 – Connectivity					
The County and its trail partners will make connecting trails between towns and villages within the County and to adjoining counties a priority.					
Strategic Directions					
4.1 Connect neighborhoods with urban and rural trails throughout the County 4.2 Link the County trails system to the Trans Canada Trail 4.3 Link the County trails system to adjacent counties					
Recommendations	Timing (Years)			Priority Level	Resource Requirements
	0 – 2	3 – 5	5 – 7		
Identify that an integrated trail network connecting trails between towns and villages within the County and to adjoining counties is a priority. Each linked section should keep its own community identity.	x	x	x	HIGH	County/Community Partners Volunteer time Operating / Capital monies to support development (levy/external)
Link the trail system to existing and future parks and trails.	x	x	x	HIGH	County staff coordination Operating / Capital monies to support development (levy/external)
Use sidewalks wherever possible as a link to the trail system.	X	x	x	HIGH	County staff coordination Operating / Capital monies to support development – signage (levy/external)
Ensure that subdivision planning and development policies and agreements identify and designate potential trail linkages to existing and future trails.	x	x	X	HIGH	County staff coordination Partnership with Developers Operating / Capital monies to support development – signage (levy/external)
Link the main trails in Norfolk to the Trans Canada Trail.	x	x	x	HIGH	County staff coordination

					Volunteer time Operating / Capital monies to support development – signage (levy/external)
Develop a future trail link East to Haldimand County via New Lakeshore Road and via Concession 14.	x			HIGH	County staff coordination Volunteer time Operating / Capital monies to support development – signage (levy/external)
Develop a future trail link West to Elgin County via Lakeshore Road.	x			HIGH	County staff coordination Volunteer time Operating / Capital monies to support development – signage (levy/external)
Develop the Lake Erie Lakeshore trail from Haldimand County to Elgin County.	x			HIGH	County staff coordination Volunteer time Operating / Capital monies to support development – signage (levy/external)

**TABLE 9 NORFOLK COUNTY TRAILS MP – ACCESSIBILITY AND AFFORDABILITY RECOMMENDATIONS**

<b>Guiding Principle 5 – Accessibility and Affordability</b>					
The County and its trail partners will endeavor to make trails reasonably accessible and affordable to all residents and visitors while recognizing the divergent uses and expectations.					
<b>Strategic Directions</b>					
5.1 Facilitate access to trail development where the need is the greatest. 5.2 Provide trails for divergent uses where and when possible. 5.3 Provide access to the Lake Erie waterfront and rivers. 5.4 Eliminate barriers for persons with disabilities.					
<b>Recommendations</b>	<b>Timing (Years)</b>			<b>Priority Level</b>	<b>Resource Requirements</b>
	<b>0 – 2</b>	<b>3 – 5</b>	<b>5 – 7</b>		
Develop an ongoing program to develop trails for all users – walkers, hikers, cyclists, geocachers, equestrian riders, ATV and trail riders, cross country skiers and snowshoers. As well, trails for canoes and kayak should be developed.	x	x	x	HIGH	County staff facilitation Volunteer time Capital monies to support land acquisition/construction Operating monies - signage/mtce costs (levy/external)
Develop on-road bicycle paths when redeveloping roadways throughout the County.	x	x	x	HIGH	County staff facilitation Capital monies to support engineering/construction Operating monies - signage/mtce costs (levy/external)
Develop trails off of rail corridors, in crown land, woodlots and road right of ways for controlled ATV and trail bike use. Consider “twining” active and abandoned railway rights of way for ATV and trail bike use.	x			HIGH	County staff facilitation Agency/Corporate Partnerships Capital monies to support land acquisition/construction Operating monies - signage/mtce costs (levy/external)
Revise trails standards to include water trails and be consistent with the trail standards developed by the Ministry of Natural Resources.	x			HIGH	County staff coordination Agency Partnership
Develop trails along waterways and the Lake Erie shoreline.	x	x	x	HIGH	County staff facilitation Volunteer time Capital monies to support land acquisition/construction Operating monies - signage/mtce costs (levy/external)

### 2.5.3.2 Trails MP Barriers to Trail Development

Throughout the public process, barriers to trail development were identified below:

- time of year and seasonal conditions, including weather, insects and competing uses (ie; ATV, trail bike, hunting);
- management / restriction upon motor vehicle noise and associated air pollution; the high lease costs with Ontario Realty Corporation for the use of ORC lands for trails;
- the high cost of insurance and the liability associated with trails; the lack of county-wide connectivity of existing trails;
- the lack of sidewalks which could provided need connectivity; the lack of accessibility for certain users;
- the incompatibility of trails and some uses within agricultural areas;
- unauthorized ATV and trail bike use & degradation of trails by their use;
- highways are seen as barriers;
- the Fence Act is an overhead cost that is too expensive for some groups;
- adjacent property owners that are seen as difficult and the supposed infringement of their property rights and the loss of enjoyment and privacy;
- the lack of an implemented Ministry of Natural Resources St. Williams' Conservation Reserve Management Plan;
- the lack of funding for acquisition, development and maintenance of trails and the high overhead maintenance costs; the lack of security - vandalism of gates and fences;
- the lack of parking and washrooms; and;
- the lack of public awareness of existing trails.

## 2.6 COUNTY OF NORFOLK PARKS, FACILITIES AND TRAILS MASTER PLAN

The Parks, Facilities and Trails Master Plan is intended to recommend policy frameworks and servicing strategies for the delivery and evaluation of the Norfolk County Parks, Facilities and Recreation Services for the next 20 years. The recommendations of this Master Plan are presented in Table 10

### 2.6.1 Parks, Facilities and Trails MP - Recommended Policy Framework

**TABLE 10 PARKS, OPEN SPACES, COMMUNITY TRAILS AND WATERFRONT RECOMMENDATIONS**

Section	Recommendation
<b>Parks and Open Space Development</b>	<p>The following should be undertaken when parks and open spaces are developed, acquired and managed:</p> <ul style="list-style-type: none"> <li>• <b>Ensure that Community Parks have sufficient trails</b>, trail heads and linkages, which could be located near woodlots, natural features and other passive areas.</li> <li>• Provide residential areas with <b>Neighbourhood Parks</b>, preferably walking or biking distance away.</li> <li>• Within <b>Open Space and Natural Heritage areas ensure that there is significant trail connectivity between communities</b>, neighbourhoods, key nodes (cultural, recreational tourism, and agricultural areas), waterfront, and downtown areas.</li> <li>• <b>Increase the number of Lakeshore Parks</b> within Norfolk County. This will allow enhanced opportunities for water based recreation, passive and active recreation near the waterfront, and connections between waterfront areas and downtown centres.</li> <li>•</li> </ul>
<b>Provisioning Standards</b>	<p>In order to ensure that the County is well served with parks and open space amenities. The County's focus should be on:</p> <ul style="list-style-type: none"> <li>• <b>Determining locations</b> for additional <b>multi-use trails on abandoned railways and other linear trail connections</b>.</li> <li>• <b>Ensuring the addition of amenities for passive recreation</b> in parks and open spaces, such as opportunities for <b>wildlife observation, walking, hiking, biking, and canoeing or kayaking</b>.</li> </ul>
<b>Parkland Dedication</b>	<p>It is recommended that Norfolk County accepts parkland dedications that:</p> <ul style="list-style-type: none"> <li>• Meet the park and open space hierarchy and provisioning standards of the Official Plan and the Parks, Facilities and Recreation Master Plan.</li> <li>• Effectively support the development of park based facilities and amenities in accordance with the parks hierarchy and development strategy.</li> </ul>
<b>Parks and Waterfront Improvements</b>	<p><b>Neighbourhood and Community Parks</b></p> <ul style="list-style-type: none"> <li>• Due to a County-wide deficit in neighbourhood parks, utilize brownfield sites for potential new neighbourhood parks – this is especially true in communities such as Simcoe and Port Dover where most of the population growth is anticipated.</li> <li>• Ensure the addition of neighbourhood park amenities such as playgrounds, seating, trails, shade structures, planting design elements, and sports fields to existing community parks in all of the urban communities within Norfolk County. This allows community parks to provide neighbourhood park amenities to residents, especially in</li> </ul>

	<p>Courtland, Langton, Port Rowan, and Waterford.</p> <ul style="list-style-type: none"> <li>• Ensure that in all urban communities within Norfolk County, schools are utilized for additional park amenities, such as sports fields where feasible, and where demand and community initiative warrants a partnership, for example for skateboard facilities.</li> <li>• Ensure that facilities which attract challenging issues at night, such as skateboard parks, are located on arterial streets, well lit and away from residential areas.</li> </ul> <p><b>Accessibility and Special Age Groups</b></p> <ul style="list-style-type: none"> <li>• Ensure that accessibility is integrated into park design and development, focusing on solutions for an aging population. Incorporate adequate and accessible seating, relatively flat trails, amenity buildings with washroom facilities, and exercise stations.</li> <li>• Develop a plan to update and renew park amenities and infrastructure. Replace aging playgrounds with an increasingly creative design.</li> </ul> <p><b>Lakeshore/Waterfront Parks</b></p> <ul style="list-style-type: none"> <li>• Ensure to provide public access to the lakeshore through the development of trails, lookouts, access points, beach areas, marina areas, promote recreational opportunities associated with the lakeshore. Allow opportunities for water-based and passive recreation and amenities that will enhance tourism.</li> <li>• Support the use of County owned Lake Erie shoreline property for the recreational use of all County residents and tourists.</li> <li>• Redevelop the Port Dover Waterfront Marina, in order to provide increased public access to the waterfront and provide lakeshore park amenities. The Port Dover Marina has the ability to attract both boaters and non-boaters to the waterfront, and provides connections to both the downtown and access to Lake Erie. Specific enhancements should include: <ul style="list-style-type: none"> <li>○ Developing an effective entrance to the Marina</li> <li>○ Building an event centre in a park setting with a pavilion designed to handle both public and private events</li> <li>○ Adding seating in the form of benches and picnic tables</li> <li>○ Improve traffic flow patterns, by adding effective sidewalks, trails and boardwalks</li> <li>○ Add green areas and planting design elements</li> </ul> </li> <li>• Undertake an update to the 2009 Lakeshore Special Policy Area Secondary Plan, incorporate the lakeshore and waterfront recommendations from this Master Plan. Also conduct further research and consultation to propose additional waterfront/lakeshore park and amenity recommendations.</li> </ul>
<p><b>Community Trail Improvements</b></p>	<ul style="list-style-type: none"> <li>• Develop an Active Transportation Plan at the county-wide level, which connects communities, as well as infrastructure such as shoulder widening, safe routes for school, and recreational/commuter bike routes.</li> <li>• Address individual urban communities in an Active Transportation Plan in order to deal with specific barriers and connections.</li> <li>• Engage a community-based group and/or champion, to gain momentum and support which will lead to resources and action regarding trail planning and improvements.</li> <li>• Work with community-based groups and/or champions in order to identify, assess and implement solutions to various County and community-wide trail issues.</li> <li>• Develop additional multi-use trails on abandoned railway corridors, connecting various communities. Proposed future trail links could include: <ul style="list-style-type: none"> <li>○ Concession 14 as well as the new Lakeshore Road which have been identified to link to Haldimand County trails</li> <li>○ Lakeshore Road which has been identified as a link to Elgin County</li> <li>○ Highways 3 and 59 which have been identified as links to Oxford County</li> </ul> </li> <li>• Utilize existing public land owned and managed by the Canadian Wildlife Service, Ontario Ministry of Natural Resources, the Long Point Region Conservation Authority and Norfolk County in order to connect and expand trail networks and recreational opportunities. In addition, take advantage of land that is owned by non-governmental agencies, such as the Nature Conservancy of Canada and the Long Point Land Trust.</li> <li>• Develop trails adjacent to the water along Big Creek and the Lake Erie waterfront.</li> </ul>

	<ul style="list-style-type: none"><li>• Incorporate a consistent, aesthetically fitting and approachable trail signage and wayfinding program. Include a trail map at each major trailhead, plus brochure maps for distribution throughout the County.</li><li>• Provide educational and interpretive signage regarding natural history, culture, heritage and wildlife.</li><li>• Develop rest areas and distance markers along trails, integrate seating at appropriate locations in order to create opportunities for rest and reflection for residents and visitors.</li><li>• Develop a trail maintenance plan including snow clearing/packing guidelines for key community trail connections to clear/pack.</li><li>• Undertake an update to the 2009 Norfolk County Trails Master Plan and incorporate the recommendations from this Master Plan, in relation to trail maintenance and connectivity. As well as undertake consultation and research to propose additional trail improvement, connection and maintenance options.</li></ul>
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## **2.7 NORFOLK COUNTY ZONING BY-LAW 1-Z-2014**

### **2.7.1 Section 4.7 – Loading Requirements**

The Norfolk County Zoning By-Law outlines the minimum dimensions of a loading spaces where loading docks are provided on a lot. An excerpt is provided in Appendix A.

### **2.7.2 Section 4.9 – Number of Parking Spaces**

The Norfolk County Zoning By-Law outlines parking space size and minimum number of parking space by land-use in Section 4.9 of the Off-Street Parking. An excerpt of parking rates are provided in Appendix A.

### **2.7.3 Section 4.11 – Parking in a Central Business District (CBD) Zone**

Parking is prohibited in the front yard for properties within the CBD zone. Furthermore, no parking spaces are required for any lands that are within the CBD.

## 2.8 NORFOLK COUNTY BY-LAW PD13 2018-75 STREET CLOSURES

Date: As enacted and passed by Norfolk County on June 26<sup>th</sup>, 2018.

By-law 2018-75 sets forth temporary street closures and parking restrictions for Port Dover's annual Friday the 13<sup>th</sup> festivities.

All vehicles are restricted from entering downtown Port Dover except for motorcycles (parking is provided in designated areas) and vehicles with a valid Resident Pass. Shuttle buses provide unlimited trips for out-of-town visitors from off-site parking lots to designated stops in the downtown for a fee, running from 8 am on Friday the 13<sup>th</sup> to 4 am on Saturday the 14<sup>th</sup>.

### 2.8.1 Street Closures and Restrictions

The by-law sets forth the following restrictions unless noted otherwise:

- Parking restrictions to come into effect between 3 am to 11:59 pm on Friday the 13<sup>th</sup>
- Traffic flow restrictions to come into effect between 6 am to 11:59 pm Friday the 13<sup>th</sup>
- Road closures to come into effect between 3 am to 11:59 pm Friday the 13<sup>th</sup> except for designated emergency vehicles, vendors, service vehicles or shuttle vehicles

Road closures are required to provide effective emergency vehicle access and safe traffic circulation for the public. Traffic flow plans, closures, and restrictions are outlined in Table 12 for designated streets.

Figure 22 outlines a map from Norfolk County of street closures and restrictions for the 2019 festivities.



FIGURE 22: PORT DOVER FRIDAY THE 13<sup>th</sup> STREET CLOSURES AND RESTRICTIONS

## **APPENDIX A: Norfolk County Parking Rates**



No.	Type of Use	Minimum Parking Requirement
<b>Residential</b>		
a)	single detached, semi-detached, duplex, tri-plex, four-plex, townhouse dwellings and vacation home [8-Z-2017]	2 parking spaces for each dwelling unit
b)	apartment dwelling[8-Z-2017]	1.5 parking spaces for each dwelling unit
c)	dwelling unit in a non-residential building	1 parking space for each dwelling unit
d)	boarding or lodging house	2 parking spaces for each dwelling unit plus 1 parking space for each room for boarders
e)	accessory residential dwelling unit	1 parking space in addition to those required for the primary residential dwelling unit use
<b>Residential Visitor</b>		
f)	<i>four-plex, townhouse dwellings, apartment dwellings or single-detached or semi-detached dwellings as part of a condominium development</i>	1 visitor space for every 3 dwelling units
<b>Non-Residential</b>		
g)	<i>animal hospital or animal kennel</i>	1 parking space for every 25 square metres of usable floor area
h)	arena, auditorium, gymnasium, assembly hall	1 parking space for every 8 fixed seats or stadium, skating rink, or for every 10 square metres of usable floor area where there are no fixed seats
i)	<i>auction centre</i>	1 parking space for every 10 square metres of usable floor area
j)	<i>bar or night club</i>	1 parking space for every 5 square metres of usable floor area
k)	<i>bed &amp; breakfast</i>	1 parking space per room for guests
l)	billiard or pool room	1 parking space for every 10 square metres of usable floor area
m)	bowling alley	2 parking spaces for each bowling lane
n)	college, university or technical institutions	1 parking space for every student enrolled full-time for day courses
o)	curling rink	10 parking spaces per curling sheet
p)	dance hall or banquet hall	1 parking space for every 10 square metres of usable floor area
q)	<i>dry cleaning distribution station</i>	2 parking spaces
r)	<i>farm produce outlet</i>	1 parking space for every 10 square metres of usable floor area
s)	financial institution	1 parking space for every 15 square metres of usable floor area
t)	funeral home	1 parking space for every 10 square metres of public assembly area
u)	<i>group home, retirement home [7-Z-2018]</i>	3 parking spaces per bed
v)	<i>golf course</i>	2 parking spaces per hole plus 1 parking space for every 10 square



No.	Type of Use	Minimum Parking Requirement
		metres of a club house <i>restaurant</i> and lounge floor area
w)	<i>home occupation</i> and <i>home industry</i> excluding an office of a health service practitioner	1 <i>parking space</i> plus 1 additional <i>parking space</i> for each employee
x)	hospital	1 <i>parking space</i> for each bed at rated capacity
y)	<i>hotel</i>	1 <i>parking space</i> for each <i>hotel</i> room plus the applicable requirement contained <i>herein</i> for other <i>hotel</i> uses
z)	industrial establishment including <i>Cannabis Production and Processing [25-Z-2018]</i>	1 <i>parking space</i> for every 90 square metres of <i>usable floor area</i>
aa)	laundromat	1 <i>parking space</i> for every 4 washing and drying machines
bb)	liquor or beer store	12 parking spaces
cc)	<i>long-term care facility</i>	1 <i>parking space</i> for every 4 patient beds
dd)	medical or dental <i>clinic</i> , and office of a health service practitioner as a <i>home occupation</i>	1 <i>parking space</i> for every 15 square metres of <i>usable floor area</i> [66-Z-2018]
ee)	movie and other theatres	1 parking space for every 6 seats
ff)	office	1 <i>parking space</i> for every 30 square metres of <i>usable floor area</i>
gg)	<i>Personal service shop</i>	1 <i>parking space</i> for every 20 square metres of <i>usable floor area</i>
hh)	<i>place of worship</i>	1 <i>parking space</i> for every 8 seats or 5 metres of pew space or every 10 square metres of <i>usable floor area</i> where there are no seats or pews
ii)	<i>private club</i>	1 <i>parking space</i> for every 10 square metres of <i>usable floor area</i> , or where applicable in accordance with the requirements for a <i>bar or night club</i> , whichever is greater
jj)	<i>restaurant</i>	1 <i>parking space</i> for every 10 square metres of <i>usable floor area</i>
kk)	<i>restaurant, fast food</i> with drive-through	1 <i>parking space</i> for every 10 square metres of <i>usable floor area</i>
ll)	<i>restaurant, fast food</i> without drive through	1 <i>parking space</i> for every 8 square metres of <i>usable floor area</i>
mm)	<i>restaurant, outdoor patio</i>	1 <i>parking space</i> per every 4 seats
nn)	<i>restaurant, take-out</i>	2 parking spaces
oo)	<i>retail store or merchandise service shop</i>	1 <i>parking space</i> for every 30 square metres of <i>usable floor area</i>
pp)	school, elementary	1.5 <i>parking spaces</i> per classroom including laboratories, libraries and workshops
qq)	school, secondary	5 <i>parking spaces</i> per classroom including laboratories, libraries and workshops



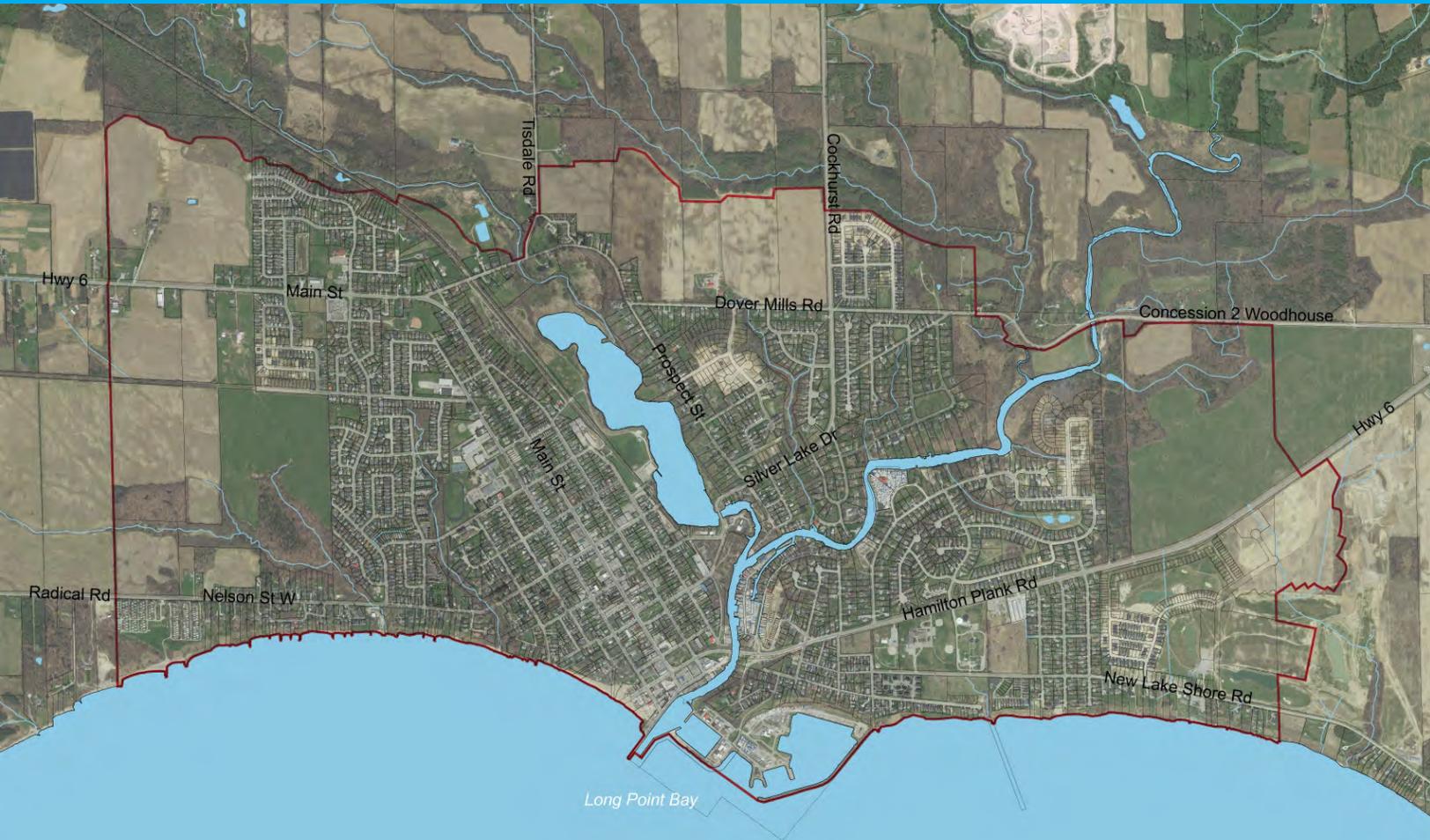
No.	Type of Use	Minimum Parking Requirement
rr)	school, trade	1 <i>parking space</i> for every student enrolled full-time for day courses
ss)	shopping plaza with three (3) or more units	1 <i>parking space</i> for every 20 square metres of <i>usable floor area</i>
tt)	warehouse or wholesale establishment	1 <i>parking space</i> for every 180 square metres of <i>usable floor area</i>
uu)	other non-residential uses	1 <i>parking space</i> for every 35 square metres of <i>usable floor area</i>

Where the calculation for the purposes of meeting this provision results in a partial parking space, a full parking space shall be provided for the partial space.





# Port Dover Secondary Plan



## Attachment C Planning Considerations and Applicable Policy and Zoning Provisions

### **Planning Act**

The *Planning Act* sets the context for planning within Ontario and establishes the basis for which Provincial Policy Statements, official plans and zoning by-laws can be established. Matters of provincial interest are outlined and guide planning policy. The Port Dover Secondary plan will be consistent with the matters of provincial interest, specifically those around logical development, providing for a variety of housing and ensuring adequate servicing is available.

Other notable sections include:

#### Section 22(2.1) Two-year period, no request for amendment

No person or public body shall request an amendment to a new official plan before the second anniversary of the first day any part of the plans comes into effect.

#### Section 22(2.1.1) Same, Secondary Plans

No person or public body shall request an amendment to a secondary plan before the second anniversary of the first day any part of the secondary plans into effect.

#### Section 22(2.1.2) Interpretation, secondary plan

For the purpose of subsection (2.1.1), a secondary plan is a part of an official plan, added by way of an amendment, that contains policies and land use designations that apply to multiple contiguous parcels of land, but not an entire municipality, and that provides more detailed land use policy direction in respect of those parcels than was provided before the amendment.

#### Section 22(2.2) Exception

If the council has declared by resolution that a request described in subsection (2.1), (2.1.1) or (2.1.3) is permitted, which resolution may be made in respect of a specific request, a class of requests or in respect of such requests generally, the relevant subsection does not apply. 2017, c. 23, Sched. 3, s. 8 (2).

### **Provincial Policy Statement, 2020 Considerations**

The Provincial Policy Statement, 2020 (PPS) provides policy direction on matters of provincial interest related to land use planning and development, which is intended to be complemented by local policies addressing local interests. The PPS promotes healthy, livable and safe communities through the efficient use of land throughout the Province of Ontario. The PPS directs municipalities to focus their growth within settlement areas where full municipal services are available.

A detailed evaluation of the Secondary Plan against the applicable policies of the Provincial Policy Statement will be included in a future comprehensive report.

### **Official Plan Considerations**

A detailed evaluation of the Secondary Plan against the applicable policies will be included in a future comprehensive report once staff have completed a detailed review of the proposed land use designations of the Official Plan and the draft Secondary Plan.

A detailed evaluation of the Secondary Plan against the applicable policies of the Official Plan will be included in a future comprehensive report.

### **Zoning By-Law Considerations**

Once the Secondary Plan is approved by Council, staff will begin to review the existing zoning to determine if zoning amendments are required to implement the policy direction and any new proposed uses.

A detailed evaluation of the Secondary Plan against the applicable provisions of the Zoning By-law will be included in a future comprehensive recommendation report.

### LOCAL

#### THE NORFOLK & TILLSONBURG NEWS

Reporter: Chris Abbott,  
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#### TODAY IN HISTORY

In 1950, it was announced that Canada's first nuclear power station, costing \$60 million, would be built near Kinrossville, on Lake Huron.  
In 1962, the first giant panda was born in captivity in Beijing.  
In 1964, Quebec borrowed \$100 million from British Columbia, the first time one province borrowed money from another.

## Theatre Tillsonburg celebrates 40th anniversary



LAURIEL WARKENT

Theatre Tillsonburg has been around for quite a while now and it is time to celebrate 40 years with 160 shows/events and 20-plus years with our very own building, the Otter Valley Playhouse!

We want to welcome back old friends who helped start and keep our group going, former Theatre Tweed, Sideline Campers and our faithful, brand new patrons and anyone who has wondered what we are all about.

The 20/40 Anniversary will be Sept. 18 from 2-5 p.m. and you are invited to come! The Playhouse is located at 144780 Potters Road.

Of course, with COVID we will follow protocols and be outside with some tents for shade (with all and new friends - a chance to show off some of our history through memorabilia. There will be 50K-70K musicals group from Bradford, called 50K (50K back wards) who will be music and groovin' as.

To celebrate there will also be a cake (chocolate please) and refreshment! Could you bring your lawn chairs, please?

Now if you are a real theatre lover, you can hang around to 6 p.m. and check out the inner workings of Theatre Tillsonburg

as we will be having our Annual General Meeting! You might like to join our theatre family as we always need more people to do our shows. Members, it is time to renew your membership!

Just to clarify some of those numbers above. We performed our first show in the fall of 1981, with Neil Simon's 'Come Blow Your Horn' on the Tillsonburg Community Centre stage after rehearsing and building the set at the Till-Cable TV building. We did two shows a year in spring and fall, managing to keep going while begging and borrowing everything we could for materials on the stage.

If not for the loans of Tillsonburg the shows and group would not have gone on. In 1987 we started the Theatre Tillsonburg Teen which was written, built and performed by teens with only minimal assistance from adults. They were very good.

Nine years performing in an auditorium never designed to do shows in, and having to move everything in the mornings, rebuild the set, lock up the paint, set the stage, lights and sound, do the shows, then move everything back to Till-Cable, started a drive for our own building.

In 1999 we purchased the 'Old Hangerman Home' on Potters Rd. And it was old! The house section was built in the late 1800s and the auditorium on the back only had a small band/strings stage. There was a lot of work and fundraising to be done.

Play It Again Sam was the first

show in the building before renovations were completed as we needed the money! There was a hole in the back corner of the stage with a straight ladder down to the green room as we didn't have stairs up to the stage!

The Grand Opening Show in November 1999 was 'Man of La Mancha', complete with a working drawbridge, which immediately after was moved to become the ramp up to the loft where our sets were stored.

Fundraising was paramount so we were involved in everything we could, from entertainment for community events to road shows. We needed money so added another show to the regular season and eventually started a Summer Camp for pre-teens.

In total we have done 160 shows or events. That includes the postponed but definitely returning 'Mamma Mia', for the hours of rehearsal, building and painting already done. **TIHK SHOW WILL GO ON!**

Please drop in for a stall and help us celebrate! To RSVP, please email info@theatretillsonburg.com or call 226-408-3026.

Just an extra note for a little-known event this Saturday the 11th, from 11 a.m. to 5 p.m. Oxford Creative Connect's Open Air Art Show will be on the Ammanville NHS museum lawn. Come see the amazing talent of our local arts community as they set up booths showcasing their work. Cost is by donation.

## Transparency versus privacy



LINDA DEJONGHE

Transparency seems to be a catch phrase these days.

I don't understand why we think the government, medical and legal professions, police, etc. should tell us everything about everything. We want complete transparency from everyone all the time and I am not sure we are entitled.

Some folks like to make sure others know everything about them all the time, too, as evidenced by what is posted online.

To some folks privacy is vital to their lives and they share nothing. Some open up to only a select few, while others don't even do that. Privacy can be deeply important and maybe for socially anxious or because of likability, religious, nervousness, indifference or isolation. Often they can't clearly explain their own need for isolation but they ardently feel it essential to keep the world out of their lives. These folks are often the topic of speculation and gossip because of their isolation.

I know people who give the impression of being open and honest but it's a pretence. They only give what they want you to believe but hide their true selves. They are always worried about what people think of them or, worse yet, that they will be found out as frauds.

Some really don't care who knows what. They have an understanding people will think what they want regardless of their true need for secrecy. What you see is what you get. These folks are often the topic of speculation and gossip but they just don't care.

It must be hard to maintain the secret of who you are. It has to be an on-going daily battle to make sure you don't slip up. To me it's much easier to just be you. Some will appreciate your uniqueness and others won't but in the end you can be content and happy.

After writing this column for 30 some years I can say I have been as transparent as I can. There is not much I haven't talked about over those years, within reason. I have only once had an editor refuse to run a column due to concerns my words could be misinterpreted. From the welcome emails I am fully aware what I say conjures up a variety of emotions and reactions in different people so I try hard not to offend too many folks at a time.

Have not shared many new words lately. I think all the secrets took my attention away from many of the news stations I often watched. I have recently learned a few new words and also want to say thank you to those who send me words they find interesting. I will continue my practice of sharing.

Pedagogy, method and practice of teaching especially as an academic subject or theoretical concept, art, science or profession of teaching.

Blowtelling, talk at length in an inflated or empty way, speak or write verbosely and windily.  
Ablutophobia, fear of cleaning.  
Vestibulitis, fear of wearing clothing.

Did you know a flock of flamingos is called a flamboyance and gathering of hummingbirds is called a bouquet? Seems appropriate to me.

linda@northvoij@gmail.com

## Learn about BBBS in September

September is Big Brothers Big Sisters month.

During BBBS month, the agency would like to educate the community on the importance and positive impact of mentoring.

BBBS is encouraging those interested in learning more about being a mentor to attend one of their virtual BBBS 101 Information Sessions.

For those who are ready to make the commitment, complete their online Volunteer Application form, both can be found on their website at [www.bbbsncf.ca](http://www.bbbsncf.ca) under 'Volunteer'.

BIDDING OPPORTUNITIES			
DOCUMENT #	DOCUMENT NAME	CLOSING DATE	To register and view bid document
CS-PD-21-04	2022 Equipment Rental Rates with Operator	Tues., October 5, 2021 Before 2:00:00 pm	<a href="https://norfolk.bidsand-tenders.ca/Module- tenders/en">https://norfolk.bidsand-tenders.ca/Module-tenders/en</a>

For all things Norfolk County, please visit [norfolkcounty.ca](http://norfolkcounty.ca) or follow us on Facebook or Twitter @NorfolkCountyCA

## NOTICE - PORT DOVER SECONDARY PLAN

In accordance with section 17(15) of the Planning Act, Norfolk County invites you to participate in the Port Dover Secondary Plan statutory Public Meeting, where you can learn more about the Draft Secondary Plan and share your feedback, questions and/or comments.

**DATE:** Wednesday October 6th, 2021 **TIME:** 9:00 pm **LOCATION:** This will be a Virtual Public Meeting

Norfolk County is undertaking the Secondary Plan to provide a detailed planning framework for Port Dover's waterfront, downtown and future residential development areas within the urban boundary. The Secondary Plan will be used to guide stewardship of the lakes, rivers and natural heritage areas and create a vision for downtown. Urban Design Guidelines are accompanying the Secondary Plan to provide direction regarding the continued development of Port Dover.

**WHERE TO GET MORE INFORMATION?**  
Information about the Port Dover Secondary Plan and the draft documents can be found on the project website on or before Sept. 15, 2021 <https://www.norfolkcounty.ca/government/planning/port-dover-secondary-plan/>

**PROVIDE YOUR FEEDBACK**  
To provide your feedback on the Draft Port Dover Secondary Plan, you may attend a Public Meeting on October 6th, 2021 or provide written comments directly to staff. The statutory public meeting will be held virtually due to COVID-19 protocols. You may register to speak through the Clerk's Office using the following link on the Norfolk County Webpage: <https://www.norfolkcounty.ca/government/public-hearing/>  
The deadline to register is 12:00 pm on October 6th, 2021.

If you are unable to attend the Public Meeting, we encourage you to provide written feedback by email to Jennifer Casarino at [Jennifer.Casarino@norfolkcounty.ca](mailto:Jennifer.Casarino@norfolkcounty.ca).

[norfolkcounty.ca/public-notice](http://norfolkcounty.ca/public-notice)

## memo

Project Name: Port Dover Secondary Plan  
Date: September 28, 2020  
Subject: Working Group Meeting #2

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The following are notes taken during the second meeting with the Working Group held on September 10, 2020 at 3:00 pm using a virtual meeting platform. The meeting began with a presentation of the team's review of background information relating to land use planning, transportation, servicing and natural heritage.

In attendance:

- Mayor Chopp
- Councillor Martin
- Helene Larochelle
- Brian Ogden
- Bruce Shoff
- Debbie Lamb Green
- Lynda Kilpatrick
- Brandon Sloan, General Manager, Planning and Development Division, Norfolk County
- Jason Burgess, CAO Norfolk County
- Donna Hinde, The Planning Partnership
- Ron Palmer, The Planning Partnership

### **New Residential Areas**

- The Built Boundary within Port Dover identifies the existing built up/ already constructed areas
- The new residential areas are outside of the current built up area of Port Dover
- Clarification is required on how much infrastructure is in place to service the greenfield sites. Need to understand the technical constraints to determining the growth
- The team is determining the capacity to accommodate growth; phasing of growth will be required

### **Downtown/Lynn River**

- Discussion regarding expanding the boundaries of downtown
- Concern about residential uses and mortgages for properties in commercial areas
- Expansion of the boundaries of downtown, need to address competing objectives – mixed use versus protection of established neighbourhoods
- Needs a thorough discussion = will it happen, what are the implementing tools to see it happen
- The team clarified that if downtown boundaries were expanded, land use would be identified as mixed use, not solely commercial
- Clarification on our role in examining privately owned vacant sites downtown – to demonstrate an appropriate height, mass, character of infill development, clarify how loading, parking and servicing may be accommodated.
- Need to ensure height limits include mechanical etc. that often fall outside of the limits on building height



- The team discussed the pros and cons of general vs. directive policies. For building height, the preference was for more specific direction which could then be translated into the zoning (example maximum 30m building height including rooftop mechanical)
- Discussion on the Furniture Barn – interest in a restaurant, no parking, Can it be included in downtown?
- Need to understand the objective for each area where commercial uses are being considered, what are the options? The team can give more flexibility re: uses, parking, etc., without necessarily extending the boundaries of downtown
- Currently in discussions with the Conservation Authority to understand how to facilitate development along the lake and riverfront given current regulations
- Special Policy Aea of the Planning Act is available to enable consideration of development
- Lynn River – possible areas for mixed uses and/or expansion of downtown
- Always concerned about spreading commercial out too far
- Concerned about big box retail on Highway 6
- There's an opportunity along river for small scale retail, coffee shops, etc
- Retail is difficult for developers – don't want to put commercial because they say they don't want to detract from downtown – will need additional retail as the town grows, having some commercial along the river is a good thing for the community
- Downtown is not at capacity – lots of empty storefronts
- Need a full array of retail so people will stay in Port Dover and not necessarily drive to Simcoe or other retail destinations
- Amount of sq. footage of retail in some of the developments is not significant enough to have an impact on downtown
- Need to understand what new development delivers besides houses. The team will suggest an amount of commercial per person to factor into new growth
- Why do boundaries of the downtown fall on one side of a road and not both sides – commercial should be both sides

## **Waterfront**

- Parking on waterfront won't be there forever – owners want development
- What is the County's role in providing parking?
- There is an opportunity to incorporate public parking in private development – it's often a negotiation
- Parking has to be dealt with in a successful downtown and waterfront
- Expand the boundary of the lake/river front areas to be studies on the west side of Silver Lake
- Clarification that all lands in the Secondary Plan area will be addressed in the document

## **Determining capacity for growth**

- Additional information is under study – understand the issue, the options, the capacity #s, how does it relate to growth and development, - what are the concepts and possibilities
- We need to understand what level of improvement is required to sewer and water – is it financially feasible, what needs to be done
- Potential for growth has to be linked to infrastructure, best phasing strategy is based on capacity of sewer, water infrastructure
- Likely won't be able to understand how much development every year but could look at projections over the planning horizon, what will the market absorb? What is appropriate for Port Dover? "



### **Affordable housing**

- There were a lot of comments re: affordable housing and development
- No ability for municipality to control price of housing, occupancy, or tenure
- County is a land owner, can use their land to deliver affordable housing
- Goal is more affordable options delivered by private sector
- Need clarity and directive policies regarding affordable housing

### **Next Steps**

- Working Group members could consider if there are particular issues individuals wish to champion
- Working Group members are interested in reviewing the content of presentation section by section. Councillor Martin will follow up with additional discussions with the Working Group and will forward consolidated comments to the consulting team.
- Clarification that the content of the presentation is a summary of the team's review of background information
- Discussion of the role of the Working Group and what is expected of them
- Discussion on the participation of Working Group members in round 2 of public consultation.
- The team is confirming the scope of options to be considered in round 2 of public consultation.
- The next Working Group meeting will follow the public consultation to discuss what we heard and the key directions for the Secondary Plan.

## memo

Project Name: Port Dover Secondary Plan  
Date: April 20, 2021  
Subject: Working Group Meeting #3

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The following are notes taken during the third meeting with the Working Group held on April 19, 2021 at 2:30 pm using a virtual meeting platform. The purpose of the meeting was to review the urban design considerations prepared as a component of the Secondary Plan.

### In attendance:

- Mayor Chopp
  - Helene Larochelle
  - Brian Ogden
  - Bruce Shoff
  - Debbie Lamb Green
  - Lynda Kilpatrick
  - Jason Burgess, CAO
  - Brandon Sloan, General Manager, Planning and Development Division
  - Tricia Givens, Director of Planning
  - Jennifer Catarino, Senior Planner
  - Donna Hinde, The Planning Partnership
  - Ron Palmer, The Planning Partnership
  - Mike Hudson, The Planning Partnership
- 
- Ron Palmer began with an explanation of the use of design guidelines in planning. Not every guideline will find its way into the Secondary Plan. Others will be addressed through Zoning or Site Plan Control. He will be finding the right balance of the best tool to use regarding the guidelines.
  - Brendon asked for clarification of the Secondary Plan and whether it would align with the structure set out the Urban Design Considerations document with respect to the Downtown and Waterfront Core, Existing Waterfront Neighbourhoods, Future Neighbourhoods etc.
  - The Secondary Plan will include an urban structure plan for Port Dover setting out neighbourhoods, focus areas, green space, locations for higher density housing, etc.
  - The Secondary Plan will not duplicate existing policies in the County Official Plan
  - There is a need to clarify the difference between waterfront versus water's edge. Definitions will be useful
  - Chapter 2, Design Framework re: section 2.4, consideration should also be given to accommodating additional dwelling units in the marine commercial areas



- Chapter 3, Downtown and Core Waterfront - Consider whether the east side of the harbour needs special consideration, apart from the “Core Waterfront” due to the functioning harbour uses
- Consider extending the boundary of Downtown to include the other side of St. George Street and St. Andrew Street, rather than having the boundary down the middle of the road
- In regards to the proposed 0 m setback, need for some flexibility in the location of the front wall of commercial buildings downtown to accommodate a front patio area. Mike explained that it would be important to have this area designed as an extension of the sidewalk
- 211 Main Street was mentioned as an example of a preferred condition
- Section 3.2, verify the reference to the 6-storey height coincides to the Zoning By-law in the Height and Massing section
- Discussion of the guidelines with respect to heritage and façade restoration as being too restrictive and costly for owners. In response to the guideline that states, “do not use material that imitate other materials”, it was mentioned that there are some very good materials now that may be suitable that this guideline would not allow
- It was noted that the heritage preservation requirements in general may be too restrictive, especially the requirement for a preservation professional
- Section 3.2 Guideline 41 restricting back lit or neon signs – discussion that there are some very good neon signs that could be acceptable. Signs could be used as art installations
- Need to make reference to floodplain and floodproofing in the Waterfront Core area. Consultation with the Long Point Region Conservation Authority
- Remove the reference to “avoiding gambrel roofs” in section 3.7 as there are some in the area
- Section 3.7, remove guideline 12 as underground parking is not realistic in the Waterfront Core. Consider using these guidelines for Downtown parking as well
- Concern that guideline 15 may be too restrictive to developers. Clarification was given that the aim is to avoid streets lined with surface parking
- Chapter 5, guideline 11, concern with requiring 3 storey buildings around parks. Comment made that most of demand is for bungalows, others mentioned that this would accommodate affordable housing that may be 3 storeys There was discussion regarding the removal of the word “minimum” of 3 storeys.
- Chapter 6, Commercial and Institutional uses –there is little interest in allowing big box retail in Port Dover, primarily because of the impact to Main Street. Comment made that seems like the guidelines offer less control for these uses than they do for Main Street retail. Mentioned that big box is the most difficult topic in development planning. Can control the use by establishing a maximum floor space in Zoning. Little ability to control the type of franchises that establish locations
- The Secondary Plan will include more specific policies regarding small businesses



- Chapter 7 – Gateways – comment to remove the Gateway Tisdale because of site conditions. Clarified that this was included because of the Future Residential Neighbourhood to the east and the opportunity to address the Gateway in the development concept. Suggestion to include an additional Gateway on Radial Road. Mentioned that Gateways can also function to calm traffic. Design criteria for road treatments may be helpful
- It was mentioned that it might be necessary to consider tiered gateways or to include a hierarchy of Gateways
- Chapter 8, Natural Heritage System and Parks – comments about providing direction on the type of recreation facilities in parks, support for the park hierarchy, in particular the Vista Parks
- Chapter 9, Active Transportation – suggestion to include a missing link on Dover Mills Road to extend the proposed cycling route, adding a multipurpose walkway along Highway 6 and look for a north south trail connection on the east side of the harbour
- Consider a river trail that connects to Lake Erie
- Chapter 10, Streets – need guidelines to address the opportunity to make Harbour Street one way, could also accommodate on street parking. Also need to address vehicle access to the Pier
- Chapter 11, Green Infrastructure and Building – add guidelines for electric vehicle charging stations. Support for guideline 2 re: solar thermal and photo voltaic equipment

### **Next Steps**

- Working Group members are to submit their comments to Jennifer. Jennifer will consolidate all comments and forward to The Planning Partnership



memo

Project Name: Port Dover Secondary Plan  
Date: July 21, 2021  
Subject: Working Group Meeting #4

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The following are notes taken during the fourth meeting with the Working Group held on July 21, 2021 at 1:00 pm using a virtual meeting platform. The purpose of the meeting was to review the draft Secondary Plan.

In attendance:

- Helene Larochelle
- Brian Ogden
- Debbie Lamb Green
- Tricia Givens, Director of Planning
- Jennifer Catarino, Senior Planner
- Donna Hinde, The Planning Partnership
- Ron Palmer, The Planning Partnership
- Mike Hudson, The Planning Partnership

Regrets:

- Mayor Chopp
  - Councillor Martin
  - Bruce Schoff
  - Linda Kirkpatrick
- Ron of The Planning Partnership presented an overview of the Draft Port Dover Secondary Plan.
  - The following discussion occurred:

- Agreement that Cockshutt Road should be identified as a gateway
- What is a block plan? How do they help phase and manage future development? Is each numbered greenfield area required to develop a Block Plan? How does the Secondary Plan prioritize between the future neighbourhoods and phase development or will development continue to be approved as each property owner submits an application?

*A Block Plan enables coordination and connection among landowners - roads, parks, servicing*

*Property Owners work together so there's not multiple plans of subdivision that don't work together also enables cost sharing*

*Sometimes the municipality does the Block Plan and charges the landowners*

*Minimum density - intended to ensure the greenfield areas aren't just single detached dwellings - need for other forms of housing within each community*

- Should there be a 4 storey max for all of Port Dover?

*A 4 storey maximum could be considered, but what's the rationale? – the first development application requesting additional height will open the door for taller heights and the maximum will height won't matter. A blanket height limit has little value*



*Compatibility of height is the test*

- Document needs more clarity with respect to designations and specific height

Why is Bucks located in Downtown and not the Waterfront? There is concern that since Bucks Orchard is located within the Downtown Designation, redevelopment could result in 6 storey buildings that would block the view to the lake.

*Carrying forward the existing policy regime*

*Site specific policy deals with that site - important that not taking away that approval*

- Urban Waterfront designation should reflect all of the river, creek, Silver Lake, and Lake Erie

*Urban Waterfront is specific policy and geography*

*Intent that development is appropriate subject to criteria including dealing with hazard land  
The other waterfronts, eg. Silver Lake, Black Creek - not expected to develop*

- Marinas are at risk (on Black Creek) - big blocks - could be redeveloped. Lions Club around Silver Lake could sell property. All need a degree of protection if development opportunities come up
- Need to clarify mapping so Hazard land designation shows more clearly-
- Active transportation - want a signed route E-W connector - relates to where bridges are - will become more appealing with greenfield development, suggest adding a blue dash line on the Schedule
- Water's edge trail should follow the river along the Misner and Gamble properties
- Guideline illustrations need a footnote that mentions floodplain requirements
- Silver Lake - Lions might not be stewards forever- future owners might want development

*Lions Club property land use designation is proposed to change from Urban Residential to Park and Open Space*

- Signage: art installations can fall under signage, Business owners want flexibility. Secondary Plan has decisive language on not allowing neon and backlit - now neon is better and less cost prohibitive. Need to provide for creativity in signage that could include neon
- Consider revising the boundaries of Downtown to include both sides of the street having the transition of the zones along the rear yards of a block – potentially to St. George Street.
- What about large format retail in Dover? The opinion of many is that they belong in Simcoe - should it be here?

*Larger format stores could go into retail/service commercial Mixed Use Corridor & Downtown*

*In Mixed Use : stores limited to 3,000m<sup>2</sup> (32,000 square feet)*

*Retail is changing - food stores getting smaller*



*Rule of thumb: 2 sq m per person retail*

*If Port Dover is 3X bigger than today, expect 3X more retail*

*Retail experts suggest large format retail will become warehouses for online shopping*

*Experiential retail will survive: Downtown and independent retail*

- Keep heights lower at Waterfront
- Like minimum density at greenfield, keep height away from Downtown, Address amenities in greenfield areas, e.g grocery
- Policies are only as good as their interpretation and decisions that implement them
- Ensure accessibility to the water for the public

**Appendix F – Comments received up until September 10, 2021.**

Date Received	From	Comment	Staff Response
November 3, 2020	Marion Gadsby, via email	<p>Hi</p> <p>During the Secondary Plan meeting last Thursday, there was a discussion about a possible trail connecting the end of the Lynn Valley Trail and the lighthouse. I am not sure if you have seen a copy of the proposed trail. Below is a description of the proposed trail and some photos at key locations.</p> <p>Donna Hinde, Amy Martin and Chris Van Paassen have copies. Is there anyone else who should be sent a copy?</p> <p>If you have any questions, please contact me.</p> <p>Marion Gadsby M.A.Sc., B.Ed.</p> <p>See Appendix F1 for attachments to original email.</p>	<p>Schedule D: Active Transportation illustrates a “proposed Water’s Edge Trail”. Staff will continue to review active transportation connections.</p>
November 3, 2020	Marion Gadsby, via email	<p>Hi</p> <p>While I was working on the proposed Lighthouse Trail, I was also looking for places where additional parking spaces could be created.</p> <p>Attached is a description of where some parking could be created.</p> <p>The Port Dover Board of Trade, Amy Martin and Chris Van Paassen have copies of this document. Is there anyone else who should have a copy of this document?</p> <p>If you have any questions, please contact me.</p> <p>Marion Gadsby</p> <p>See Appendix F2 for attachment to original email.</p>	<p>Further investigation is required.</p>

Appendix F – Comments received up until September 10, 2021.

<p>November 4, 2020</p>	<p>Gabriel Gasbarrini, via email</p>	<p>Ms. Givens,</p> <p>Further to my correspondence with the secondary planning consultants, can you please clarify the county policies regarding planning proposals during the ICB</p> <p>I learned at the secondary planning meeting that the lands west of me have already applied for a development concept, This was news to me as I was under the impression that applications were not being accepted.</p> <p>As you may recall that prior to the ICB, I made representations to Council and I was trying to reach an agreement with the County on conducting comprehensive planning studies. I am on record as asking to be a party to any such planning studies, after the County stated that it wanted to control the studies and that it was necessary to do a comprehensive Dover West Study as stated by your engineers.</p> <p>I believe that any plans that are abutting my lands would have significant impact on my development opportunities and would have thought that there would be some neighborhood or comprehensive planning discussions that need to take place before passing on such a concept to the architects of your secondary plan review.</p> <p>I do not understand how you can accept a road pattern or pond location design without infrastructure studies regarding the engineering, hydrology or the traffic studies or the community connectivity via bicycle paths etc. or how such baseless preliminary concepts can be included in your secondary plan overviews.</p> <p>gabriel gasbarrini president</p>	<p>The draft Secondary Plan proposes a new Block Plan process which will address many of the concerns raised by Mr. Gasbarrini as it introduces a comprehensive approach to planning future neighbourhoods when there are various owners.</p>
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Appendix F – Comments received up until September 10, 2021.

<p>November 21, 2020</p>	<p>Marion Gadsby, via email</p>	<p>Hi</p> <p>When you are writing the report on the Secondary Plan for Port Dover, you should be aware that the water level in Lake Erie can change very fast during a storm. In Port Dover, the difference in water level is often measured in feet not inches. This is especially important in the beach area.</p> <p>On Sunday November 15, 2020, a storm drove the water onshore at Port Dover. There was extensive flooding along Walker Street. The water at the intersection of Walker Street and St George Street was about 50 cm or 20 inches deep. Pictures of the flooding can be seen in the Port Dover Maple Leaf and on Facebook.</p> <p>The following pictures show how fast the water level in Lake Erie can change during a storm. A storm similar to the November 15, 2020 storm occurred on October 7, 2009. In September, 2009 we had a new higher break wall installed in front of the original break wall on our property. Each of the large concrete blocks shown has a height of 22 inches. One block was left out to give access for swimming. The first picture shows the normal water level in September. The water level on October 7 was about the same.</p> <p>When we woke up on October 7, 2009, the storm was driving the water onshore. At about 9:00 in the morning, the waves were over the top of both our break wall and the pier. At noon, the centre of the low (the equivalent to the eye of a hurricane) passed over Port Dover and the wind became completely calm.</p> <p>In the early afternoon, the wind started blowing the water offshore with the same force as it had been blowing the water onshore in the morning. In the afternoon of October 7, about seven hours after I took the first pictures, the water level had dropped so much that I was able to walk on dry sand along the beach.</p> <p>In the evening, after the wind was back to normal, the water level returned to its normal level similar to the level shown in the first picture.</p> <p>If you have any questions or would like more pictures or ones with better resolution, please contact me.</p> <p>Marion</p>	<p>Noted.</p>
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Appendix F – Comments received up until September 10, 2021.



Appendix F – Comments received up until September 10, 2021.



Appendix F – Comments received up until September 10, 2021.



Appendix F – Comments received up until September 10, 2021.

<p>April 22, 2021</p>	<p>Marion Gadsby, via email</p>	<p>Hi</p> <p>My name is Marion Gadsby and [REDACTED] in Port Dover. I am a member of the executive of the Port Dover Waterfront Preservation Association. As a member of the PDWPA, I listened to most of the LPAT hearing on the Misner Landing development.</p> <p>At the LPAT hearing, a colour coded map from the Planning Partnership Secondary Plan showing ownership of the land was screen shared. On the map, several lots were coloured yellow indicating that the ownership was not clear. One of the Lots in question is Lot 12, Plan 230. This lot is below me between my property and the water. Another Lot in question is Lot 17, Plan 230. This lot is also below Elm Park between some of my neighbours and the lake.</p> <p>If you are interested in the history and origins of these two lots and the likely owner of these lots, please contact me.</p> <p>Keep Safe Marion Gadsby M.A.Sc., B.Ed.</p> <p>P.S. I look forward to meeting with you, Jim Dover and Robert McKinnon about the Silver Lake Revitalization Project.</p>	<p>The map referenced is no longer included in the draft Port Dover Secondary Plan documents.</p> <p>Staff reached out to Ms. Gadsby for the historical context of the area.</p>
<p>June 10, 2021</p>	<p>Jim Dover, Port Dover Waterfront Preservation Association</p>	<p>Hello Brandon.</p> <p>Please find attached a letter signed by Paul Creighton (President of the Port Dover Waterfront Preservation Association) requesting that the former Public Works Yard on Nelson St. in Port Dover be redesignated from Urban Residential to Open Space.</p> <p>Both Tricia and Jennifer are familiar with the background to this request. I have copied Teresa to expedite placement of this correspondence in Council's Information Package.</p> <p>Should you have any questions, please get in touch. We look forward to your response.</p> <p>Jim Dover</p>	<p>Senior staff are currently reviewing the request to re-designate the property at 212 Nelson Street. A recommendation will be provided in a comprehensive report to Council when the Port Dover Secondary Plan is presented for a decision.</p>

Appendix F – Comments received up until September 10, 2021.

		<p>Project Manager Silver Lake Revitalization Project – Phase 1</p> <p>See Appendix F.3.</p>	
<p>June 22, 2021</p>	<p>Timothy Salvatore, via email</p>	<p>Hey Everyone,</p> <p>I hope you all are doing well, now that Norfolk County has entered stage 1 of the reopening plan I figured I would reach out again and see if there is anything more I can do in order to possibly move forward with ideas in regards to pedestrianizing downtown Dover.</p> <p>Since we last spoke I have had a few conversations with local businesses in the Harbour Street area to gauge feedback as well as Nick Childs from the board of trade. So far everyone I have talked to seems to be showing support for more pedestrianization and getting a bit clearer vision on what the area could possibly look like along with the economic/environmental impacts. Of course dissenting opinions, especially in regards to parking, will be present but I think we can gather enough support to at least start looking at the possibility of pedestrianization of the downtown and creating a vision.</p> <p>Another aspect that I have been researching lately is funding for such a project and working with a friend of mine, Sydney Clarysse, who is currently interning with the city of Timmins while finishing her masters of Environmental Science. With her academic connections and current time with the city of Timmins we have been able to look at quite a few grants that could possibly help fund such pilot and infrastructure projects as well as some of the studies needed in order to look into present plans to council. Knowing Norfolk's current budget outlook I understand that tackling projects with very little actual costs to the county is paramount but thankfully there seems to be quite a few grants available for exactly these types of ideas.</p> <p>Are there any committees, boards or other organizations you could point me to that would help progress these ideas? I am definitely motivated and willing to put in the work with local businesses and community members in order to keep improving our little downtown.</p>	<p>Planning staff are investigating opportunities to incorporate future pedestrianization within Port Dover. There are draft policies in the plan that reference this concept, however, further investigation is required.</p>

Appendix F – Comments received up until September 10, 2021.

		<p>Talk soon,</p> <p>Tim Salvatore</p>	
June 22, 2021	Jim Dover, Waterfront Preservation Association	<p>Hello Brandon.</p> <p>I've attached a News Release regarding the Silver Lake Revitalization Project that is being distributed to the news media today. Please feel free to forward this News Release to any Norfolk County Staff or Council members who you think may be interested.</p> <p>I have copied Teresa given the request that this News Release be included in Council's Information Package.</p> <p>If you have any questions, please get in touch.</p> <p>Jim Dover Project Manager Silver Lake Revitalization Project – Phase 1</p> <p>See Appendix F.4.</p>	Noted.
July 12, 2021	Jim Dawson, via email	<p>Hi Jennifer! I just had a comment to pass along. Some of the restrictions to homeowners I think are too restrictive- eg requiring an infill property to have street front facing facade or having all the same windows. While in an ideal world, these might be a nice to have, I think this is too restrictive and would cause people wanting to build, for example an affordable unit as infill to say forget it - it is too much trouble. This is just an example of the over-reach in the requirements- I would suggest that it should be reviewed by people who plan in-fill development to get their input.</p>	Further investigation is required. Planning staff is preparing additional engagement with the development industry and this topic will be explored.
July 12, 2021	Eldon Darbyson, via email	<p>Good afternoon</p> <p>I'm enjoying the presentation for this afternoon. Thank you for allowing me to speak.</p> <p>My question/comment is as follows:</p>	The Planning Partnership considered this comment and expanded the Port Dover Downtown designation along St. George Street between Sloan Street and Chapman Street West.

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		<p>Has there been any thought to recognizing that the lands on the opposite side of the street of the Central Business District have similar advantages of contributing to the Port Dover CBD area and should not strictly be residential? Consideration for unique tourism proposals such as rental accommodations and limited retail may be beneficial to helping Port Dover Thrive. The lands on George Street, south of Chapman are a good example, similar to the north east blocks on Main street between Chapman and McNabb. St. Andrew street has the same divide between CBD and Residential.</p> <p>Thanks to Ron for his response. I'm glad that additional consideration is going to be given to this issue.</p> <p>There is a public parking lot on George Street which adds to the potential to consider additional complimentary uses adjacent to the current CBD designation.</p> <p>Sincerely,</p> <p><i>Eldon Darbyson BES, MCIP, RPP, Director of Planning</i>  <b>G. DOUGLAS VALLEE LIMITED</b>          Consulting Engineers, Architects and Planner          2 Talbot Street North Simcoe Ontario N3Y 3W4          Phone: 519.426.6270 Fax: 519.426.6277  <a href="http://www.qdvallee.ca">www.qdvallee.ca</a></p> <p>During the COVID-19 pandemic, I will be working remotely.          You can reach me by email or on my cell at (905) 321-2029</p> 	
<p>July 15, 2021</p>	<p>Jim Dover,          Port Dover Waterfront Preservation Association,          via email</p>	<p>Hello Donna!</p> <p>For the record, I just wanted to correct something that I said at today's Engagement Session regarding the draft Port Dover Secondary Plan. I stated that before Covid the Port Dover Waterfront Preservation Association had approximately 90 paid members but that our membership was currently standing at approximately 40. While my pre</p>	<p>Noted.</p>

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		<p>Covid number was accurate, the number of current members was under stated. Our incorporated not for profit organization currently has 58 paid members. Our membership has been increasing since the launch of our website – <a href="http://www.portdoverwaterfront.ca">www.portdoverwaterfront.ca</a> .</p> <p>Thank you for hosting the Engagement Sessions. We found them very informative. We have a much better understanding as to how the documents have been put together, why and how they are meant to be read. The general sense I have is that we all have the same goal in mind with regards to the public’s physical and visual access to the waterfront – that the County shall actively preserve the public’s physical and visual access to the waterfront and seek enhancements where there are opportunities. We’re all trying to find the best way to express this clearly in the documents. With this in mind, we plan to submit one aggregated response in writing on behalf of the Association by July 20<sup>th</sup>.</p> <p>Thanks again for hosting the meetings and for allowing me so much time to ask questions on behalf of the PDWPA and for responding to our suggestions. We are much better informed thanks to the Engagement Sessions. We hope that our written submission will be helpful to you, Ron, the Community Working Group and County Staff.</p> <p>Jim Dover Vice President Port Dover Waterfront Preservation Association</p>	
<p>July 15, 2021</p>	<p>Jim Dover, Waterfront Preservation Association, via email</p>	<p>Hello Brandon and Paula.</p> <p>I was surprised to see the attached article regarding the closure of the pathway on Regent St. will be appearing in tomorrow’s Simcoe Reformer – a day after our Association participated in the Engagement Sessions for the Port Dover Secondary Plan and were reassured that Council and Staff support the preservation of the public’s physical access to the Port Dover waterfront.</p> <p>It appears from the article that the attached Report’s recommendations were approved though I’m unable to confirm through the website at this time. I’m acting on the understanding that this Report’s recommendations will be put forward to Council for ratification this coming Tuesday. At this point, our Association has not taken a position</p>	<p>Staff responded to Mr. Dover and provided feedback regarding the drafting of the report.</p>

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		<p>but we are quickly trying to gather more information so that we can decide what action, if any, we wish to take.</p> <p>Brandon, I noted in the Report that it appears Planning did not provide comment on the Report. I'm wondering whether this is the case and, if so, what position Planning has on this matter.</p> <p>Paula, I was surprised that the AODA was used as a justification for the recommendation given the position that the County took at the Misner Landing OLT Appeal. Has something changed? Do we know the cost of replacing the staircase and of installing a sign directing persons with disabilities to other points of access?</p> <p>There's not much time for our Association to act. I'm hoping you will be able to respond to my questions quickly and possibly provide any more information you think we should be aware of.</p> <p>Thanks!</p> <p>Jim Dover Vice President Port Dover Waterfront Preservation Association</p> <p>See Appendix F.5 for attachments to original email.</p>	
<p>July 19, 2021</p>	<p>Paul Lipsit, via email</p> <p>Attached to the email was an edited version of the Draft Secondary Plan with comments.</p>	<p>To All:</p> <p>I am a resident of Port Dover and an active member of the boating community. I've been sailing the waters of the Great Lakes for many years and have enjoyed the hospitality of several ports on Lake Erie. I have a passionate interest in the Port Dover waterfront, and as a concerned citizen, I ask that you include the following stated revisions into the public record.</p> <p>I've attached a copy of the Draft Secondary Plan that I've edited with comments and suggested revisions. Please see highlighted areas noted on pages 3 to 7 inclusive, pages 9,18, 43, 45, 46 &amp; 48.</p>	<p>A copy of the comments and edited document were forwarded to The Planning Partnership. Further consideration will be given to the inclusion of these comments as they provide an unique perspective into Port Dover.</p>

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	<p>Comments have been added below the email.</p>	<p>I trust you will find my commentary self- explanatory, as taken from the perspective of a mariner, on the water, not solely from the perspective of a pedestrian on land.</p> <p>Regards,</p> <p>Paul Lipsit, Port Dover</p> <hr/> <p>Section 2.1 Vision Statement (pg. 3)</p> <p>"...a network of publicly accessible green spaces and waterfront, protecting natural areas..."</p> <p>Section 2.2 Guiding Principles (pg. 3)</p> <p>Principle 2: highlighted "will be compatible in character"</p> <p>Principle 6: Principle 6 should also include connective networks to maritime facilities as well, to provide mariners improved access to the transportation network.</p> <p>Section 3.1 Objectives for a Successful Community (pg. 5)</p> <ul style="list-style-type: none"> <li>a) i) third arrow "... parks, open spaces and waterfront areas, ..."</li> <li>b) i) correct the grammar here.</li> <li>c) "...public parks, open spaces and waterfront areas that celebrate the community, ..."</li> <li>ii) "...into the community, by land or by sea."</li> <li>"... structural elements, marine fixtures and/or..."</li> <li>d) Active transportation also includes water activities such as swimming, kayaking, rowing, canoeing, boarding, etc.</li> <li>ii) "To design the community around pedestrian and maritime activity..."</li> <li>Active transportation should include land AND water based activity!</li> </ul> <p>Section 3.2 Policies for a Successful Community (pg. 9)</p>	
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		<p>c) i) "Improve public access to the Lake Erie shoreline and protect public ownership of the Port Dover Harbour Marina;</p> <p>Section 4.2 Community Structure (pg. 17) c) iii) sentence is incomplete.</p> <p>Section 6.2 Port Dover Urban Waterfront Designation 6.2.1 Intent a) "...supports tourism, the recreational boating community, the commercial fishery and..." a) iv) third arrow "The publicly owned and operated facilities and services of the Port Dover Harbour Marina."</p> <p>Section 6.2.3 Development Policies (pg. 45) e) Does the 6.5.2.2 reference exist? The Marina Board of Management no longer exists. Replace with: "...facility sharing with the Port Dover Harbour Marina, ..." "open space, boating and parking facilities within..."</p> <p>g) "foster pedestrian and maritime activity"</p> <p>s) quote from draft document "improvements for pedestrians, cyclists, the elderly and physically-challenged, and those using taxis, transportation for the disabled and transit services."  Comment - this clause to include "mariners" or "boaters".</p>	
<p>July 19, 2021</p>	<p>Paul Creighton, Port Dover Waterfront Preservation</p>	<p><i>FOR THE PUBLIC RECORD</i> <i>Please find attached a summary of the Port Dover Waterfront Preservation Association's response to the Draft Port Dover Secondary Plan.</i> <i>For the convenience of the Study Group, we have listed our comments in the order that the relevant sections appear in the Draft Port Dover Secondary Plan. Please note that they are not listed in priority order. Strikethrough indicates those words in the Draft Port</i></p>	<p>A copy of the comments and attached letter were forwarded to The Planning Partnership for consideration. Further review is required by staff.</p>

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	<p>Association, via email</p>	<p><i>Dover Secondary Plan we recommend be deleted and bolding highlights new wording that is suggested.</i></p> <p><i>We have limited our response at this time to those items we consider significant and require meaningful consideration by the Study Group. Our suggested revisions will of course drive the need for further revision throughout the document which our volunteers are willing to help with if necessary. However, we wanted to submit this response now so that the Study Group would have our Association’s input for the July 21<sup>st</sup> meeting. We would appreciate you ensuring that this information is included in the Public Record and that you forward it to all members of the Study Group in advance of the July 21<sup>st</sup> meeting as well as to any other persons you feel would be interested.</i></p> <p><i>I would like to thank you for the advice and guidance you have provided our volunteers through this process. And, please pass on our appreciation to County Staff and The Planning Partnership for hosting the Engagement Sessions, answering questions and responding to public input. The sessions have helped us immensely in preparing a response that we hope will be helpful.</i></p> <p><i>Should you have any questions, please contact Jim Dover at 519-426-5677.</i></p> <p><i>Paul Creighton President Port Dover Waterfront Preservation Association</i></p> <p>See Appendix F.6.</p>	
<p>July 29, 2021</p>	<p>Norfolk Vacant Land Condominiu m Corporation No. 33, via Ron Mill,</p>	<p>County of Norfolk July 29, 2021 50 Colbourne Street South SIMCOE, ON N3Y 4H3 ATTENTION: Jennifer Catarino, MCIP, RPP, Senior Planner Dear Ms. Catarino: <b>Re: <u>Port Dover Secondary Plan – comments</u></b> The following comments are being submitted on behalf of the Flats at Black Creek condominium (Norfolk Vacant Land Condominium Corporation No. 33). The mapping of the Provincially Significant Wetland and Hazard Lands on the Land Use Schedule (Schedule B), appears to be out-of-date. In 2019, and prior to the turn-over of the Condominium and the associated common elements, the Developer (Black Creek Developments) prepared a study pursuant to Section 10.2 of the Zoning By-law to refine the boundaries of the PSW. That study was carried out by a firm of qualified biologists. It</p>	<p>Staff are working with the Long Point Region Conservation Authority and NVLCC No. 33 to determine the extent of the PSW. Further investigation is required.</p>

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		<p>included the staking and geolocating of the boundaries of the wetland. The study was submitted to and approved by MNR in 2019.          According to information provided as background by The Planning Partnership, it appears that the mapping has only been up-dated to include the 2018 data – the probable reason why the revised boundaries approved by MNR subsequent to that are not shown.          Notwithstanding, I am requesting, on behalf of NVLCC No. 33 that the revised boundaries between the Provincially Significant Wetland and the Hazard Land designations, as approved by MNR in 2019, on our property (Roll No. NCP-0033), be shown on Schedule “B” of the Port Dover Secondary Plan.          Thanking you for your attention to this matter, I remain.          Yours truly          ORIGINAL SIGNED          Ron Mills, on behalf of the Board of Directors, NVLCC No. 33</p> <p>c.c. Edd King, President, NVLCC No. 33          Robert Hunter, Black Creek Developments          Andy Balogun, Wilson Blanchard</p>	
<p>July 29, 2021</p>	<p>Jim Dover,          Port Dover Waterfront Preservation Association,          via email</p>	<p>Hi Jen!</p> <p>At a recent meeting of our Board of Directors I was asked to confirm whether the message below had been received and that the attached Deputation would be included On the Record of the Port Dover Secondary Plan as requested.</p> <p>We consider the issue addressed in the Deputation as relevant to the Port Dover Secondary Plan Work Group’s deliberations. The original Report and its Recommendations, submitted by the CAO to Council, was contrary to the assurance we were being given by senior Staff during the Engagement Sessions - that Senior Staff and Council supported the preservation and expansion of the public’s access to the waterfront.</p> <p>Fortunately, the second Report submitted by the CAO to Council was more in line with that assurance and an example of a proactive initiative that the County could undertake to preserve and enhance the public’s access to the waterfront. In accordance with our submission dated July 19, 2021, we believe that Section 3.2 j) Waterfront Acquisition of</p>	<p>The deputation is attached to this report as Appendix F.7.</p>

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		<p>the Port Dover Secondary Plan should be revised to commit the County to pursue a variety of proactive initiatives such as this one in addition to “waterfront acquisition” which is the only method currently referenced.</p> <p>Thanks! If you have any questions, please call.</p> <p>Jim Dover Vice President Port Dover Waterfront Preservation Association</p> <p><b>From:</b> Jim and Mie Dover <b>Sent:</b> July 20, 2021 9:51 PM <b>To:</b> Jennifer Catarino (<a href="mailto:Jennifer.Catarino@norfolkcounty.ca">Jennifer.Catarino@norfolkcounty.ca</a>) &lt;<a href="mailto:Jennifer.Catarino@norfolkcounty.ca">Jennifer.Catarino@norfolkcounty.ca</a>&gt; <b>Cc:</b> Tricia Givens (<a href="mailto:Tricia.Givens@norfolkcounty.ca">Tricia.Givens@norfolkcounty.ca</a>) &lt;<a href="mailto:Tricia.Givens@norfolkcounty.ca">Tricia.Givens@norfolkcounty.ca</a>&gt;; Donna Hinde (<a href="mailto:dhinde@planpart.ca">dhinde@planpart.ca</a>) &lt;<a href="mailto:dhinde@planpart.ca">dhinde@planpart.ca</a>&gt;; Brandon Sloan (<a href="mailto:brandon.sloan@norfolkcounty.ca">brandon.sloan@norfolkcounty.ca</a>) &lt;<a href="mailto:brandon.sloan@norfolkcounty.ca">brandon.sloan@norfolkcounty.ca</a>&gt; <b>Subject:</b> Port Dover Secondary Plan Submission</p> <p>Hi Jen!</p> <p>Would you please ensure the Port Dover Secondary Plan Study Group receives a copy of the attached deputation that I made today to Council on behalf of the Port Dover Waterfront Preservation Association regarding the proposed closure of a pathway leading down to the Port Dover beach together with this covering email.</p> <p>I'd also appreciate if you would ensure both are placed On The Record.</p> <p>Thanks!</p> <p>Jim</p> <p>See Appendix F.7 for original attachment to the email.</p>	
July 30, 2021	Andrew Hannaford,	Good morning,	

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	<p>from MHBC Planning, Urban Design &amp; Landscape Architecture on behalf of Shore Developments Limited (225 &amp; 230 Chapman Street and 2 Lynn Street), via email</p>	<p>On behalf of our client, Shore Developments Limited, please find attached our comments on the Draft Port Dover Secondary Plan and Design Guidelines released by Norfolk County with respect to the lands located at 225 and 230 Chapman Street East and 2 Lynn Street.</p> <p>We appreciate the opportunity to provide these initial comments and trust the County will take them into consideration during the Secondary Plan’s further review and revisions.</p> <p>Should you have any questions, please do not hesitate to contact me.</p> <p>Thank you,</p> <p><b>ANDREW HANNAFORD</b>, BES, MCIP, RPP   Senior Planner</p> <p><b>MHBC Planning, Urban Design &amp; Landscape Architecture</b>          442 Brant Street, Suite 204   Burlington   ON   L7R 2G4   T 905 639 8686 x 236   <a href="mailto:ahannaford@mhbcpplan.com">ahannaford@mhbcpplan.com</a></p> <p>Formal letter is attached as Appendix F.8.</p>	
<p>August 8, 2021</p>	<p>Cathy Salvatore, on behalf of the Climate Action Committee of the Long Point Biosphere Reserve, via email</p>	<p>To Norfolk County Mayor and Council,</p> <p>Thank you very much for the opportunity to provide feedback on the Port Dover Secondary Plan as developed thus far. It is a progressive and well-thought-out plan.</p> <p>As a committee of the Long Point Biosphere committed to promoting and supporting actions to mitigate and adapt to climate change, we are pleased to see the inclusion of such actions in the plan. The PD secondary plan is also in line with the recently approved Norfolk County Climate Adaptation Plan which this group actively supports.</p> <p>Although there are many strategies in this plan that are ecologically sound, several of the most important points from a climate change and ecological standpoint are included in the appendices.</p>	<p>Further review is required.</p>

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	<p><b>Plan Elements that Support Climate Action</b></p> <p>It is great to see an emphasis in the plan on <b>active transportation</b>. Vehicles are the leading <u>carbon emitter</u> for individuals (London GHG audit, Norfolk GHG audit). Encouraging active transportation through appropriate infrastructure such as walking and bike lanes is effective in limiting or reducing carbon emissions and pollution from vehicles. As well, fewer vehicles on the streets means less local heating produced by internal combustion engines, which will become increasingly important as we experience increasingly intense and longer heat waves.</p> <p>Regarding lighting, <b>keeping lighting to a minimum</b> in Port Dover will minimize harm to vulnerable insects (the majority of pollinator insects are nocturnal), migrating birds, and a variety of other nocturnal species which are threatened by climate change as well as other factors. Some Port Dover neighbourhoods have traditionally lacked street-lighting and residents have been grateful for the opportunity for night-sky viewing.</p> <p>The use of <b>permeable paving</b> for newly acquired municipal parking lots or those requiring resurfacing will reduce local flood risks associated with climate change and improve filtering of water before entry into waterways and the lake. This will complement native plantings around parking lots, which filter both air and water. Electric car chargers will be an important inclusion for parking lots, and building the infrastructure to add more chargers as electric cars are increasingly adopted will ensure readiness for the (near) future.</p> <p>It is our view that the number of <b>drive-through restaurants or services should be limited or reduced</b>. These establishments encourage a car culture and vehicle idling with the associated GHG emissions, air pollution and local heating effects.</p> <p>We strongly support the planting of location appropriate <b>native plants, including trees</b>, wherever possible. Planting a variety of native vegetation has been demonstrated to <b>cool</b> the immediate area up to 4.5 degrees Celcius (Hamberg, 2020), as well as to <b>sequester significant amounts of carbon</b>. Native plantings also serve many climate adaptation functions. Depending on the species, native plants are drought tolerant or can absorb excess water, and control erosion. Native gardens, especially those that include keystone species, support insects which are in serious decline. Insects create the biological foundation for all terrestrial systems – they cycle nutrients, pollinate plants,</p>	
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	<p>disperse seeds, and maintain soil structure and fertility, as well as control populations of other organisms. Insects generate billions of dollars for the economy.</p> <p>Native plants also support other wildlife that introduced plant species can not. Many existing grassed areas on Norfolk County land could be replaced with appropriate native species to serve the above functions as well as to ultimately reduce regular maintenance (i.e., mowing) which currently emits a large amount of carbon. With appropriate signage and publicity, public native gardens may educate and encourage residents to undertake similar projects on their own properties.</p> <p>Identifying existing <b>natural spaces</b> in Port Dover is a great step. However many of these spaces are currently littered with garbage and full of invasive species which inhibit native plant species. These issues compound each other - an unattractive area is not seen as a valuable wildlife area. It is suggested that Norfolk County pairs with community groups to clean these areas and address the issue of invasive species. Access to truly natural local areas that support a variety of species supports the notion that nature can and should be nurtured and enjoyed everywhere, and is not just a specific travel destination.</p> <p>Relatedly, engaging with local businesses around <b>decreasing the use of single-use plastics</b> is recommended. The Biosphere's Cleaning Up Norfolk committee has regularly conducted beach and trail cleanups where discarded takeout items and beverage containers, along with cigarette butts, are the majority of items collected. In Port Dover the tourist, commercial fishing, recreation, and service industry revolve around Lake Erie and its ecological sustainability. In addition to their impact on water quality and ecology, single-use plastics have a large carbon footprint compared to plant-based products and are a huge contributor to landfill capacity problems and their associated footprint. Encouraging or mandating <b>reusable items</b> for patio dining, along with <b>compostable takeout containers</b>, would significantly reduce litter in the beach and lake areas. Establishing composting stations as well as expanding the <b>municipal composting program</b> would result in significantly lower carbon emissions from food waste (Allen, Cancel and Arduna, 2020).</p> <p>The installation of <b>water fountains/filling stations</b> in highly-frequented areas would help reduce the prevalence of one of the most common waste items discarded. Bottled water is 2000 times more energy intensive than tap water and uses four times the</p>	
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		<p>amount of water. With only 20 to 50 per cent of plastic bottles recycled, they are a huge expense in terms of landfill space. Water stations placed at high traffic tourist and recreation destinations (e.g., sports complex, beach area) would directly address the issue (Blue Bayfield project). In addition, publicly available water stations is an effective adaptation to heat waves, especially for high-risk populations.</p> <p>Consideration should be given to increasing <b>cigarette butt disposal units</b> as cigarette butts are the single-most common item picked up at cleanups and are toxic to aquatic life. Healthier aquatic systems are more resilient to climate change impacts.</p> <p>Finally, installing <b>green energy micro-grids</b> (which continue to provide electricity during power outages associated with more severe and frequent storms) and green roofs (cool, absorb runoff, etc.) are excellent ways to mitigate and adapt to a changing climate and it is extremely encouraging to see these recommendations in the plan.</p> <p>Thanks again for this opportunity to comment on the Port Dover Secondary Plan. It is hoped that the positive climate and ecologically friendly aspects of this plan will be adopted county-wide.</p> <p>Submitted by Cathy Salvatore on behalf of the Climate Action Committee of the Long Point Biosphere Reserve</p>	
<p>August 11, 2021</p>	<p>Carmen Jandu, IBI Group, on behalf of Knechtel Lands – Port Dover</p>	<p>Good Afternoon Jennifer, hope you are well.</p> <p>I reviewed the June 2021 Draft of the Port Dover Secondary Plan and had some questions that I hope you can clarify for me. I found that the Draft Secondary Plan was generally comparable to the original 2001 Sec. Plan which encouraged development and redevelopment which we were happy to see.</p> <p>We are currently drafting a letter in response to the proposed Secondary Plan Policies. Clarification regarding the above would help us evaluate the proposed policies more comprehensively. Our questions are as follows:</p> <ol style="list-style-type: none"> <li>1. We note that the majority of the areas designated Urban Waterfront on Schedule B Land Use are also designated as within the Greenlands System on Schedule A. The Greenlands System generally prohibits development while the Urban</li> </ol>	<p>Hi Carmen,</p> <p>Thank you for your questions and I look forward to receiving the formal letter. I have provided responses below in blue.</p> <p>Please do not hesitate to contact me if you have any additional questions or comments.</p> <p>Jen</p>

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		<p>Waterfront policies provide for development and redevelopment within the designation. Can you clarify if areas within the Greenlands System would permit new development?</p> <ol style="list-style-type: none"> <li>2. Are area's within the Greenlands system considered as being within the Built Up Area and are they areas where intensification would be directed?</li> <li>3. We note that the Hazard land overlay does not include the lands west of Walker Street, can you please confirm if the area west of Walker Street will need to comply to Section 7.3 of the Parent OP?</li> <li>4. Will the Port Dover Secondary Plan supersede the Lakeshore Secondary Plan as well as Policies contained within Section 6.5.2.2 and 6.8.1 of the Parent OP?</li> <li>5. It is not clear to me if the SP permit ground floor residential within the Urban Waterfront designation as the proposed Urban Design Guideline in Appendix I provide policies for ground floor residential requiring connection to the pedestrian zone.</li> </ol> <p>I appreciate your help, looking forward to hearing back from you.</p> <p>Carmen</p> <p>Carmen Jandu MCIP RPP</p> <p>Associate, Sr. Planner mob 416-938-3838</p> <p><b>IBI Group</b> Suite 200, East Wing-360 James Street North Hamilton ON L8L 1H5 Canada tel +1 905 546 1010 ext 63106</p>	<p><b>Jennifer Catarino, MCIP, RPP</b> Senior Planner Community Development Division 185 Robinson Street, Simcoe, Ontario, Canada, N3Y 5L6 <u>519-426-5870</u> x. 8013</p>  <p><a href="http://norfolkcounty.ca">Working together with our community norfolkcounty.ca</a></p> <ol style="list-style-type: none"> <li>1. We note that the majority of the areas designated Urban Waterfront on Schedule B Land Use are also designated as within the Greenlands System on Schedule A. The Greenlands System generally prohibits development while the Urban Waterfront policies provide for development and redevelopment within the designation. Can you clarify if areas within the Greenlands System would permit new development?</li> </ol> <p>As a note, the schedules will be updated in the next draft of the plan to be easier to read. In response to your question, the Greenlands System illustrates the Natural Heritage System and some parks/open space. It is not a landuse designation, rather it helps illustrate the existing open space and potential connectivity within Port Dover. Where a site is designated as Hazard Lands, Provincially Significant Wetlands and/or a significant woodlot, the standard policies will apply.</p> <ol style="list-style-type: none"> <li>2. Are area's within the Greenlands system considered as being within the Built Up Area and are they areas where intensification would be directed?</li> </ol>
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			<p>The Greenlands system is located within the built boundary and the “future neighbourhood” designation. As noted above, any development proposal would have to reflect the policies of the land use designation on Schedule B.</p> <p>3. We note that the Hazard land overlay does not include the lands west of Walker Street, can you please confirm if the area west of Walker Street will need to comply to Section 7.3 of the Parent OP?</p> <p>The Schedules are under review and will be updated to reflect the existing areas that are designated as Hazard Lands. I believe the area you are referring to is subject to the 100 year flood regulations of the Long Point Region Conservation Authority.</p> <p>4. Will the Port Dover Secondary Plan supersede the Lakeshore Secondary Plan as well as Policies contained within Section 6.5.2.2 and 6.8.1 of the Parent OP?</p> <p>We are currently working with The Planning Partnership on the approach to applying this Secondary Plan and its relationship to all the other policies in Norfolk County.</p> <p>Staff have flagged the absence of Section 6.5.2.2 and are working with the consultants on this item.</p> <p>5. It is not clear to me if the SP permit ground floor residential within the Urban Waterfront designation as the proposed Urban Design Guideline in Appendix I provide policies for ground floor residential requiring connection to the pedestrian zone.</p>
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Appendix F – Comments received up until September 10, 2021.

			<p>It is the intent of the Urban Waterfront designation to permit residential uses within mixed use building and staff have flagged this item to be addressed in the next draft of the Plan.</p> <p>Looking through the Urban Design Guidelines, which section refers to ground floor residential as use?</p>
<p>September 1, 2021</p>	<p>Paul Mccorquodale, Revera, via email</p>	<p>Hi Jennifer, Thank you for speaking with me regarding the Port Dover Secondary Plan. I appreciate the time that you have taken to chat multiple times over the last few weeks.</p> <p>As discussed, I just wanted to follow up with you regarding the Secondary Plan for Port Dover. As you know, we have been working with the owners of the Dover Coast Development to acquire a 5.5 acre parcel for the purposes of constructing a new Long Term Care Home at Dover Cliffs Blvd and Barrett Court (east side). We would be proposing a Long Term Care Home for 128 residents (2 stories). From our previous conversations, I understand that a Long Term Care Home would fall under the definition of Special Needs Housing (Section 5.3.5).</p> <p>As discussed, I would like to confirm that when the time comes for us to proceed with a development application that we will not have to complete an amendment to the Secondary Plan / Official Plan. As such, I would like to get your thoughts on Section 6.4.2 a) iii), and whether it is intended to exclude Special Needs Housing that accommodate more than 8 residents / occupants.</p> <p>Thank you.</p> <p>Sincerely,</p> <p><b>Paul McCorquodale</b> Manager, Property Development</p>	<p>Response comments are pending.</p>

Appendix F – Comments received up until September 10, 2021.

		<p><b>Revera Inc.</b>                      5015 Spectrum Way, Suite 600, Mississauga, ON, L4W 0E4  <b>T: 905-267-0598 C: 289-937-0372</b>  <a href="mailto:paul.mccorquodale@reveraliving.com">paul.mccorquodale@reveraliving.com</a></p> <p><a href="http://reveraliving.com">reveraliving.com</a>                      Respect   Integrity   Compassion   Excellence</p>	
<p>September 9, 2021</p>	<p>Carmen Jandu, IBI                      Carmen Jandu</p>	<p>Good Afternoon Jennifer, hope you are well.</p> <p>As previously discussed, we have had an opportunity to review the Draft Port Dover Secondary Plan and respectfully provide our comments as outlined in our letter attached.</p> <p>Sincerely,</p> <p>Carmen Jandu MCIP RPP</p> <p>Associate, Sr. Planner                      mob 416-938-3838</p> <p><b>IBI GROUP</b>                      Suite 200, East Wing-360 James Street North                      Hamilton ON L8L 1H5 Canada                      tel +1 905 546 1010 ext 63106</p> <p>Formal letter is attached as Appendix F.9.</p>	<p>Response comments are pending.</p>

# Appendix F.1

## **PORT DOVER SECONDARY PLAN** **WATERFRONT AREA IDEAS – LIGHTHOUSE TRAIL**

During the online discussions on the Waterfront Areas of the Port Dover Secondary Plan, the following concerns and ideas were discussed by several people either directly or indirectly.

### **AREAS OF CONCERN:**

1. Create a trail to connect the end of the Lynn Valley Trail and the lighthouse.
2. Public viewing of the Lynn River, Black Creek and Silver Lake.
3. Public access to the shorelines along the Lynn River, Black Creek and Silver Lake.

### **INTRODUCTION (See Diagram #1)**

The Lynn Valley Trail is a very popular trail between Simcoe and Port Dover. The Port Dover end of the trail is at Bridge Alley by the Port Dover Lions Silver Lake Park and their Market Building. Many people would like a trail between this end of the Lynn Valley Trail and the lighthouse at the end of the pier. There has been a suggestion that this trail be called the Lighthouse Trail.

This document outlines the preliminary concept of one possible path for the Lighthouse Trail.

### **SECTION A: BRIDGE ALLEY TO CHAPMAN STREET (See Diagram #2)**

The Lynn Valley Trail follows the old railway right of way and ends at Bridge Alley. The preliminary concept for the revitalization of Silver Lake includes reclaiming land at the end of Bridge Alley along the shoreline of Silver Lake. This land would have benches for viewing Silver Lake and a floating dock so that canoes, kayaks and paddle boards could be launched.

The original railway right of way continues on to Chapman Street. Between Bridge Alley and Nelson Street, it is a narrow road with no sidewalk. A sidewalk or dedicated walking path could connect these two roads.

Where Nelson Street ends at Silver Lake, the view of the water is obstructed by scrub trees. If the scrub trees are removed, then benches could be installed for viewing Silver Lake. Also, the preliminary concept for the revitalization of Silver Lake includes reclaiming land at the end of Nelson Street for a park.

Between Nelson and Chapman Streets, there is a storage business on land previously used as a works yard by Norfolk County. If this land is still owned by Norfolk County, then when the lease is up, this land could be cleaned up for use by the public. This area is large enough for a walkway, benches and some parking. There are several possible uses for the building on the property related to the revitalization of Silver Lake. For example, it could become an interpretive centre or a visitor centre.

Ideally, a pedestrian walkway would be created from the old works yard to Chapman Street. This would allow for the Lighthouse Trail to connect with the sidewalk on Chapman Street. People could then walk along Chapman Street to view the Misner Dam and/or continue on to Bridge Street.

### **SECTION B: CHAPMAN STREET TO BRIDGE STREET (See Diagram #3)**

Pedestrians following the trail along Chapman Street will have a choice to make when they get to Lynn Street. They could cross Chapman Street, turn left and follow the right of way of the old road to the Misner Dam. At this location there could be a viewing area to see the repaired Misner Dam, the south end of Silver Lake and down the Lynn River to the bend. There could also be story boards outlining the history of the Dam, the original Norfolk Knitters and other local businesses.

They could also cross Chapman Street and follow the path shown in Diagram #3 along the Lynn Street property to Bridge Street.

The Development document of the Lynn Street property includes the following interesting proposals:

- a change in the location of the 100 Year Floodplain (ref: Section 4.1 Design Process, Lynn Street Dev Plan). The proposed new location of the flood plain is the line on Diagram #3 closest to the Lynn River.
- the development will include “a passive trail system along the Lynn River to allow connections along the waterfront area.” (ref: “Section 4.2 Connectivity/Access”, Lynn Street Dev Plan.)

As shown on Diagram #3, the Lighthouse Trail could follow the line of the proposed new floodplain to connect Chapman Street with Bridge Street. This section of the trail should be as close as possible to the Lynn River.

### **SECTION C: BRIDGE STREET (See Diagram #4)**

At the present time, Bridge Street is a narrow road with a very poor view of the water. The road is situated on the side of the road allowance farthest from the river and there are many scrub trees between the road and the water. Replacing the scrub trees with well-placed Carolinian trees would improve the view of the water.

While the present road is narrow, the road allowance varies in width from 20 m (66 ft) to 35 m (113 ft). Where the road is the widest, Bridge Street is on high ground and the bank is very steep. At this location, a viewing point could be constructed to give a very good view up river to the confluence of the Lynn River and Black Creek and down river to the lift bridge. There could be benches and signage. This viewing point is also the location where the Lighthouse Trail would meet Bridge Street if it follows the route described in Section B.

At the present time, there is very little traffic on Bridge Street. The traffic is two-way and there is no sidewalk. There will likely be more pedestrians using Bridge Street once the condos and townhouses proposed for the area are completed. Making Bridge Street a one-way street would allow for a sidewalk along the entire length of Bridge Street. This will make walking along Bridge Street safer for pedestrians.

#### **SECTION D: ST PATRICK and HARBOUR STREETS (See Diagram #5)**

St Patrick Street extends past Bridge Street to the water just north of the lift bridge. This area could be developed to give public access to the water. Benches could be installed for public viewing or resting.

Just north of the lift bridge, there is a very small inlet. A small dock or ramp would allow for paddle boards, canoes and kayaks to be launched upstream of the lift bridge.

At this end of St Patrick Street is a set of stairs up to the lift bridge. Crossing Hwy 6 or the Plank Road, leads pedestrians to a set of stairs from the lift bridge down to Riverfront Park by the Port Dover Museum. For safety reasons, a pedestrian crossing should be considered. Ideally, a pedestrian tunnel could be installed under the road so that pedestrians would not have to climb the stairs or cross a busy road.

At the present time, there is no sidewalk connecting the stairs by the lift bridge to the sidewalk in Riverfront Park. A sidewalk in this area would help to complete the Lighthouse Trail between the end the Lynn Valley Trail and the lighthouse. There are already benches for viewing the water but they are hard to access because there is no sidewalk.

The final section of the Lighthouse Trail from the Riverfront Park and the Port Dover Museum to the lighthouse will soon be completed. Once the new condo development on Harbour Street is completed, there will be a pedestrian walkway joining the Museum walkway with the pier at the bottom of Main Street.

#### **SUMMARY (See Diagram #6)**

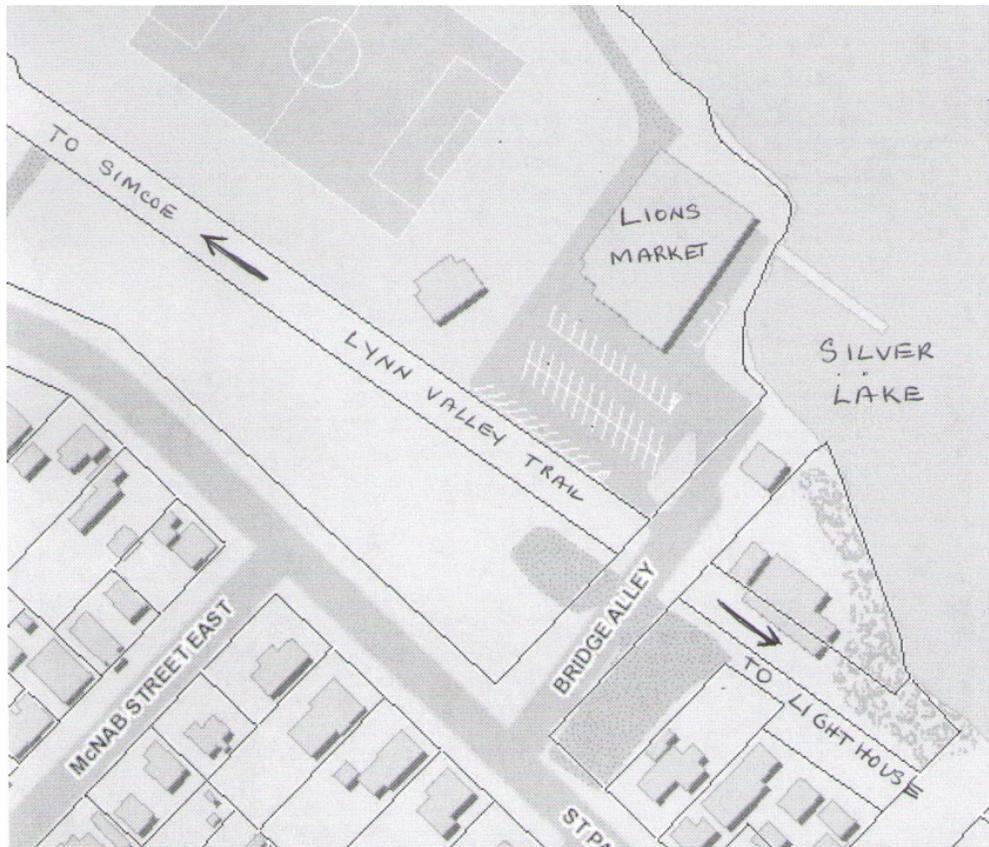
If all of these locations are developed as suggested:

1. **LINKING TRAILS:** The Lynn Valley Trail would be linked to the Lighthouse Trail at Bridge Alley. The Lighthouse Trail would then continue to Chapman Street, along Chapman Street, a path along the Lynn River in the Lynn Street development, a sidewalk along Bridge Street, the stairs by the lift bridge, or a tunnel, and a new sidewalk between the stairs and Riverfront Park. From Riverfront Park there is (or will be once the condo construction is completed) pedestrian access all the way to the lighthouse at the end of the pier.

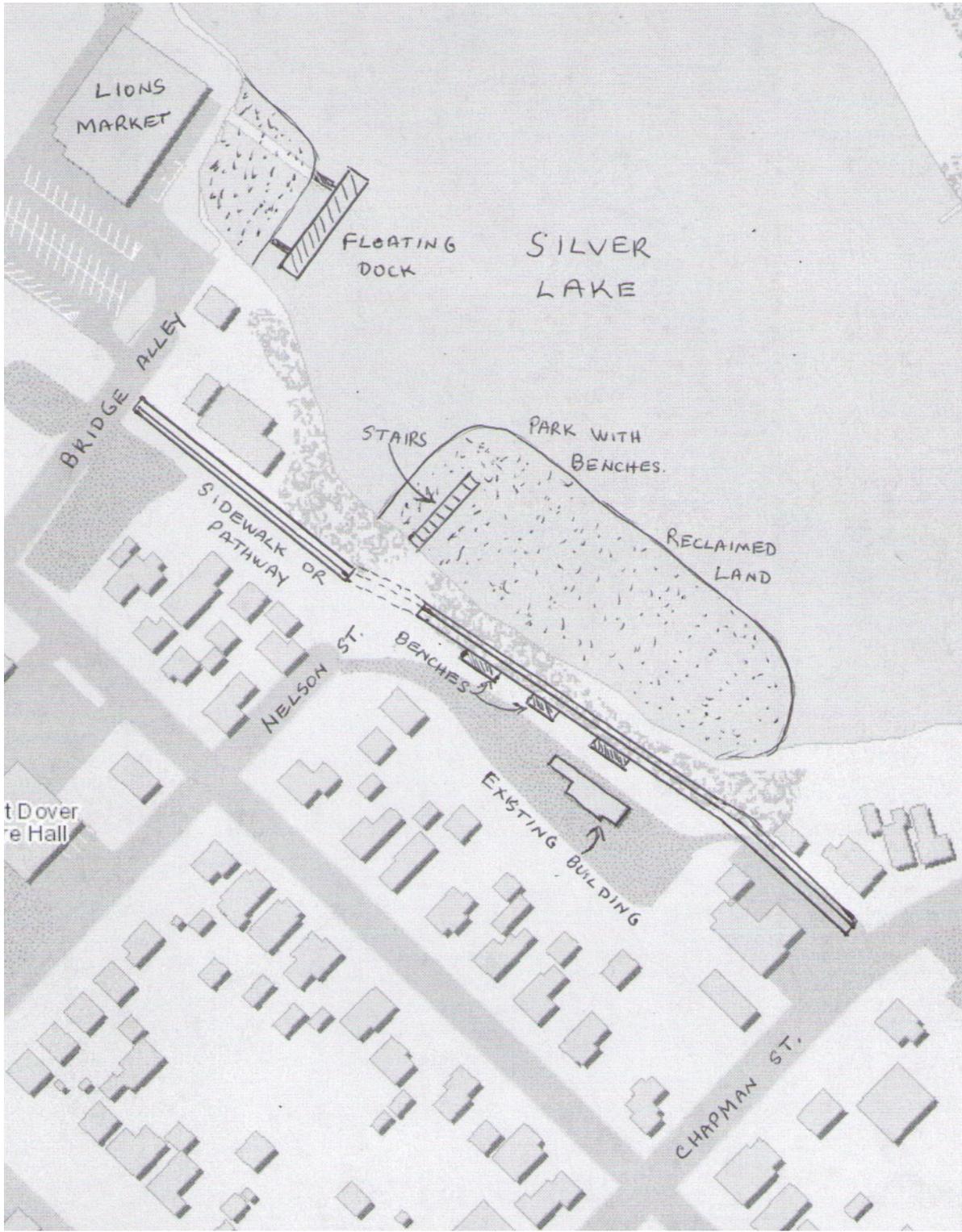
2. **PUBLIC VIEWING:** There would be public viewing: LHT page 4
- a. of Silver Lake from the end of Bridge Alley, the end of Nelson Street and by the Misner Dam
  - b. of the Lynn River and Black Creek from a path along the flood line by Lynn Street and a viewing point where Bridge Street is the widest.
  - c. of the harbour all the way from St Patrick Street to the end of the pier.
3. **PUBLIC ACCESS:** There would be public access:
- a. to Silver Lake at the end of Bridge Alley and the end of Nelson Street when the revitalization of Silver Lake is completed.
  - b. to the Lynn River at the end of St Patrick Street.
  - c. to the harbour at Riverfront Park.

As stated earlier, this document is intended to be a preliminary concept of one possible path for the Lighthouse Trail. It is the starting point for a discussion of what could become a popular trail in Port Dover. The final path of the trail depends on many things including the final plans for both the Misner Landing and Lynn Street developments. Negotiations with both developers will be required before the route of the Lighthouse Trail can be finalized.

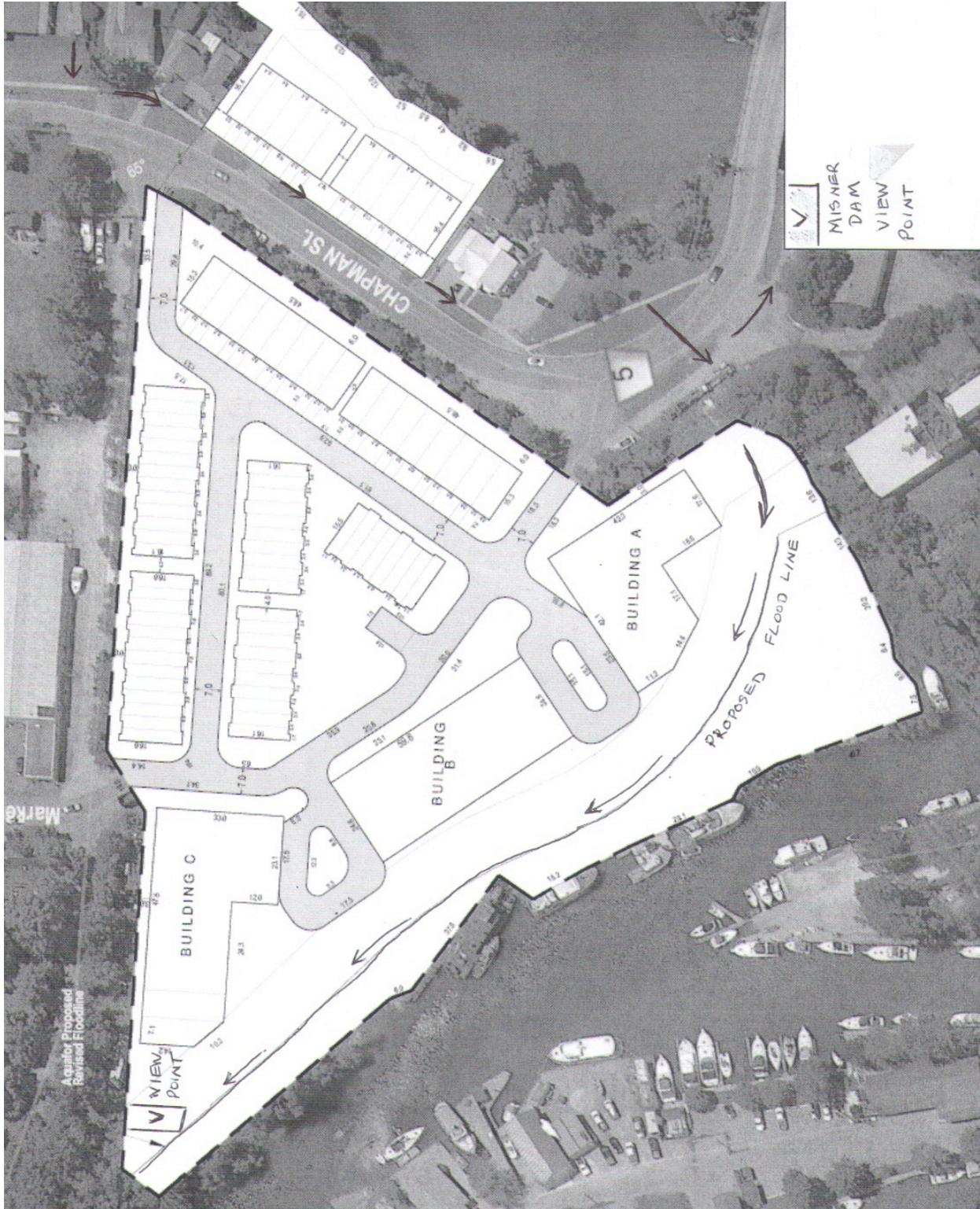
#### **DIAGRAM #1: INTRODUCTION**



**DIAGRAM #2 – SECTION A: BRIDGE ALLEY TO CHAPMAN STREET**



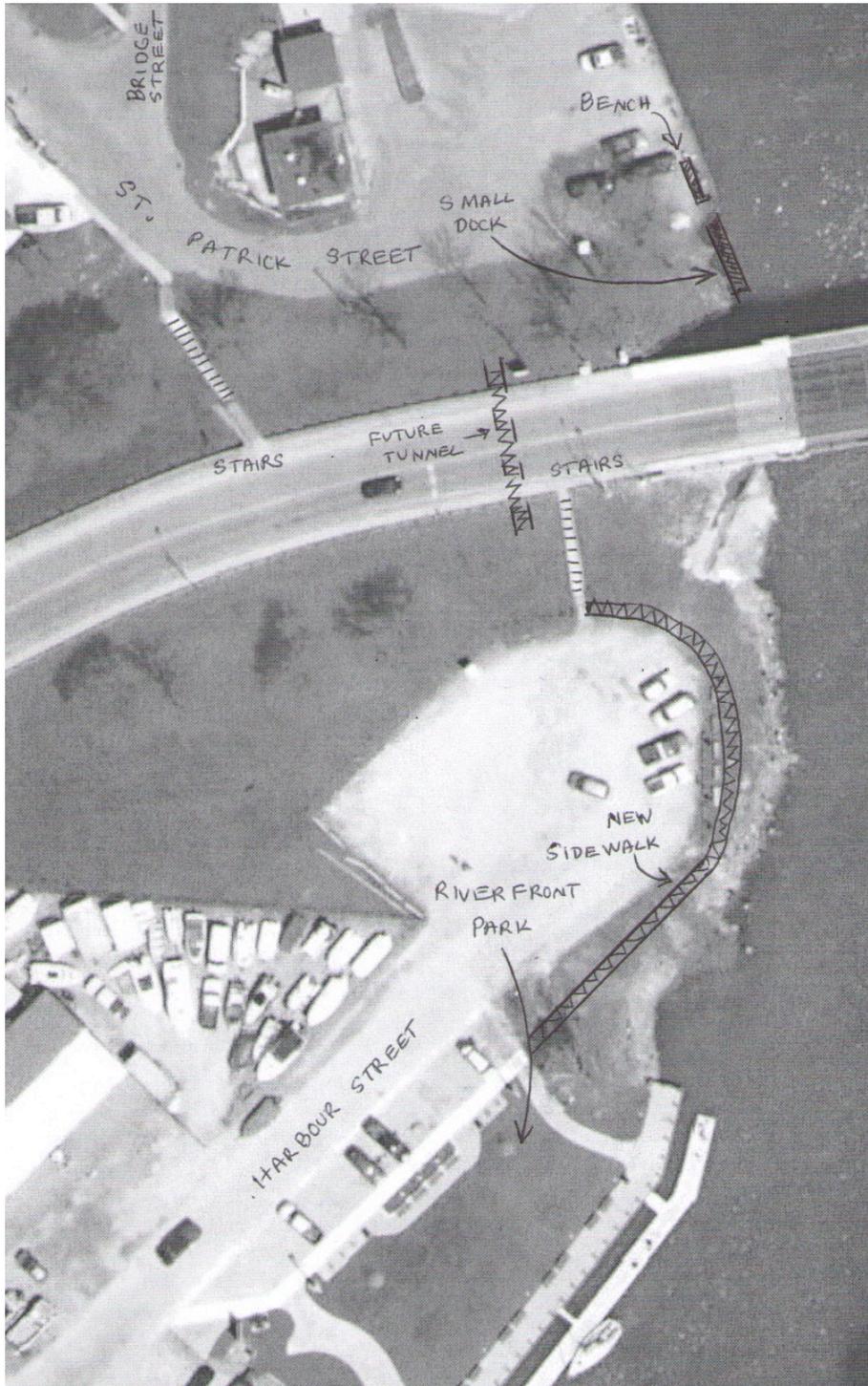
**DIAGRAM #3 – SECTION B: CHAPMAN STREET TO BRIDGE STREET**



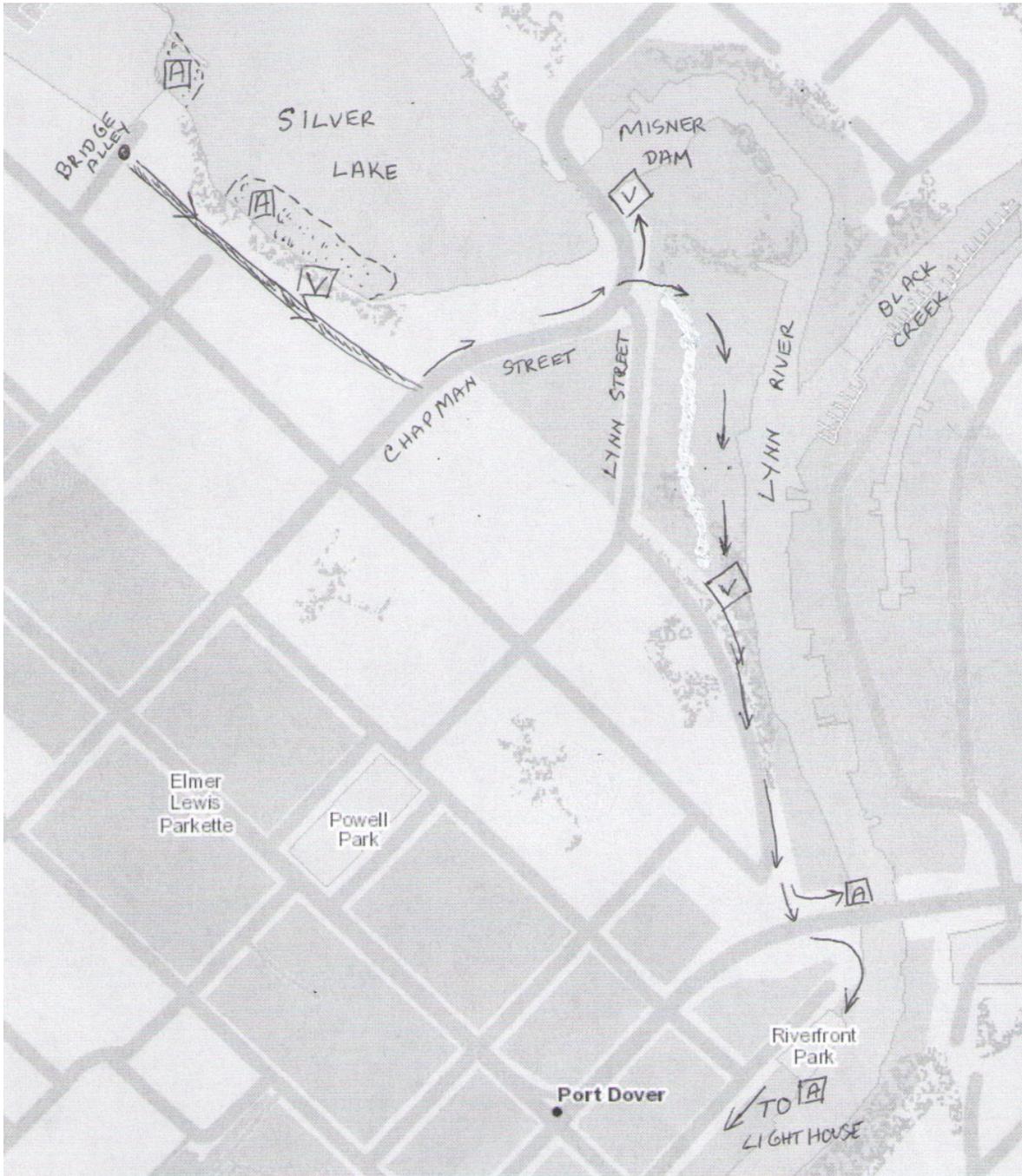
**DIAGRAM #4 – SECTION C: BRIDGE STREET**



**DIAGRAM # 5 – SECTION D: ST PATRICK and HARBOUR STREETS**



**DIAGRAM #6: THE LIGHTHOUSE TRAIL**



"V" denotes a dedicated viewing area with benches and signage. There will also be water views from other locations.

"A" Denotes public access to the water. Where there is access, there will also be viewing of the water.

**LIGHTHOUSE TRAIL PHOTOGRAPHS**



Lynn Valley Trail to Simcoe



The start of the Lighthouse Trail between Bridge Alley and Nelson Street. The former Norfolk County works yard is at the top of the picture.



South end of Silver Lake from the Chapman Street bridge. The building on the right is the Lions Market building at the end of bridge Alley.



View across the Lynn River from the intersection of Lynn and Chapman Streets.



View down the Lynn River to the lift bridge from the intersection of Lynn and Chapman Streets.



View from the approximate location of the viewing platform on Bridge Street showing where the Lynn River and Black Creek meet.



The intersection of St Patrick Street and Bridge Street. The buildings on the left belong to Norfolk County. They are located on the road allowance.



The end of St Patrick Street showing where benches could be located and the small inlet where a small dock or ramp could be installed.



The walkway along the water in Riverfront Park, the floating dock with the lighthouse in the distance.

## Appendix F.2

### PARKING IN PORT DOVER

Parking has always been a concern in Port Dover especially in the summer. There are three locations where additional parking could be created.

#### **BRIDGE STREET (See Diagram #1)**

At the present time, Bridge Street is a narrow road with very little traffic. The traffic is two-way and there is no sidewalk. At the St Patrick Street end of Bridge Street, the road is very narrow with a steep bank down to the river on one side and a hill on the other side. If Bridge Street stays as a two-way street, there is no room for a sidewalk near St Patrick Street.

There will likely be more pedestrians using Bridge Street once the condos and townhouses proposed for the area are completed. Bridge Street is the shortest route to the lighthouse, the beach area and Tim Horton's from the two developments. Making Bridge Street a one-way street would allow for a sidewalk along the entire length of Bridge Street. This will make walking along Bridge Street safer for pedestrians.

While the present road is narrow, the road allowance is quite wide. At Market Street, the Bridge Street road allowance is about 24 m (79 ft). It then widens to about 35 m (115 ft) and then becomes narrower as it approaches St Patrick Street.

Bridge Street has a usable width of at least 20 m for a distance of about 100 m from Market Street. If Bridge Street is one way as shown, then there would be room for one driving lane, a wide sidewalk and angled parking in this area. If this area is compared to the streets on either side of Powell Park, there should be parking for about 30 cars.

A viewing platform could be built where Bridge Street is the widest. There is a good view up the river to where the Lynn River and Black Creek meet. A few of the parking spots could be reserved for people wanting to see the view.

#### **NELSON STREET AT SILVER LAKE (See Diagram #2)**

Between Nelson and Chapman Streets, there is a storage business on land previously used as a works yard by Norfolk County. If this land is still owned by Norfolk County, then when the lease is up, this land could be cleaned up for use by the public.

This area is large enough for a wide walkway, benches and some parking. There are several possible uses for the building on the property related to the revitalization of Silver Lake. For example, it could become an interpretive centre or a visitor centre.

The former Norfolk County works yard at the end of Nelson Street is relatively large. Leaving room for a wide pathway along the edge of Silver Lake and benches for viewing the water, it should be possible to create parking for about 40 to 45 cars if they are parked perpendicular to the property line and the sidewalk as shown.

### **HARBOUR STREET BY RIVERFRONT PARK**

At the end of Harbour Street by the Riverfront Park, there is an open area where many cars are usually parked. The parking is disorganized and cars can become blocked. A more efficient use of this area would be to pave the area and have designated parking spaces. The parking could be angled parking with the traffic flow being one-way similar to the main parking area by Lakeside Savings. This parking area is slightly wider than the main parking lot on St George Street and about 1/3 the length. This means that about 15 – 18 cars could be parked in this area.

### **PARKING METERS**

Norfolk County should consider installing the new style of parking kiosks in many areas of Port Dover. The new style of parking kiosk uses credit cards. The driver prints a parking ticket to place on the dashboard. Some parking kiosks have an App so that you can add time to your ticket using your smart phone. There could be different rates for the time of the year, the day of the week and possibly for special dates. There could be an overnight rate. Different areas could have different rates.

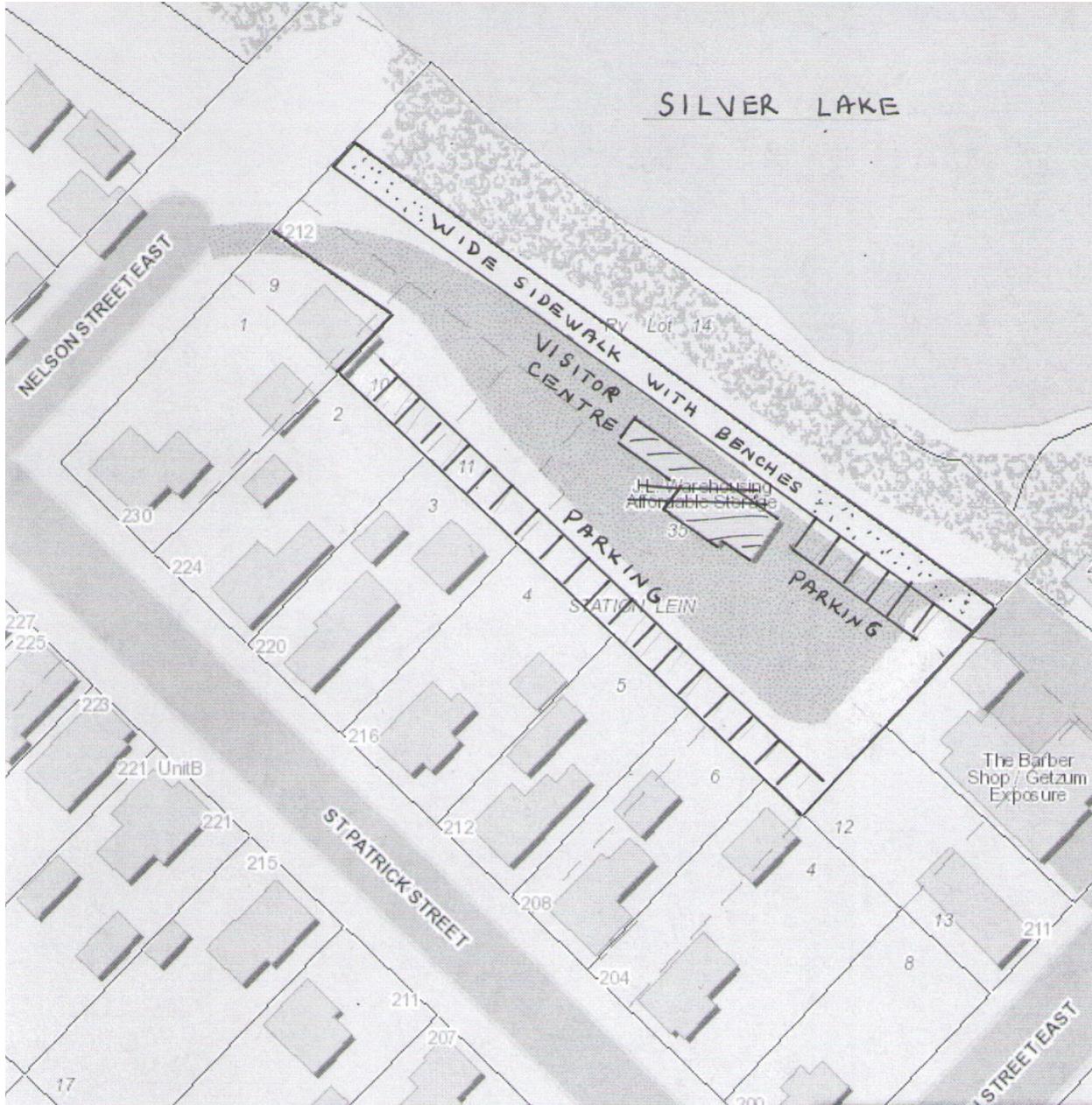
Examples:

- Main Street and Walker Street could have different rates.
- The paid parking could be only between May 1 and October 31. It could be free the rest of the year.
- The paid parking could be only weekends and holidays between May 1 and October 31. It could be free the rest of the year.
- Along Main Street where stores are located, the first 30 minutes could be free in the same way that the parking limit by the Post Office is 15 minutes.
- In the main municipal lot on St George Street, the paid parking could be only from 9:00 AM to 6:00 PM. The first 30 or 60 minutes could be free or have a very much lower rate (e.g. \$1 for the first 60 minutes)

As you can see from these examples, there are a variety of options available for the parking rate if the new style parking kiosks are used.



**DIAGRAM #2 – NELSON STREET at SILVER LAKE**



## Appendix F.3



Port Dover Waterfront  
PRESERVATION ASSOCIATION

### PLEASE INCLUDE IN COUNCIL'S INFORMATION PACKAGE

June 9, 2021

Brandon Sloan

General Manager, Community Development Division

Norfolk County

185 Robinson St., Suite 100

Simcoe, ON N3Y 5L6

**Subject: Former Public Works Yard – Nelson St. Port Dover**

On behalf of the Port Dover Waterfront Preservation Association (PDWPA), I would like to request that the former Public Works Yard property, located on the Silver Lake shoreline at the east end of Nelson Street in Port Dover, be redesignated from Urban Residential to Open Space.

Our Association is leading Phase 1 of the Silver Lake Revitalization Project. One of the Project's objectives is to provide recreational opportunities on the water and shoreline for everyone's enjoyment. For example, we envision canoeing and kayaking as well as a trail along the Silver Lake and Lynn River waterfronts that connects the end of the Lynn Valley Trail to the Port Dover harbour.

As you know, proposed, and potential, residential development risks limiting public access to our waterfront. Representatives of the PDWPA and Port Dover Lions Club met with Tricia Givens and Jennifer Catarino on June 3, 2021 to discuss ways to protect and improve the public's physical access to Silver Lake specifically. We were pleased to hear that the Port Dover Secondary Plan review is considering revisions to the Official Plan that would serve to protect and improve the public's access to waterfront locations throughout Port Dover.

When discussing other strategies that could be used to secure the public's access to Silver Lake, we pointed out that the former Public Works Yard property would be an excellent location for a small waterfront park or viewing area. The property is currently Zoned as General Industrial and being leased by the County. If the property were redesignated to Open Space, consideration could be given to amending the Zoning to a classification that would complement the Silver Lake Revitalization Project objectives and improve the public's enjoyment of our waterfront.

I appreciate your consideration of our request and look forward to your response. Should you have any questions, please contact Jim Dover Vice President, PDWPA at 519-426-5677.

Sincerely,



Paul Creighton

President, Port Dover Waterfront Preservation Association

Copy: Teresa Olsen, County Clerk

Tricia Givens, Director of Planning

Jennifer Catarino, Senior Planner

Amy Martin Councillor, Ward 6

## Appendix F.4



### NEWS RELEASE

Date: June 22, 2021

#### Vision for Revitalized Silver Lake Taking Shape

Now that Misner Dam has been repaired, community organizations have come together to launch the Project to revitalize Silver Lake.

“For years we’ve had lawn signs throughout the community saying ‘Save Silver Lake.’ Well, now we’re setting out to do that,” says Paul Creighton, President of the Port Dover Waterfront Preservation Association (PDWPA).

The PDWPA has formed a Project Steering Committee and appointed a Project Manager to begin action on a renewed vision for this valued community asset.

The Steering Committee includes Creighton as Chairperson and the following members: Robert Mckinnon, President of the Port Dover Lions Club; Rick Levick, President of the Long Point Biosphere Reserve; Amy Martin, Councillor for Ward 6 Norfolk County; Al Homeniuk, member of the Port Dover Yacht Club; Nick Childs, President of the Port Dover Board of Trade; and Eric Ryerse, President of the Port Dover Harbour Authority.

Jim Dover, Vice President of the PDWPA, has been appointed Project Manager for Phase 1 of the Project.

“The work on Misner Dam had to be completed before we could focus our efforts on revitalizing Silver Lake and the wetland,” says Creighton. “We believe Silver Lake should be revitalized so that it can continue to provide environmental and recreational benefits into the future for everyone.”

“Doing nothing is not an option. If left on its own, the lake will continue to silt in, all the way to the dam, and be choked out by phragmites and purple loosestrife, as has already happened in the northern section.”

Creighton notes that the Project will likely take five to ten years to complete and will need to be done in phases.

The first phase involves bringing together a core group of volunteers, creating a website, and confirming with regulatory agencies the environmental studies and work approvals that will be required.

Sufficient funds to complete this first phase have already been raised. A website has been developed. A contract to consult with the regulatory agencies – local, provincial and federal – to confirm the studies and work approvals that will be required has been awarded to GHD, an environmental consulting firm based in Waterloo.

Details of Phase 2, and what needs to happen after that, will depend on the GHD report.

However, the community will not have to wait for some improvements and activities on Silver Lake. The Port Dover Lions Club recently installed a floating dock near the Market building. This dock was gifted to the Lions Club by the Silver Lake Rowing Club Legacy Fund.

Already, people are using the dock to launch canoes and kayaks as well as operate remote-controlled model boats.

In addition, the Silver Lake Revitalization Project has been selected by the Canadian Freshwater Alliance as one of 50 groups to participate in the Lake Erie Guardians Program. Local volunteers will be submitting their test results of water samples drawn from Silver Lake to Water Rangers, a national group collecting baseline data on lakes, rivers and streams flowing into Lake Erie.

The Port Dover Waterfront Preservation Association, formed in 2008, is a not-for-profit corporation whose primary goal is to advocate for the public's physical and visual access to the waterfront. For more information, visit us at [www.portdoverwaterfront.ca](http://www.portdoverwaterfront.ca).

For more information contact:

Jim Dover  
Project Manager  
Silver Lake Revitalization Project – Phase 1  
519-426-5677  
jdover@kwic.com

## Council-In-Committee Meeting – July 13, 2021

Subject: Closure of Pathway on 2 Regent Street, Port Dover  
Report Number: CAO 21-40  
Division: Office of the Chief Administrative Officer  
Department: County Solicitor  
Purpose: For Decision

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### Executive Summary:

The property owners at 2 Regent Street, Port Dover (“Grumbachs” or “Owners”) approached the County to discuss an existing pathway (“Pathway”), that bisects their property (see Attachment A, Part 2, Subject Lands). The Pathway is accessed from Grace Street and leads down to the Port Dover beach.

The Grumbachs sought to explore possible changes to the Pathway configuration to improve access to all parts of their property, given that the current configuration has the effect of limiting access to a considerable portion of the Grumbachs’ lands.

Following meetings held with the Grumbachs and County staff, staff concluded, for reasons that will be discussed below, that it is in the best interests of the County to close the Pathway and remove the staircase.

In addition, staff identified the need for an easement agreement from the Grumbachs over a portion of their lands on Part 3 (Attachment A), to address storm sewer needs and erosion concerns. The Grumbachs have agreed to permit this easement. The easement conditions would mean the Grumbachs cannot encumber the lands in any way.

### Discussion:

The Pathway is in a poor state of repair and cannot be reconstructed in a manner that is accessible to persons with disabilities. As a result, County staff are recommending that the Pathway and stairway down to the beach be dismantled and closed to the public to mitigate the liability risk, and instead, the public should be directed to the beach access off Walker Street.<sup>1</sup>

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<sup>1</sup> There is another pathway/staircase to the beach from Regent Street. See the green rectangular box at the bottom of Regent Street within Attachment A for location of that entry to the Lake. The staircase, and associated beach, is often under water. Until or unless safety issues are resolved at that location, the County is likely to close the staircase to address safety concerns and/or will be posting a “use at own risk” sign, and redirecting people to the Walker Street entrance.

The title abstract refers to this Pathway as a “an outlet to the shore of Lake Erie from Grace Street”. The title search of the Pathway lands has also revealed the last registered owner to be Jesse Forster, by a deed registered September 8, 1881. Although the County does not own the Pathway, the County, or its predecessor municipality, has had possession of the Pathway for an extended period and has allowed its use by the public as an access point to the Port Dover beach, including through a staircase at the end of it. Because the County has been in possession of these lands, legal ownership may first have to be established in the name of the County, potentially through a vesting order, and then ownership could be transferred to the Grumbachs. The Grumbachs’ counsel would be required to prepare and file all materials, and bear all costs associated with same. The County would review and approve any filings and registrations. If other or different legal steps are required, the Grumbachs would be responsible for all costs associated with same.

County staff recognize that area users may oppose this closure. However, access to the beach is much safer from Walker Street and residents and others visiting the Port Dover beach should access the beach from that location.

The acquisition of the Pathway by the Grumbachs will result in increased contributory value to the property owners, if they are able to obtain ownership. If they are not able to acquire title, then it will be lands they can “possess”, as the County is giving up its possession.

At the same time, the County, through the easement to be registered, will be affecting lands that are approximately the same size, and which lands cannot be encumbered in any way (landscaping or otherwise). Realty Services calculated that the value of the Pathway and the value of the lands subject to the easement are approximately equal (approximately \$12,500), though normally we would not pay any fee for acquiring easement rights. Given that the County is removing a liability, and that the Grumbachs will pay for all steps to acquire title, the County does not propose to seek any more than nominal compensation, should it acquire title, even if briefly for the purposes of transfer.

The closure proposal was circulated for comment to County departments and to external agencies. Risk Management favours closure of the pathway. Environment & Infrastructure Services (EIS) and Operations Services similarly support the closure and Operations (Roads) would remove the Pathway and staircase. External agencies had no objections to the closure of this pathway or provided no comments.

Costs associated with deconstructing the Pathway and staircase would be borne by the County and are estimated to be approximately \$10,000. This can be accommodated within the existing Roads operating budget.

If the Grumbachs ultimately cannot obtain title, the Grumbachs still agree to the establishment of the easement. In any case, the County recommends closure of the Pathway.

**Financial Services Comments:**

As outlined in this report, County staff have estimated costs to deconstruct the Pathway and staircase at approximately \$10,000. If Council approves staff's recommendation to close and remove the Pathway and staircase at the County's expense, Operations management have confirmed that this could be accommodated within the existing Roads operating budget using some combination of internal staff time and equipment or an external contractor depending on resource availability and the timing of when the work is to be completed.

As staff's recommendation is to close the Pathway and staircase with or without removal due to liability concerns, some operational savings would be realized as this area would not need to be maintained by County staff going forward, however, these savings are expected to be minimal. Although no current capital project is budgeted to repair or replace the existing structure, the County would avoid potential future capital costs to repair or replace the structure to meet the current standards that would likely equal or exceed the estimated \$10,000 cost to remove it.

As noted, current cost estimates associated with establishing the required easement agreement are expected to be within the normal budgeted activity range for these departments and no adjustment is required at this time.

**Interdepartmental Implications:**

Roads would be responsible for dismantling the Pathway and staircase. The County Solicitor would work with the Grumbachs' counsel and Realty Services to facilitate legal proceedings and the establishment of the easement for the County.

**Consultation(s):**

Internal departments and external agencies were consulted.

**Strategic Plan Linkage:**

Providing reliable infrastructure, from essential community amenities to transportation networks and water and wastewater services.

**Conclusion:**

Staff recommend the closure of the pathway bisecting 2 Regent Street, in Port Dover. Any legal proceedings related to the establishment of title and transfer to the Grumbachs would be prepared and paid for by the Grumbachs. The County would be responsible for removing the Pathway and staircase. At the same time, the County will take steps to establish a necessary easement on the Grumbachs' property related to erosion control and a storm sewer, at the County's cost.

**Recommendation(s):**

THAT Report CAO 21-40 Closure of Pathway on 2 Regent Street, Port Dover be received as information;

AND THAT County Staff take steps to close and dismantle the Pathway and staircase at 2 Regent Street, as identified within the report, at the County's cost;  
AND THAT the County staff be directed to assist with and/or facilitate the Owners' efforts to obtain title to the Pathway, with all materials and/or required communications to be prepared by the Owners, and all associated costs, including legal and court fees, being borne by the Owners;

AND THAT the Pathway, if it comes to be owned by the County, be declared surplus to municipal needs and then be transferred to the Owners for nominal consideration, with all costs, including legal costs, being borne by the Owners;  
AND THAT County staff take the necessary steps to establish an appropriately sized easement for the County storm sewer and erosion control on the Part 3 lands (to be surveyed), at the cost of the County, with a condition of the easement precluding any encroachment by the Owners onto the easement lands;

AND FURTHER THAT the Mayor and Clerk be authorized to sign all documents necessary to complete the transfer of title and the easement agreement.

**Attachment(s):**

- Attachment A – Grace Street Map

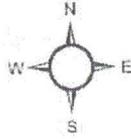
Submitted By:  
Jason Burgess  
CAO

For more information, call:  
519-426-1228 ext. 1226

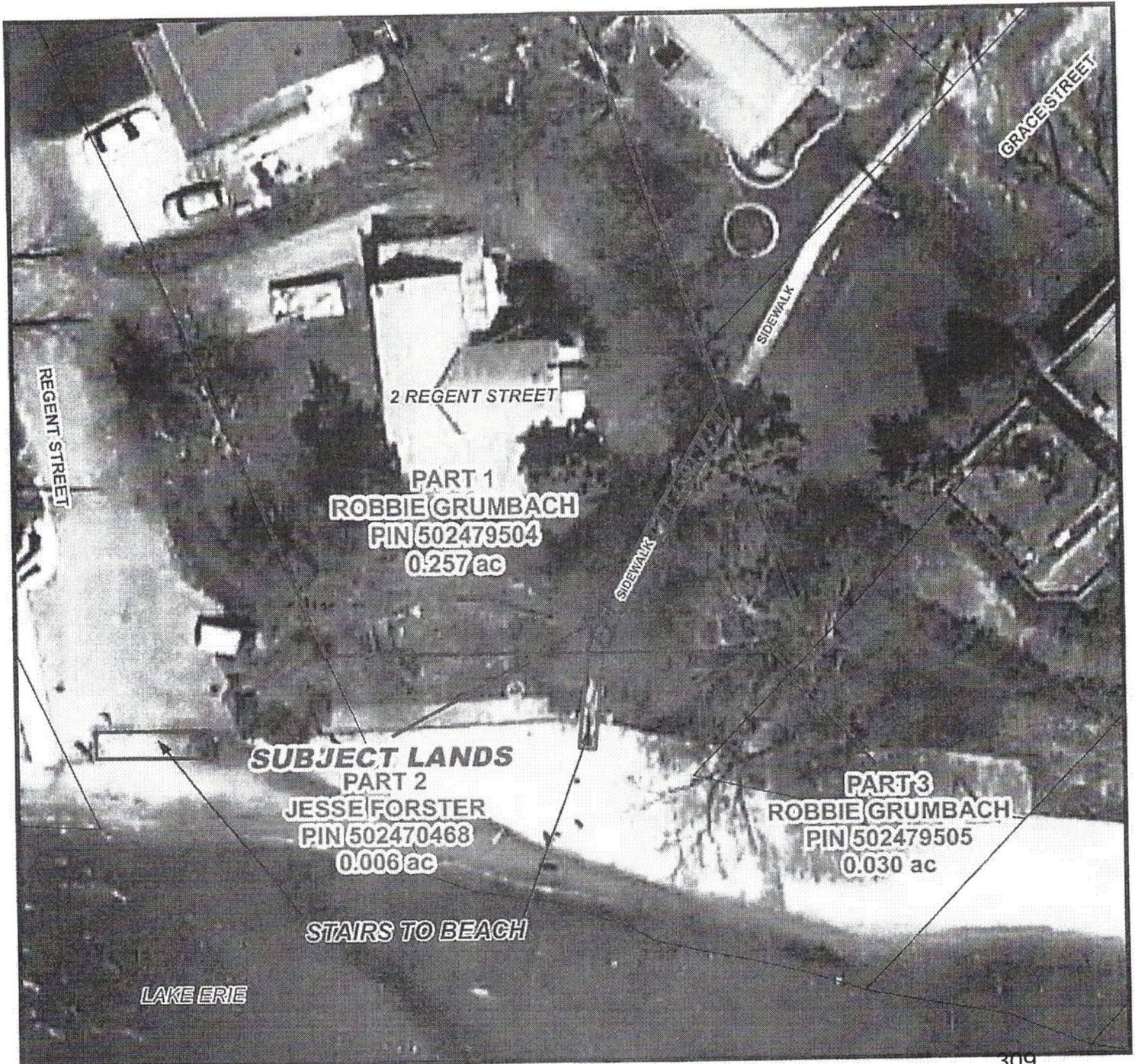
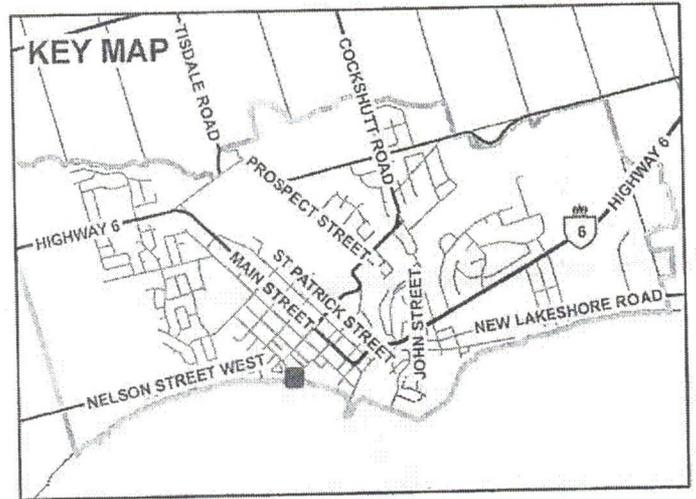
Prepared By:  
Paula Boutis  
County Solicitor

For more information, call:  
519-426-5870 ext. 1315

**MAP 1**  
**2 Regent Street and**  
**Grace Street Sidewalk**  
 Urban Area of  
**PORT DOVER**



1:400



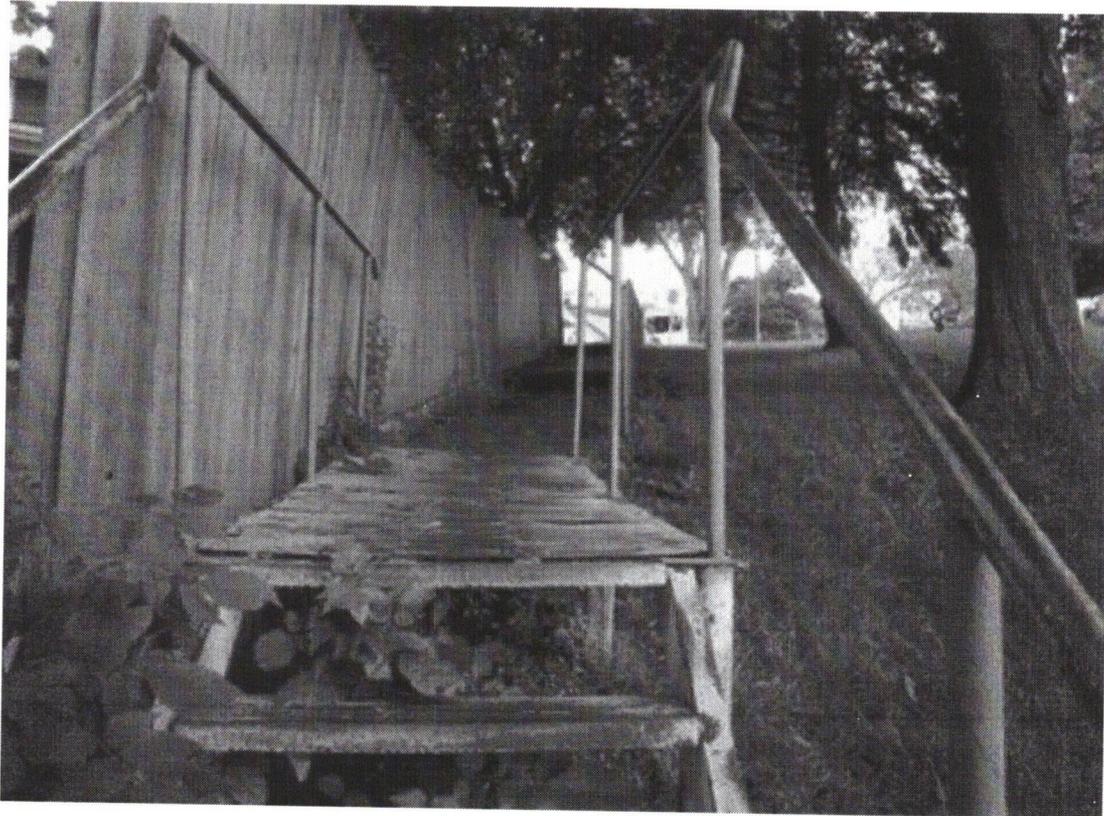
News / Local News

## Beach walkway in Port Dover under review

*Steep staircase deemed a liability hazard*

Monte Sonnenberg

Jul 15, 2021 • Last Updated 21 minutes ago • 2 minute read



The beach access at the foot of Grace Street in Port Dover features a steep length of sidewalk followed by an even steeper metal staircase leading to the waterfront. Norfolk County is exploring the possibility of closing the access and selling the land for a nominal fee to adjoining property owners. – Monte Sonnenberg

PORT DOVER – Norfolk County is preparing to decommission a long-standing walkway and staircase to the beach in Port Dover.

STORY CONTINUES BELOW





A decision is also pending on the potential closure of a second stairway to the beach nearby at the foot of Regent Street.

If the county follows through, anyone heading to the beach in Port Dover will have to do so by way of Walker Street or Harbour Street in the area of the Fishermen's Memorial.

County lawyer Paula Boutis acknowledges that some in Port Dover will have trouble with this. However, in a report to council Tuesday, Boutis said the walkway and 24-step staircase at the foot of Grace Street are in poor repair, are not handicapped-accessible, and pose a liability hazard to the municipality.

"County staff recognize that area users may oppose the closure," Boutis says in her report. "However, access to the beach is much safer from



Norfolk County reviewed the matter after the owners of 2 Regent Street asked to take possession of the access at the foot of Grace Street. The access bisects the property at 2 Regent Street and represents an obstacle to maintaining and enjoying the waterfront parcel.

A title search revealed that the last registered owner of the land beneath the Grace Street walkway was a Jesse Forster dating to 1881. Boutis has been unable to identify the current owner. However, in her report, Boutis suggested the municipality is the owner by virtue of traditional use, care and upkeep.

As a legality, Boutis says she would like to establish Norfolk as the owner – possibly through a vesting procedure – for the purposes of transferring the land, at a nominal fee – to the residents of 2 Regent Street.

STORY CONTINUES BELOW

This advertisement has not loaded yet, but your article continues below.

The metal stairway at the foot of Grace Street is steep, as is the much smaller metal staircase at the foot of Regent Street. Currently, beach-goers using these access points are met with ankle-deep water at the bottom due to historically high water levels in Lake Erie.

Agent Matthew Harms represented the Grumbach family – owners of 2 Regent Street – at Tuesday’s meeting.

“The reality is the path isn’t used very much,” Harms said, adding those who do use it often jump a nearby fence and detour across private property when confronted with water at the bottom.

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## SIMCOE REFORMER

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Harms added his clients purchased 2 Regent Street in 2014 and were aware there was a long-standing beach access on the property.

The county has yet to formulate a position on the smaller staircase at the foot of Regent Street. An alternative to closure, Boutis said, is posting a sign warning beach-goers that they use this access at their own risk.

If the county divests itself of the Grace Street pathway, plans are to secure an easement on 2 Regent Street to secure access to a stormwater outlet nearby and for the purposes of shoreline erosion control.

[MSonnenberg@postmedia.com](mailto:MSonnenberg@postmedia.com)

VIEW ON SIMCOE REFORMER

**THIS WEEK IN FLYERS**

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## PORT DOVER WATERFRONT PRESERVATION ASSOCIATION RESPONSE TO THE DRAFT PORT DOVER SECONDARY PLAN

### 1. Revise 2.1 Vision Statement on page 3 as follows:

#### 2.1 Vision Statement

Port Dover is a healthy, sustainable and complete community growing and prospering within the context of its 18 km of water's edge **waterfront** along the rivers and lakes. **The community cherishes its history, heritage** and recognizing and celebrating the history and heritage of the community **small-town character**.

New development will contribute to **preserving and enhancing the public's physical and visual access to the waterfront**, establishing beautiful neighbourhoods, a network of green spaces, protecting natural areas, a connected trail systems, safe streets and providing a variety of shops and services.

The Downtown and Urban **Downtown** Waterfront Areas will remain the heart and soul of Port Dover, with new development contributing to the creation of successful business districts and neighbourhoods.

### 2. Revise 2.2 Guiding Principles beginning on page 3 as follows:

#### 2.2 Guiding Principles

The Secondary Plan includes Guiding Principles<sup>2</sup> that alongside the Vision Statement to give direction on how the physical aspects of the community will be developed.

**Principle 1:** **The public's physical and visual access to the waterfront will be preserved and enhanced throughout Port Dover to provide a variety of recreational activities both on land and on the water for residents and tourists.**

~~Principle 12:~~ Downtown Port Dover will thrive as a destination for residents and visitors alike to access shopping, restaurants, culture and entertainment while new development will be compatible in **with its small-town character, history and heritage.** ~~by enhancing streets and public spaces.~~

~~Principle 23:~~ Port Dover's Urban **vibrant Downtown** Waterfront will continue to be a pedestrian focused destination that provides the greatest mix of uses, including residential uses to this vibrant district. Development will be compatible in character **with, front and feature the waterfront and maximize preserve as well as enhance** public access to the water's edge **and front the waterfront.** ~~—along the Lake Erie Shoreline, as well as adjacent to the Lynn River and Black Creek.~~

Principle 67: Provide a well-connected, multi-modal transportation network that gives priority to creating safe streets for pedestrians and cyclists while providing a balanced supply of parking. **Connective networks to the waterfront will provide recreational boaters with improved access to the transportation network and the Downtown Area.** The concepts of complete streets and active transportation will be incorporated in all transportation planning decisions.

**Renumber remaining Principles as required.**

### **3. Revise 3.2 Policies for a Successful Community Waterfront Acquisition j) on Page 12 as follows:**

~~Waterfront Acquisition~~ **Preservation and Enhancement**

j) Much of the ~~Lake Erie and Silver Lake Shorelines, as well as the banks of the Lynn River and Black Creek~~ **waterfront throughout** within Port Dover is privately owned and has been developed for many years. **The County shall preserve and enhance the public's physical and visual access to the waterfront throughout Port Dover in the following ways:**

- i) Where development is proposed, the ~~provision of~~ **preservation and enhancement of public access to the river's edge or to the shorelines of Lake Erie and Silver Lake waterfront** shall be pursued through **such means as, but not limited to:**
  - required parkland dedications;
  - gifts **and** bequests;
  - purchases from funds allocated through the cash-in-lieu of parkland provisions;
  - ~~or~~ other appropriate sources, **such as** land exchanges;
  - ~~and/or negotiated~~ **negotiations** through the development approval processes. ~~The County shall pursue the acquisition of waterfront property and public access to the waterfront in appropriate locations throughout Port Dover, particularly to ensure preservation of the beach within the Port Dover Urban Waterfront Designation.~~
  
- ii) **Proactively pursuing initiatives such as, but not limited to, the preservation and enhancement of the public's physical and visual access to the waterfront in Port Dover by:**
  - **identifying all publicly owned and potential waterfront locations throughout Port Dover;**
  - **setting quantifiable targets as to how much waterfront access will be enhanced and expanded;**
  - **exploring ways to preserve and enhance the public's access to the waterfront with property owners;**
  - **partnering with landowners and community organizations;**
  - **purchasing property**
  - **negotiating Privately Owned Publicly Accessible Spaces agreements;**

**4. Rename Section 6.2 Port Dover Urban Waterfront Designation on Page 43 as follows:**

Section 6.2 Port Dover ~~Urban~~ **Downtown** Waterfront Designation

**5. Regarding Section 6.2 Port Dover Urban Waterfront Designation on Page 43 and Schedule B: Land Use:**

Create a new Sub Section titled: Port Dover Harbour Marina Site Specific Policy Area

**6. Regarding the newly created Port Dover Harbour Marina Site Specific Policy Area:**

Include as an Objective that the Port Dover Harbour Marina shall continue to be a publicly owned and operated facility that provides public access;

**7. Regarding Section 6.2.5 Lynn River/Black Creek Special Policy Area on page 51 and Schedule B: Land Use:**

Expand the boundary of the Lynn River / Black Creek Special Policy Area by including the Black Creek waterfront northeast to Concession 2 Woodhouse;

**8. Regarding Section 6.3.5 Silver Lake Special Policy Area on page 57 and Schedule B: Land Use:**

Revise the boundary of the Silver Lake Special Policy Area to include:

- a. All the property owned by the Port Dover Lions Club along the Silver Lake waterfront including the northern shore east to the Lynn River and the strip of property that extends north to Prospect Street;
- b. The southern portion of the Silver Lake waterfront that is currently included in the Port Dover Urban Waterfront Designation

**9. Regarding Section 6.3.5 Silver Lake Special Policy Area on page 57 and Schedule B: Land Use:**

Change the designation of the former Public Works Yard on Nelson Street from Urban Residential to Open Space;

**10. Provide an opportunity for the County Solicitor to review and input to the Draft Port Dover Secondary Plan before finalizing the version that will be presented to Council for approval;**

**11. Provide a minimum of 30 days for public consultation immediately before the Port Dover Secondary Plan is presented to Council for approval.**

## Appendix F.7

### DEPUTATION July 20, 2021

Thank you Mayor Chopp!

I am addressing Council members today on behalf of the Port Dover Waterfront Preservation Association regarding Report CAO 21-61 Public Access to Port Dover Beach.

As you know, our Association has been advocating for the preservation and enhancement of the public's physical and visual access to Port Dover's waterfront since 2008. Our Association has successfully advocated for the Port Dover Harbour Marina to remain a publicly owned and operated facility, the preservation of the Port Dover Lighthouse and repair of Misner Dam. More recently, the Association announced it will lead Phase 1 of the Silver Lake Revitalization Project. The Association is also advocating for the creation of a waterfront trail along the Silver Lake and Lynn River waterfronts to connect the Lynn Valley Trail to the Port Dover pier. We are currently seeking volunteers for this Project. Just last week, Association volunteers participated in the Engagement Sessions hosted by County Staff regarding the development of the Port Dover Secondary Plan.

I have been asked by our Association to express our Association's support for Option 2 that is recommended in Report CAO 21-61. However, the Association notes that parts of this Report are also being discussed In Camera. Our Association requests that nothing be decided In Camera that would hinder the

County's ability to preserve and enhance any Port Dover waterfront access including those access points in the vicinity of 2 Regent Street.

In the Port Dover Secondary Plan Engagement Sessions, our Association advocated for the County to adopt, as a matter of Policy in the Port Dover Secondary Plan, a more proactive role in preserving and enhancing all 18 kilometers of Port Dover's waterfront. Option 2 is an example of the proactive role we envisage. We are advocating for this type of approach to be incorporated into the Port Dover Secondary Plan.

To be clear, when our Association refers to waterfront, we are referring not just to the Port Dover beach and harbour. We are referring to the Lynn River up to Misner Dam and through Silver Lake to Ivey's Dam as well as up Black Creek from Coleman's Point to Concession 2 Woodhouse and all of the Lake Erie shoreline. Our representatives made this point, I think strongly, during the Engagement Sessions.

Report CAO 21-61 mentions "The County's strong record of accomplishment of maintaining public access to waterfronts". Unfortunately, the original Report – CAO 21-40 Closure of Pathway on 2 Regent Street Port Dover – has seriously undermined the public's faith in the County's and Staff's commitment to protect and enhance the public's access to the waterfront by recommending the closure of one pathway leading to the Port Dover beach and putting into question a second.

Thankfully, that Report and its recommendations are now dead and we understand withdrawn. This outcome is thanks to:

- the Simcoe Reformer and journalist Monte Sonnenberg who made the community aware of the recommendations in the original Report;
- residents like Helen Wagenaar who placed a post on social media asking people to indicate whether they agreed the pathway should be kept open – and the over 500 who expressed their agreement in just four days – it was the least controversial post on Currently In Port Dover that I’ve ever seen!;
- and the 15 residents who took the time to express their concerns in writing to Council that are included in your Agenda today;

The first Report that made its way to Council regarding this issue demonstrates the need for continued vigilance by the public. With this in mind, our Association will be requesting a meeting with the new CAO to review this issue and to explore ways to avoid a turn of events such as this from happening again. Some suggestions will include ensuring that:

- Planning Staff have the opportunity to review and comment on Reports related to the public’s access to the waterfront before they are finalized;
- Reports are presented and fully discussed at Council In Committee first as a matter of practice – including those that are deferred – unless there is good reason to do otherwise.

Our Association will continue to advocate for the preservation and enhancement of the public’s access to all of Port Dover’s 18 kilometers of waterfront. We

encourage the public to get involved as well. In particular, we encourage residents to become aware of the Port Dover Secondary Plan review. Copies of the draft Plan are available on the County website. Another round of public consultation is planned for possibly as early as this September.

The Association has formally requested that the County provide the public sufficient time – at least 30 days - to review the next Draft Plan, to ask questions and provide their input before it is presented to Council.

On behalf of the Port Dover Waterfront Preservation Board of Directors, I would like to thank you for permitting our Deputation today at such short notice.

July 30, 2021

Mr. Brandon Sloan, RPP, MCIP  
Norfolk County – Planning Department  
Norfolk County Administration Building  
50 Colborne Street South  
Simcoe, ON  
N3Y 4H3

Dear Mr. Sloan:

**RE: Draft Port Dover Secondary Plan, Urban Design Guidelines and Urban Forestry Guidelines  
Initial Comments  
Shore Developments Limited (225 and 230 Chapman Street East and 2 Lynn Street)  
OUR FILE: 19190**

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MHBC are retained by Shore Developments Limited (“**Shore Developments**”) in relation to their lands located at 225 and 230 Chapman Street East and 2 Lynn Street in Port Dover in the County of Norfolk (the “Subject Lands”). Shore Developments submitted applications in January of 2019 to facilitate the remediation and redevelopment of the Subject Lands to provide for much needed new housing in a range of residential forms inclusive of townhomes and apartments. Since submitting the applications, we have participated in and monitored the Port Dover Secondary Plan Study that we understand is being undertaken as a result of the Interim Control By-law passed in September of 2019.

We have now had an opportunity to briefly review the proposed Draft Port Dover Secondary Plan and the associated Urban Design Guidelines and Urban Forestry Guidelines that were released on June 25, 2021. Through our initial review we have identified a number of land use planning matters and issues that we believe warrant further examination and review.

The Subject Lands are currently designated *Urban Residential* (225 Chapman Street East) and *Urban Waterfront* (230 Chapman Street East and 2 Lynn Street) in the County Official Plan. The current Official Plan policies provide for residential development in the *Urban Residential* designation and mixed-use residential development in the *Urban Waterfront* designation up to a maximum height of 4-storeys. We have continued to provide input respecting the Port Dover Secondary Plan process by stating that the current height limit should be revised to allow for increased height and density along with additional built form policies to provide for appropriate opportunities for planned intensification that optimizes the existing underutilized lands in the area, including the Subject Lands, which represent a unique brownfield opportunity.

The Draft Secondary Plan does not reflect policies that provide a framework to facilitate the much-needed redevelopment and growth for the area, including especially the Subject Lands.

Our initial issues and concerns with the Draft Secondary Plan can be summarized as follows:

1. The policies of the new *Urban Waterfront* designation in the Secondary Plan have been modified to reduce the maximum permitted building height from 4-storeys to 3.5-storeys or 12.5 metres. This represents a down-designation, ignores the current application and planning justification, and is contrary to the policies of the Provincial Policy Statement that require opportunities for intensification in the built-up area and does not direct the “down zoning” of areas. No justification for the reduction in height has been provided and justification as to why key underutilized lands, and brownfield sites, including our client’s lands, have not been identified for additional growth opportunities. Rather than down designating these lands, policies must be implemented that encourage the revitalization of brownfields, which in turn mitigates environmental impact. Increasing the height permissions on the subject property would encourage a brownfield redevelopment as well as intensification and infill in an area that has access to existing municipal infrastructure, social, and recreational activities. We request the Draft Secondary Plan be revised accordingly.
2. The 225 Chapman Street East property is currently designated *Urban Residential* and is proposed to be re-designated as *Urban Waterfront*. This re-designation would require the redevelopment of 225 Chapman Street East to be a mixed-use development (requiring non-residential uses at grade with residential above). As noted in our development applications, we believe this site is appropriate for infill as a townhome development given its depth and frontage. We do not believe non-residential uses at grade at this location is appropriate. Requiring additional non-residential development on this site and the Subject Lands in general, is not appropriate as such uses are best located in “main street” locations such as the Downtown. Forcing prescribed non-residential uses in the redevelopment of the Subject Lands, especially given the further restrictions on height, is simply not reflective of an appropriate policy to facilitate a form and level of intensification, given the site context. It does not facilitate efficient or achievable redevelopment. In our view, the use permissions in the *Urban Residential* designation are more appropriate for the subject lands and we request that the Draft Secondary Plan be revised accordingly.

Our issues and concerns with the Draft Urban Design Guidelines can be summarized as follow:

3. When describing lands along the Lynn River (Port Dover Urban Waterfront), the design guidelines state the following: “*The lands along the Lynn River to the north of the harbour are no longer used as part of the commercial fishing industry and are in the process of being redeveloped for residential uses.*” We fully support this statement. However, this conflicts with the Draft Secondary Plan, which states: “*Port Dover Urban Waterfront area is to remain a unique area that supports tourism, the commercial fishery and provides recreational, commercial and cultural opportunities*”. We believe the land along the Lynn River should be an evolving area that supports and integrates new residential development recognizing its unique context and this should be consistent in both the Secondary Plan and the Design Guidelines. It is appropriate and represents good planning to include polices in the Draft Secondary Plan that reflect this principle.
4. When describing the general building guidelines for the *Urban Waterfront*, the design guidelines state the following: “*Taller buildings may be encouraged at strategic locations such as corner sites and other prominent sites to enhance community structure, sense of place and provide landmarks*”. It is not clear what height is envisioned by this statement, as the maximum permitted height within the *Urban Waterfront* designation of the Secondary Plan is 3.5-storeys or 12.5 metres which is highly

restrictive in terms of achievable built forms and not reflective of the principle articulated earlier. In our view, the as of right height maximum for the subject property must be 8-storeys at a minimum.

5. When describing the general building guidelines for the *Urban Waterfront*, the design guidelines also state: *"Where buildings have residential units at ground level, provide a 'front yard' landscape zone between the unit and the pedestrian zone to include demarcation of the private realm front yard space"*. This statement also conflicts with the *Urban Waterfront* policies of the Draft Secondary Plan, as new residential uses are only permitted above-grade as part of a mixed use building: *"New residential apartments shall only be permitted as part of a mixed-use building, with another permitted use located at-grade and the residential use located above grade"*. Again, we believe it is not appropriate to require mixed use development in all of the *Urban Waterfront* areas and should not be imposed in areas where such uses were previously not permitted. We request that the draft design guidelines be revised accordingly. It is unclear why this shift has been recommended.
6. When describing waterfront sites, the design guidelines state: *"Facing the water's edge, the building wall at the base of the building should be a maximum of 13.5 metres or 4 storeys in height, whichever is less"*. This statement conflicts with the Draft the Secondary Plan, which only permits heights up to 3.5 storeys or 12.5 metres within the new *Urban Waterfront* designation. Again, the Secondary Plan and Design Guidelines should be consistent. However, as discussed earlier, we do not support the down zoning of the Subject Lands in the Draft Secondary Plan and we request that maximum height permissions be increased to 8-storeys.
7. There are also urban design guidelines which require compliance with an angular plane measured from the water's edge pedestrian zone. It is not clear where this zone is located. We request that the draft design guidelines be revised to appropriately define this and to permit exceptions where appropriate.
8. When addressing surface parking, the design guidelines state it is to be limited to underground parking. However, with a 3.5-storey height limit, it is unclear how underground parking would be feasible or required given the limited built forms that can be achieved within 3.5-storeys. We request that the draft design guidelines be revised to permit at grade parking.
9. Section 8.2.16 of the Design Guidelines describe a water's edge trail as follows: *"The water's edge trail should have a minimum width of 3m wherever possible"* and, *"Provide a minimum 2m landscape/furniture zone in association with the trail."* We recommend that the cost and methods of the acquisition of this land by the municipality be assessed prior to the adoption of this policy as implementation may be a concern. In addition, we recommend that the impact of this additional taking be assessed to ensure that it will not sterilize what would otherwise be developable land. Finally, we request that the Draft Secondary Plan be revised to clearly state that this proposed trail may be located within Hazard Lands or a buffer along the water's edge

We strongly believe the Draft Secondary Plan has not addressed the many opportunities to accommodate growth and much needed housing supply through appropriate intensification as required by Provincial policy. We believe there are several opportunities for planned intensification within existing areas of Port Dover that have been overlooked and we strongly object to the down zoning of the Subject Lands.

We note that the deadline to provide comment on the initial Draft Secondary Plan and Design Guidelines was not published on Norfolk County's website, stated within the July 2021 online discussion materials, or provided through the County's project circulation list. We further request that subsequent drafts of the Draft Secondary Plan and Urban Design Guidelines be released with justification for any proposed changes, and that a clear timeframe through which the County will accept formal comments be provided alongside the next release of materials in order to ensure that all stakeholders are given proper notice to fully review and provide comments.

We appreciate the opportunity to provide these initial comments and trust the County will take them into consideration during the Secondary Plan's further review and revisions.

Yours Truly,

**MHBC**

A handwritten signature in black ink, appearing to read 'Dana Anderson', written in a cursive style.

Dana Anderson, MA, FCIP, RPP  
Partner

# Appendix F.9



**IBI GROUP**  
200 East Wing – 360 James Street North  
Hamilton ON L8L 1H5 Canada  
tel 905 546 1010  
[ibigroup.com](http://ibigroup.com)

September 9, 2021

Ms. Jennifer Catarino, MCIP RPP  
Senior Planner  
County of Norfolk  
Community Development Division  
185 Robinson Street  
Simcoe, Ontario N3Y 5L6

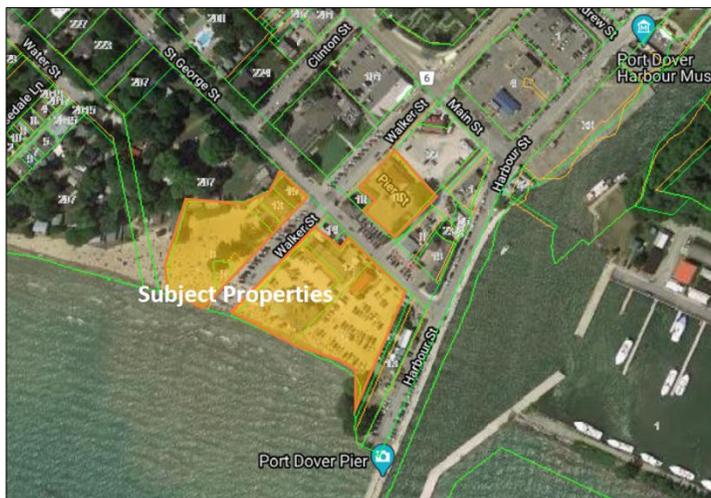
Dear Ms. Catarino:

**RESPONSE TO DRAFT PORT DOVER SECONDARY PLAN  
F.W. KNECHTEL FOODS LTD.  
15 WALKER STREET, BOX 549, PORT DOVER**

IBI Group Professional Services (Canada) Inc. (“IBI”) has been retained by F.W. Knechtel Group (collectively “the Knechtel Group”), to provide professional land use and planning services with respect to their lands municipally known as 2, 5, 9, 11, 12, 13, 15 and 18 Walkers Street, and 5 St. George Street, Port Dover (collectively “the Subject Lands”). More specifically, IBI has been engaged to monitor and participate in the County of Norfolk’s Port Dover Secondary Plan Review process, in relation to the Subject Lands.

**Subject Property**

Our client’s property is approximately 1.49ha (3.69acres) and contains existing buildings generally bounded by Main Street to the north, Harbour Street to the east and Lake Erie to the south. The Subject Lands also includes area east and west of Walkers Street. Please see Figure 1 below for a location plan of the Subject Lands.



*Figure 1: The Knechtel Group landholdings location plan, Port Dover, Ontario (Google Imagery)*

Ms. Jennifer Catarino, MCIP RPP – **Error! Reference source not found.**

The Subject Lands are designated Urban Waterfront on Schedule B-16 of the Norfolk County Official Plan and identified as being within the Port Dover Waterfront Special Policy Area governed by Policy 6.5.2.2 of the Official Plan. Furthermore Schedule “F” of the Lakeshore Special Policy Area Secondary Plan designates Port Dover as Urban Area and identifies the community as an Urban Tourism Node (Conceptual). Lastly, the 2001 Port Dover Secondary Plan designates the Subject Lands as Harbour Commercial and “Other Hazard” Lands.

The Subject Lands are zoned Central Business District on Schedules A-32 and A-33 in the County of Norfolk Zoning By-law I-Z-2014. In addition, the Subject Lands are subject to Section 3.38 of the Zoning By-law requiring Site Plan Control pursuant to Section 41 of the Planning Act.

## **Review**

Through the update of the Port Dover Secondary Plan and Draft Design Guidelines IBI Group has been monitoring attending workshops providing correspondence during the process. We are pleased and support the need to update the Port Dover Secondary Plan in order to reflect the importance of Port Dover and its waterfront as a significant waterfront community and tourist node.

We have reviewed the Draft Port Dover Secondary Plan document and generally support the revisions to the policies, however we require further direction or understanding on how the of the waterfront area will be supported to permit redevelopment into the desired waterfront urban community.

On this topic we offer the following questions/concerns:

- The redevelopment of the waterfront is a community objective to maintain the economic and function of Port Dover as a major beach destination. As such, resiliency and preparedness to flooding and hazards should be done as a community effort. Further clarification on how protection of the beach area through appropriate shore management structures will be funded be it through the use of area specific development charges, front ending agreements under the Development Charges Act, or other suitable arrangements to implement the development of the secondary plan area and fairly allocate related costs for the protection of the shoreline.
- Concern and clarification are required for areas within the Greenlands System as shown on Schedule A (Community Structure). Section 4.2 ii) of the Plan describes that the Greenlands System identifies areas where development will be prohibited in which is contradictory to other development/redevelopment policies for the urban waterfront area.
- Concern and further clarification required to identify areas promoted for future intensification. In terms of the urban waterfront area, provide a density target for redevelopment that would endorse the objective of developing into a mixed-use including new residential units.
- Further clarification and policy direction are required to support the redevelopment, expansion or relocation of existing buildings as well as new buildings within the urban waterfront area.
- Concern with Policy 6.2.3 m) restricting building heights to 3.5 stores and a suggestion that the height be aligned with the Section 2.3 of the draft urban design guidelines which allow for heights up to 4 storeys for the Lake Erie shoreline area. In addition, bonusing provisions for increased height and density should be provided recognizing development/redevelopment in the urban waterfront area which propose elements for the public benefit.

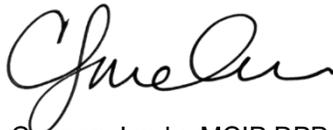
Ms. Jennifer Catarino, MCIP RPP – **Error! Reference source not found.**

We would like to reiterate our general support of the draft secondary plan and look forward to further iterations of the draft secondary plan for review. The Subject Lands owned by our client will play an integral part towards the success of the redevelopment and protection of the Lake Erie waterfront area and Port Dover beach destination.

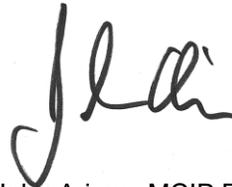
We would appreciate an opportunity to discuss the concerns clarifications we have listed above. Through this letter, we request to be notified of any future public/committee/Council meetings or staff reports on this matter and look forward to our continued correspondence through the process.

Yours Sincerely,

**IBI Group**



Carmen Jandu, MCIP RPP  
Associate, Senior Planner



John Ariens, MCIP RPP  
Associate Director, Practice Lead

Copy:

Mr. D. Knechtel – FW Knechtel Foods Ltd.